



Madison, Wisconsin

CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

BIKE PATHS - 2012

INDEX OF SHEETS

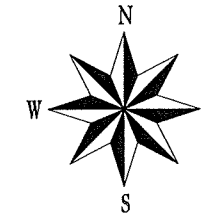
SHEET NO.	TITLE
1	GENERAL NOTES, LEGEND AND EARTHWORK
2	TYPICAL SECTIONS
D-1	CONSTRUCTION DETAILS
D-2	PILGRIM PARK PATH PLAN & PROFILE
P-1	ODANA HILLS PATH PLAN
P-2	SOUTHWEST PATH PLAN
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X-1 - X-2	REINDAHL PARK PATH CROSS SECTIONS
X-3 - X-7	

NOTES:
1. SEE SHEET 2 FOR GENERAL NOTES AND EARTHWORK SUMMARIES

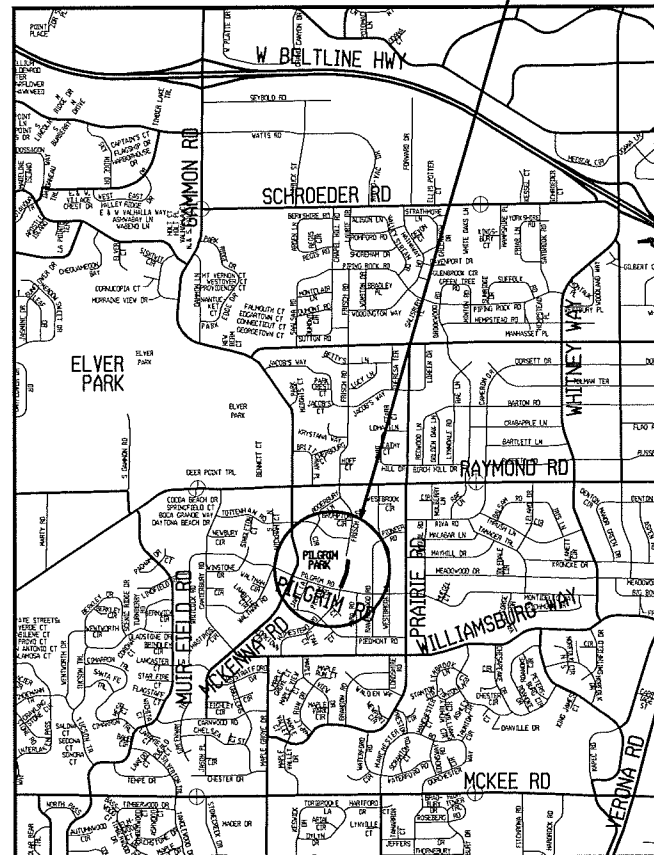
REINDAHL PARK PATH, PILGRIM PARK PATH,
ODANA HILLS PATH, SOUTHWEST PATH SHOULDER
RECONSTRUCTION

CITY PROJECT NO. 53W1297

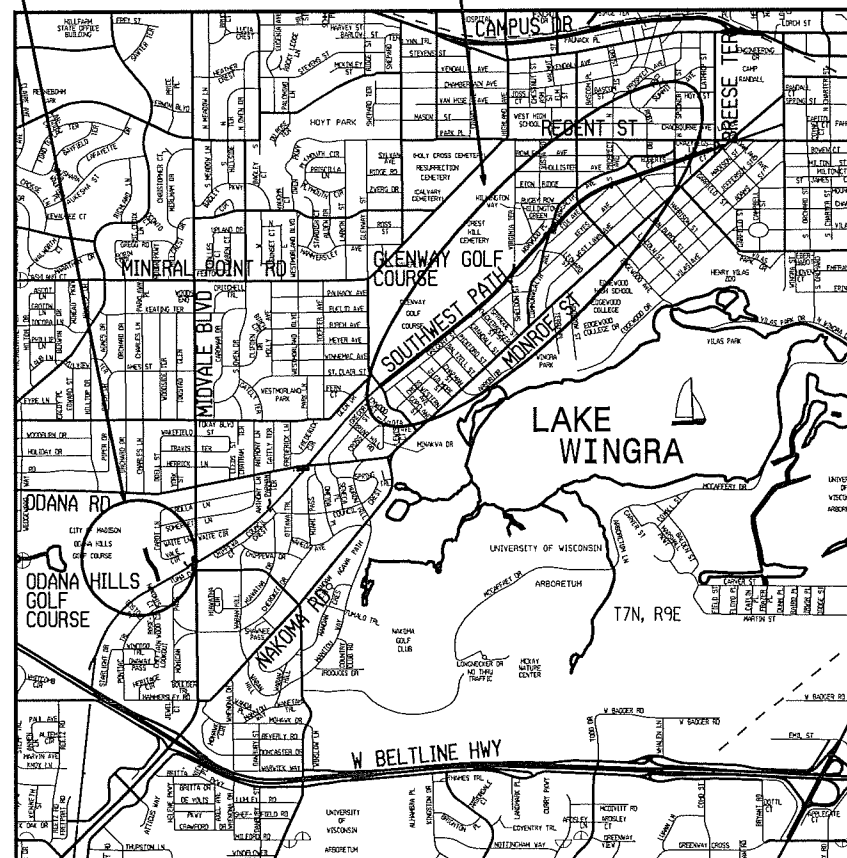
CONTRACT NO. 6776



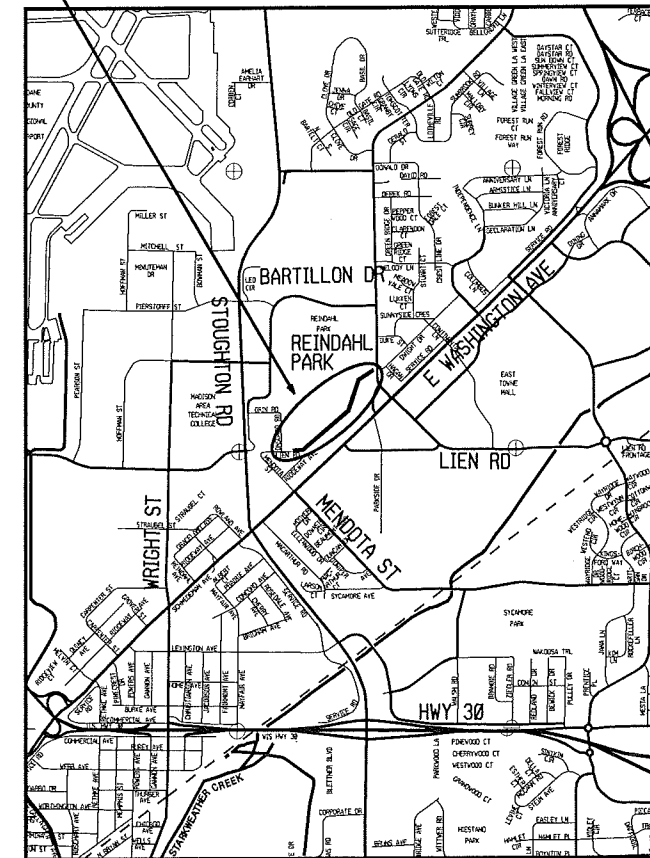
PROJECT LOCATION
PILGRIM PARK



ODANA HILLS SOUTHWEST PATH



REINDAHL PARK



PUBLIC IMPROVEMENT PROJECT APPROVED

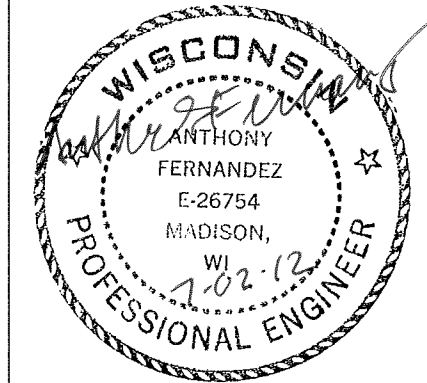
JUNE 19, 2012

BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

Michael R. Daily 7/3/12
City Engineer Date

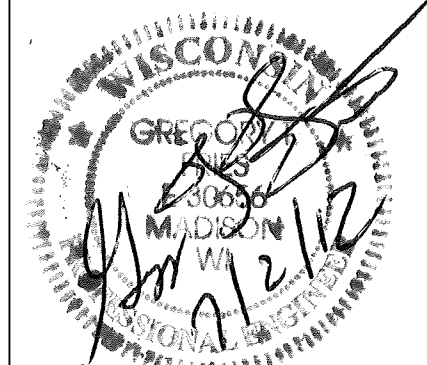
PATH AND SIDEWALK GEOMETRIC
DESIGN BY:



PATH GRADES DESIGNED BY:



STORM SEWER AND EROSION
CONTROL DESIGNED BY:



GENERAL NOTES

TERRACES SHALL SLOPE TOWARD THE GUTTER EXCEPT WHERE OTHERWISE SHOWN ON CROSS SECTIONS. SLOPES FORMING DITCHES SHALL NOT EXCEED 4:1 UNLESS OTHERWISE SHOWN ON PLANS AND CROSS SECTIONS.

THE CROSS SLOPE OF SIDEWALKS SHALL BE 2%. THE LONGITUDINAL GRADE OF CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.03. AND DETAILS IN THESE PLANS.

LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED THE GRADE SHOWN ON PLANS NOR BE LESS THAN 0.5%. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 3:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND APPROVED BY THE ENGINEER PRIOR TO PLACING CONCRETE.

CURB STATION AND OFFSETS SHALL BE TO THE FACE OF CURB UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE TOP OF CURB (OR EXTENDED TOP OF CURB FOR DRIVEWAYS OR RAMPS) UNLESS OTHERWISE INDICATED.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.


ALL PERMANENT SIGNING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

EROSION CONTROL NOTES

1. FOR PILGRIM PARK PATH SEE SHEETS D-1 AND U-1 FOR EROSION CONTROL INFORMATION
2. FOR SOUTHWEST PATH SHOULDERS SEE SHEET D-1 AND P-3 FOR EROSION CONTROL INFORMATION.
3. FOR REINDAHL PATH SEE SHEET D-1 AND SHEETS P-6 THROUGH P-10 FOR EROSION CONTROL INFORMATION

CONVENTIONAL SYMBOLS
FIELD VERIFY ALL UTILITY LOCATIONS

- GAS — G —
- STORM SEWER — ST —
- SANITARY SEWER — SAN —
- WATER — W —
- OVERHEAD ELECTRIC — OH —
- UNDERGROUND ELECTRIC — E —
- UNDERGROUND FIBER — FO —
- POWER POLE — □
- COMBUSTIBLE FLUIDS — 

EARTHWORK SUMMARY - PILGRIM PARK PATH

UNCLASSIFIED EXCAVATION (PLAN QUANTITY)..... 250 CY
ESTIMATED UNDISTIBUTED UNDERCUT 100 CY
TOTAL EXCAVATION CUT (PAID QUANTITY) 350 CY
ADJUSTED FILL (EXPANSION FACTOR 1.2) 8 CY
① ESTIMATED WASTE 242 CY

EARTHWORK SUMMARY - ODANA HILLS PATH

UNCLASSIFIED EXCAVATION (PLAN QUANTITY)..... 25 CY
ESTIMATED UNDISTIBUTED UNDERCUT 0 CY
② TOTAL EXCAVATION CUT (PAID QUANTITY) 25 CY
ADJUSTED FILL 0 CY
① ESTIMATED WASTE 25 CY

EARTHWORK SUMMARY - SOUTHWEST PATH SHOULDERS

UNCLASSIFIED EXCAVATION (PLAN QUANTITY)..... 490 CY
ESTIMATED UNDISTIBUTED UNDERCUT 0 CY
② TOTAL EXCAVATION CUT (PAID QUANTITY) 490 CY
ADJUSTED FILL 0 CY
① ESTIMATED WASTE 490 CY

EARTHWORK SUMMARY - REINDAHL PARK PATH

UNCLASSIFIED EXCAVATION (PLAN QUANTITY).....895 CY
ESTIMATED UNDISTIBUTED UNDERCUT 395 CY
TOTAL EXCAVATION CUT (PAID QUANTITY) 1290 CY
ADJUSTED FILL (EXPANSION FACTOR 1.2)265 CY
① ESTIMATED WASTE 630 CY

EARTHWORK NOTES

- ① UNDERCUT NOT USED TO BALANCE EARTHWORK
- ② PAID FOR AS A LUMP SUM ITEM

PLOT SCALE:

PLOT NAME:

REV. DATE:

NOTES FOR TYPICAL SECTION SHEETS:

1. HMA PAVEMENTS SHALL BE CONSTRUCTED AS FOLLOWS:

TYPE	LAYER	THICK.	NOM. SIZE
5 1/2" E-1	UPPER	1.75"	12.5 MM
	LOWER	3.50"	19.0 MM
3" E-0.3	ONE LIFT	3.00"	12.5 MM

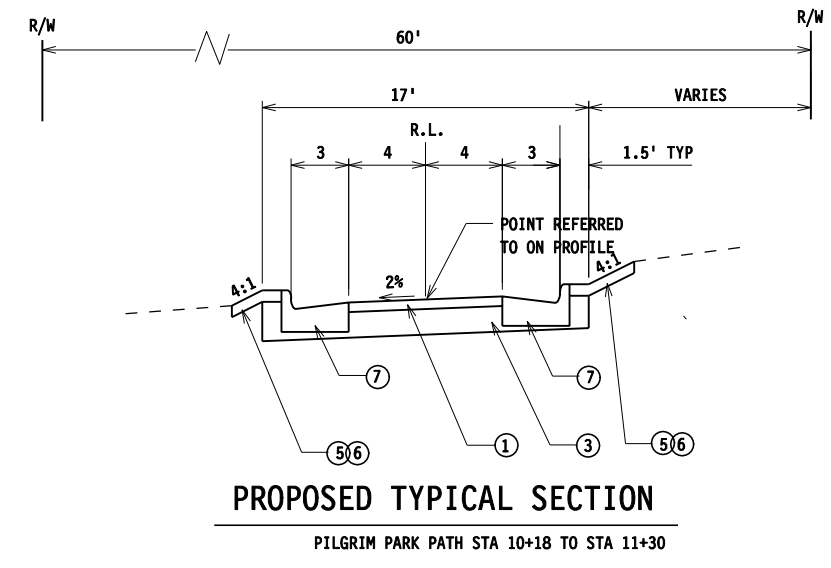
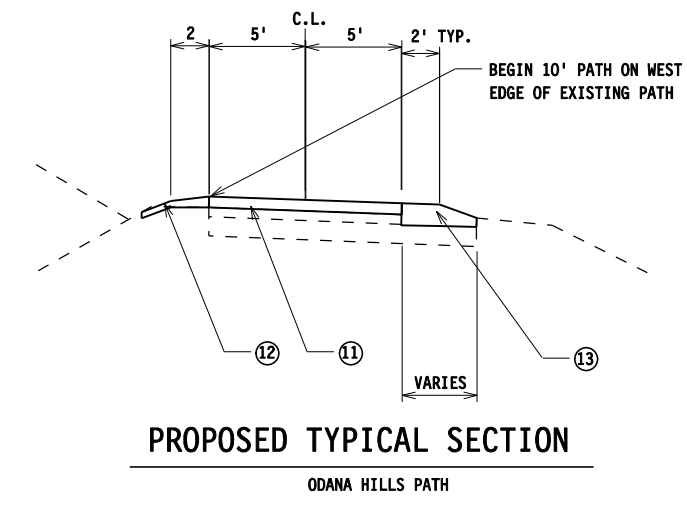
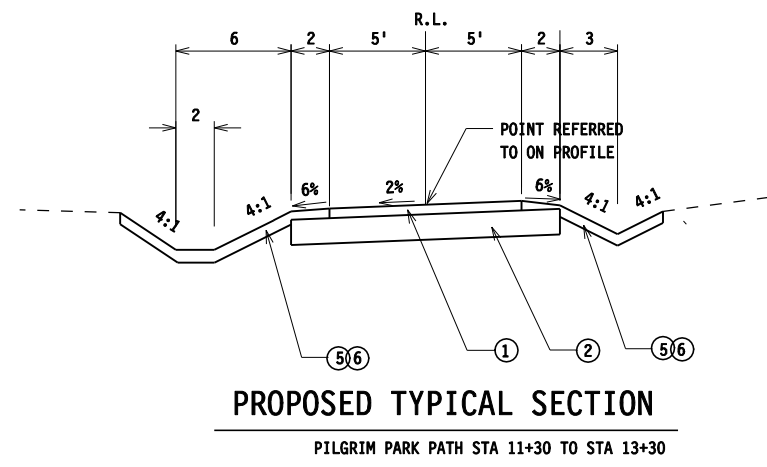
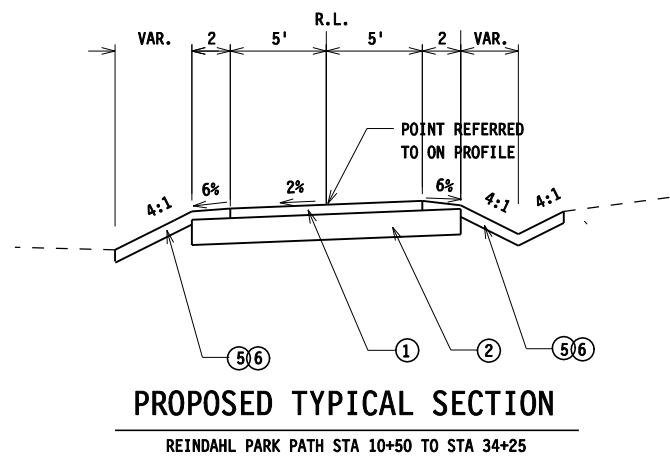
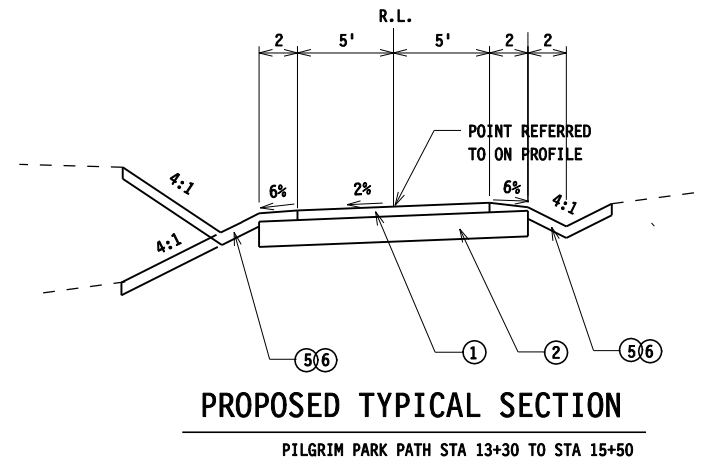
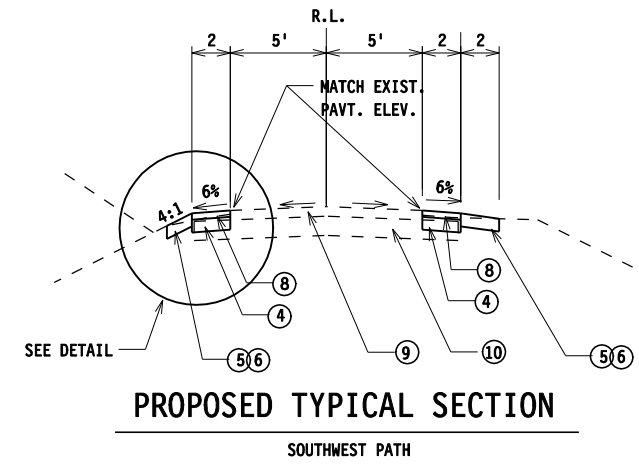
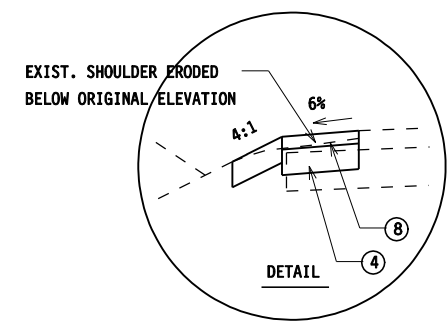
2. FOR PHASE 1 PATH FROM STA 10+35 TO STA 16+90 THE GRADING BASE COURSE AND RESTORATION OUTSIDE SHOULDER LIMITS IS BY OTHERS AND IS NOT INCLUDED IN THIS CONTRACT. SEE SPECIAL PROVISIONS.

3. SEE CROSS SECTION SHEETS FOR CUT SLOPES.

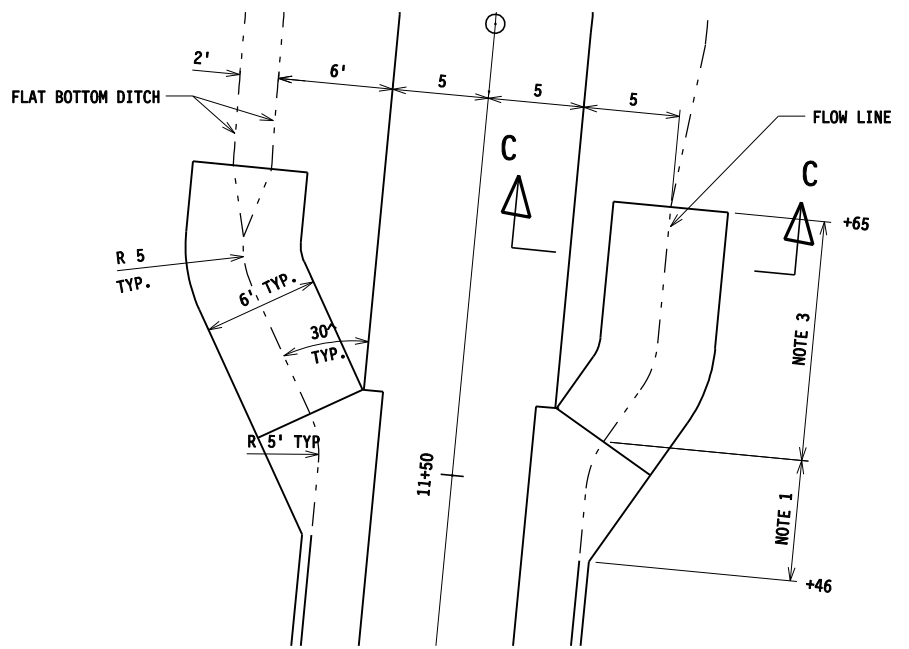
4. ALL SEEDED AREAS SHALL BE MATTED WITH EROSION CONTROL MATTING CLASS I TYPE URBAN A EXCEPT WHERE OTHERWISE SHOWN ON EROSION CONTROL PLANS.

LEGEND

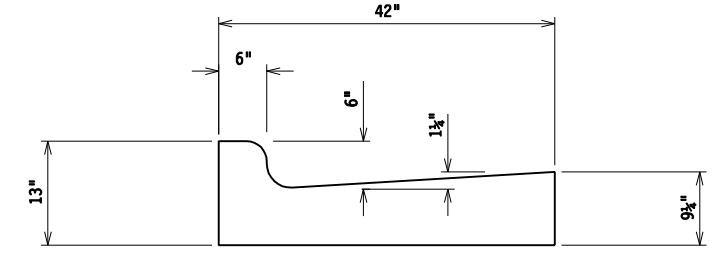
- ① 3 IN. HMA PAVEMENT (NOTE 1)
- ② 6-INCH CRUSHED AGGREGATE BASE (GRAD NO. 2)
- ③ 8-INCH CRUSHED AGGREGATE BASE (GRAD NO. 2)
- ④ 4-INCH CRUSHED AGGREGATE BASE (GRAD NO. 3)
- ⑤ 4 INCH TOPSOIL
- ⑥ TERRACE SEEDING AND MATTING (NOTE 2)
- ⑦ 42 INCH CURB AND GUTTER, SPECIAL
- ⑧ 2 INCH LIMESTONE SCREENINGS (2/8" MINUS)
- ⑨ 3 INCH EXISTING ASPHALT PAVEMENT TO REMAIN
- ⑩ EXISTING AGGREGATE BASE TO REMAIN
- ⑪ 2" OVERLAY
- ⑫ 2-INCH (MIN.) TOPSOIL, SEED, MAT
- ⑬ 4-INCH MIN. TOPSOIL, SEED, MAT



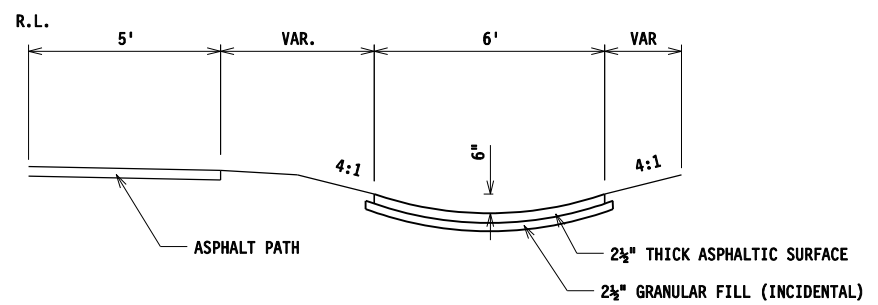
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PLAN
ASPHALT FLUME DETAIL



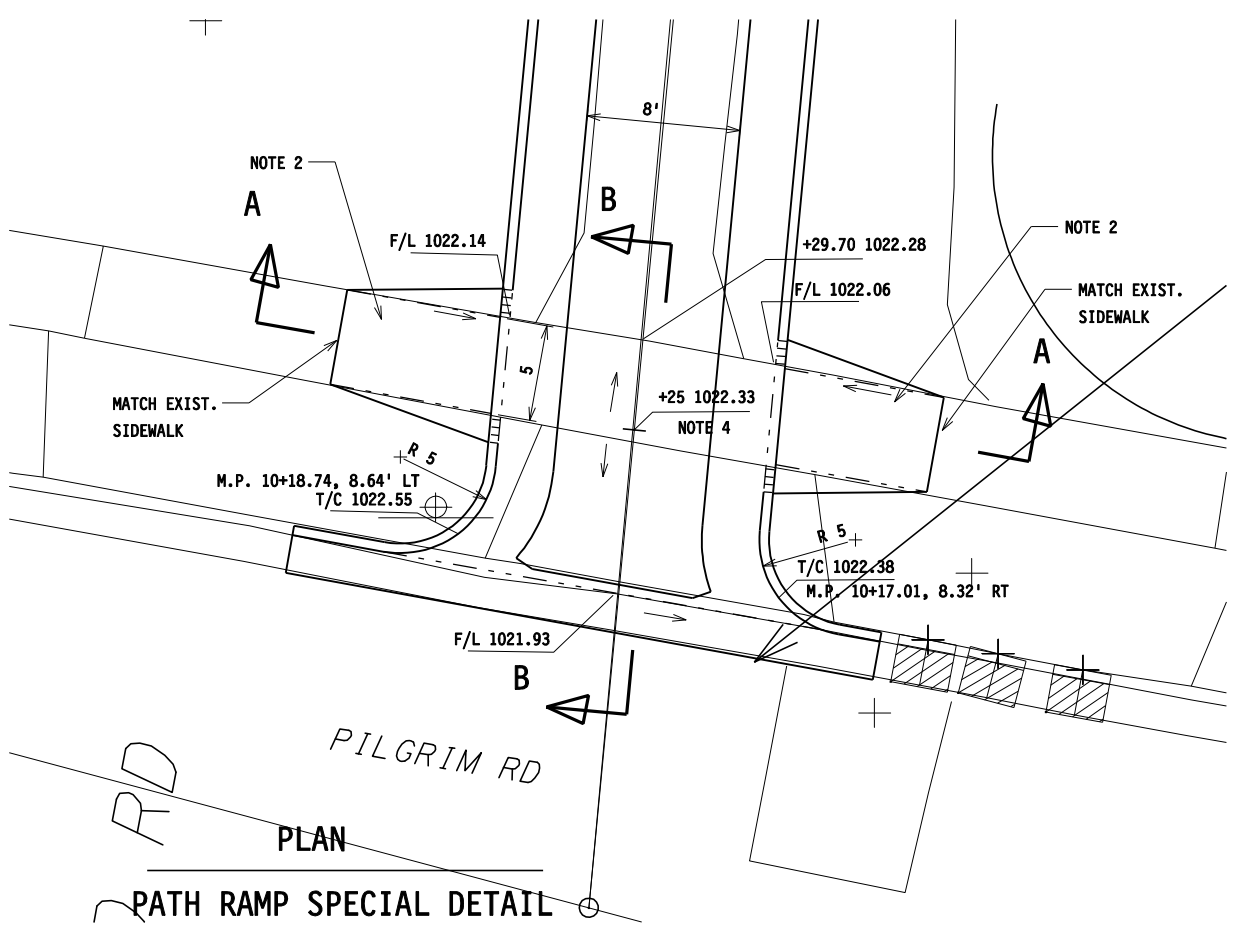
42-INCH CONCRETE CURB & GUTTER, SPECIAL



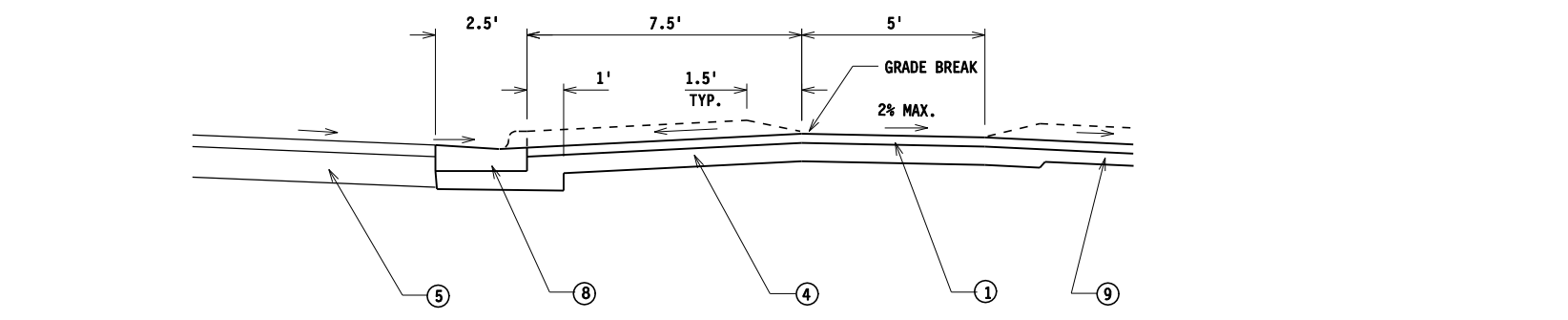
SECTION C-C

- LEGEND**
- ① 3 INCH HMA PAVEMENT (SEE TYPICAL SECTIONS)
 - ② 7 INCH CONCRETE SIDEWALK (NOTE 2)
 - ③ 2-INCH GRANULAR FILL (INCIDENTAL)
 - ④ 8 IN. CRUSHED AGGREGATE BASE COURSE GRADATION NO. 2
 - ⑤ EXISTING ASPHALT PAVEMENT
 - ⑥ EXISTING CONCRETE SIDEWALK
 - ⑦ 42" CONCRETE CURB & GUTTER, SPECIAL
 - ⑧ 30" CONCRETE CURB & GUTTER (DRIVEWAY SECTION)
 - ⑨ 6" CRUSHED AGGREGATE BASE COURSE

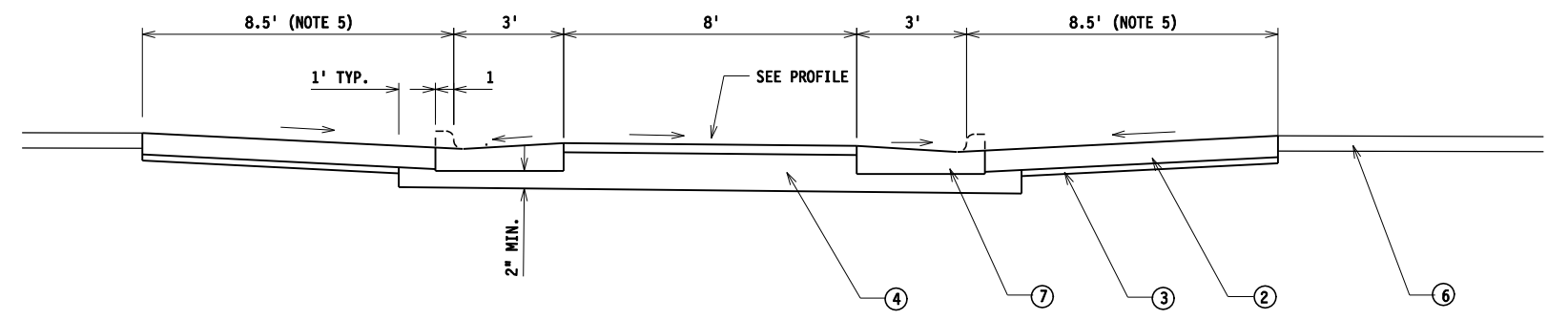
- NOTES**
1. TRANSITION CURB SHAPE TO APPROXIMATE SHAPE OF FLUME. PAID FOR AS 42-INCH CONCRETE CURB & GUTTER, SPECIAL
 2. RAMPS PAID FOR AS 7-INCH CONCRETE SIDEWALK & DRIVE. DETECTABLE WARNING FIELDS NOT REQUIRED
 3. PAID FOR AS ASPHALT FLUME. SEE SECTION.
 4. CONTRACTOR SHALL VERIFY EXISTING PILGRIM ROAD ELEVATIONS AND ADJUST RAMP GRADES TO PROVIDE HIGH POINT APPROX. 0.4' ABOVE GUTTER FLOW LINE.
 5. DIMENSION MAY BE ADJUSTED TO MATCH EXISTING SIDEWALK JOINTING PROVIDED RAMP SLOPE DOES NOT EXCEED 12:1



PLAN
PATH RAMP SPECIAL DETAIL

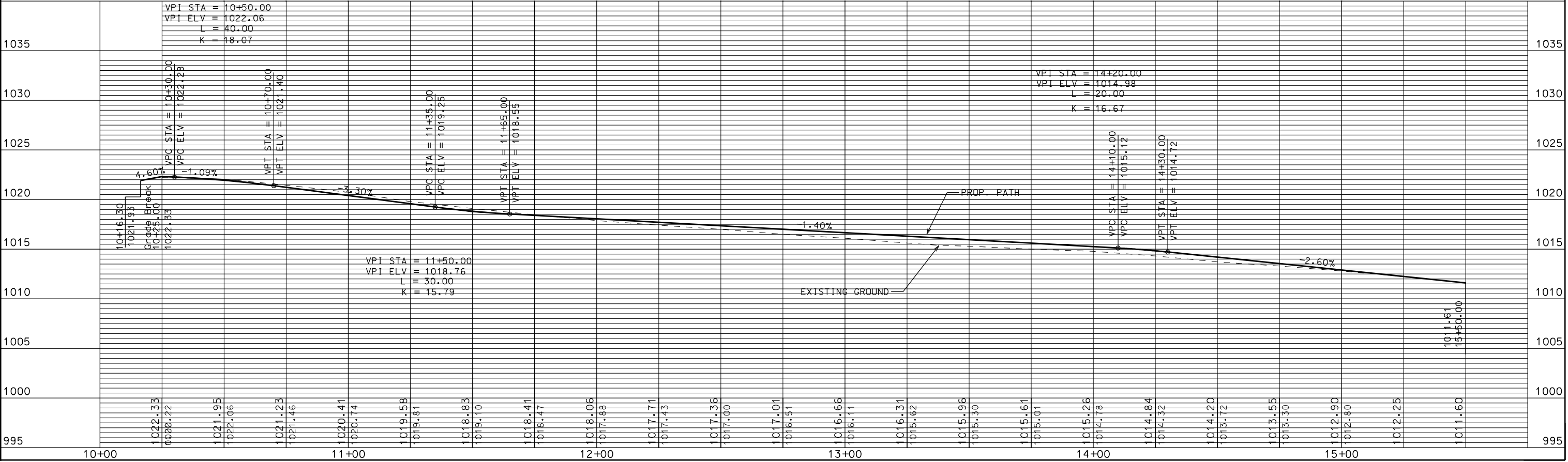
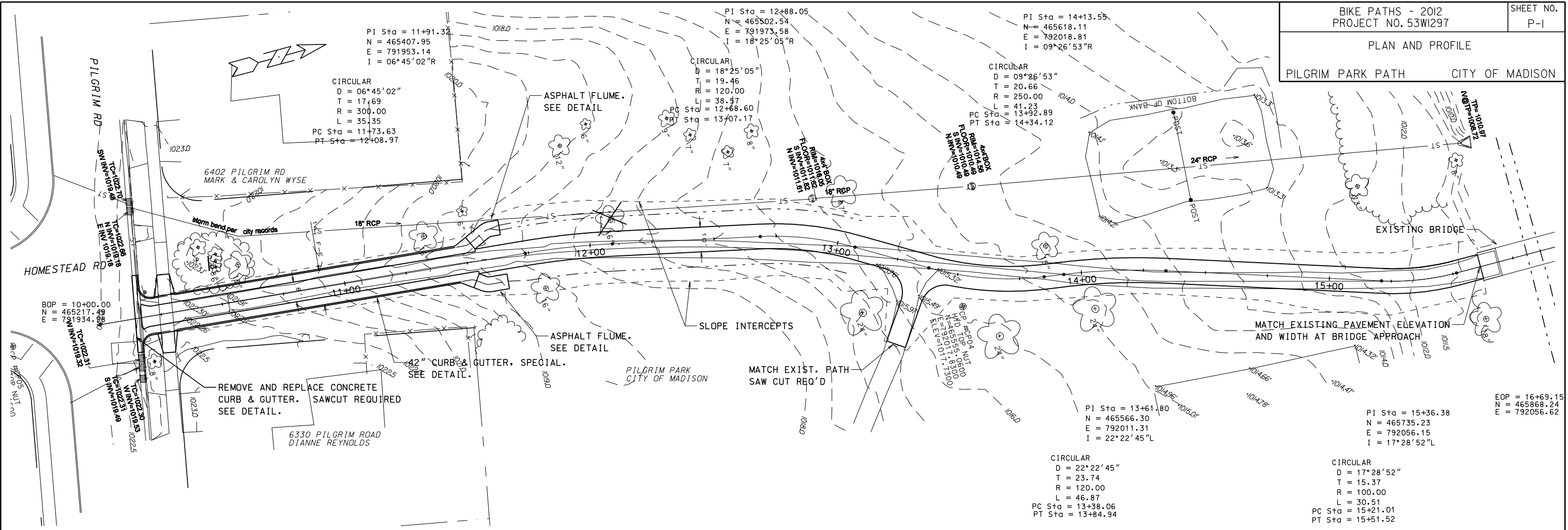


SECTION B-B



SECTION A-A
PATH RAMP SPECIAL DETAIL

LEVELS ON = 1,2, 4,5,6,7,8,9, 11,12,13,14, 16,17,18,19,20,21,22,23,24,25,26,27,28,29,30,31,32,33,34,35,36,37,38,39, 42,43,44,45,46,47,48,49, 51,52,53,54,55,56,57,58,59, 63

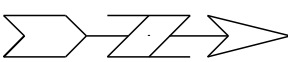


PLOT SCALE: 1" = 40'

PLOT NAME: 53W1297.PLT

REV. DATE: 7/3/2012

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



END MILL AND OVERLAY
(FIELD VERIFY LOCATION)

LOCATION WHERE EXISTING PATH IS NOT 2'

2' TYPICAL WIDTH
EXISTING PATH TO BE REMOVED

FINAL PATH SHALL BE 10'

OVERLAY WITH
2" OF NEW ASPHALT.
START ON WEST PAVEMENT EDGE
AND PAVE 10' WIDE PATH.

BEGIN OVERLAY
(FIELD VERIFY LOCATION)
CONSTRUCTION ACCESS FROM
ODAMA HILL GOLF COURSE

CONTRACTOR TO PROVIDE A BUTT JOINT/WEDGE CUT.
MATCH INTO EXISTING PAVEMENT.

ODANA HILL BIKE PATH

EXISTING PATH TO BE REMOVED
TYPICALLY 2' WIDE TO A MAXIMUM OF APPROX. 8' WIDE

CONTRACTOR TO PROVIDE A BUTT JOINT/WEDGE CUT.
MATCH INTO EXISTING PAVEMENT.

SAWCUT THE ENTIRE LENGTH
OF THE PATH ON THE EASTERLY
EDGE OF THE OVERLAY LIMITS.

NOTE: IN THE AREA OF THE WOODS THE PATH IS AN OLD SEVICE ROAD.
OVERLAID PATH SHALL REMAIN. THE AREA OUTSIDE OF OVERLAY
SHALL BE REMOVED PER TYPICAL SECTION.

FORMER RR ROW

SOUTHWEST COMMUTER BIKE PATH

CABOT LANE

CABOT LANE

60'

ADDITIONAL
THIRD ADDITION

VALE CIR

SCALE 1" = 40'

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLAN

SOUTHWEST PATH

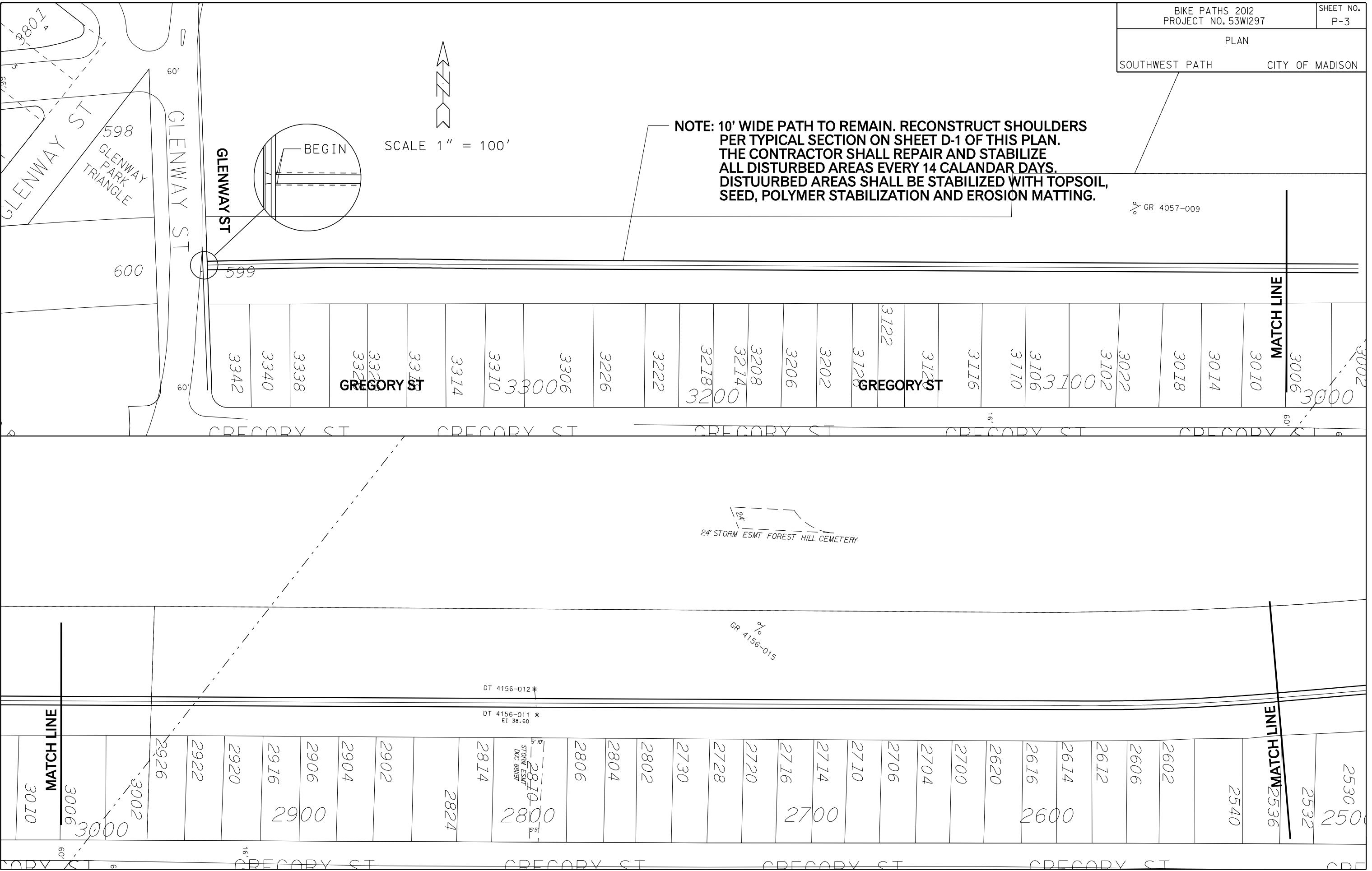
CITY OF MADISON



SCALE 1" = 100'

NOTE: 10' WIDE PATH TO REMAIN. RECONSTRUCT SHOULDERS PER TYPICAL SECTION ON SHEET D-1 OF THIS PLAN. THE CONTRACTOR SHALL REPAIR AND STABILIZE ALL DISTURBED AREAS EVERY 14 CALANDAR DAYS. DISTUURBED AREAS SHALL BE STABILIZED WITH TOPSOIL, SEED, POLYMER STABILIZATION AND EROSION MATTING.

GR 4057-009



GR 4156-015

DT 4156-012 *

DT 4156-011 *
EI 38.60

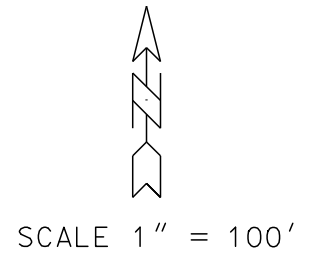


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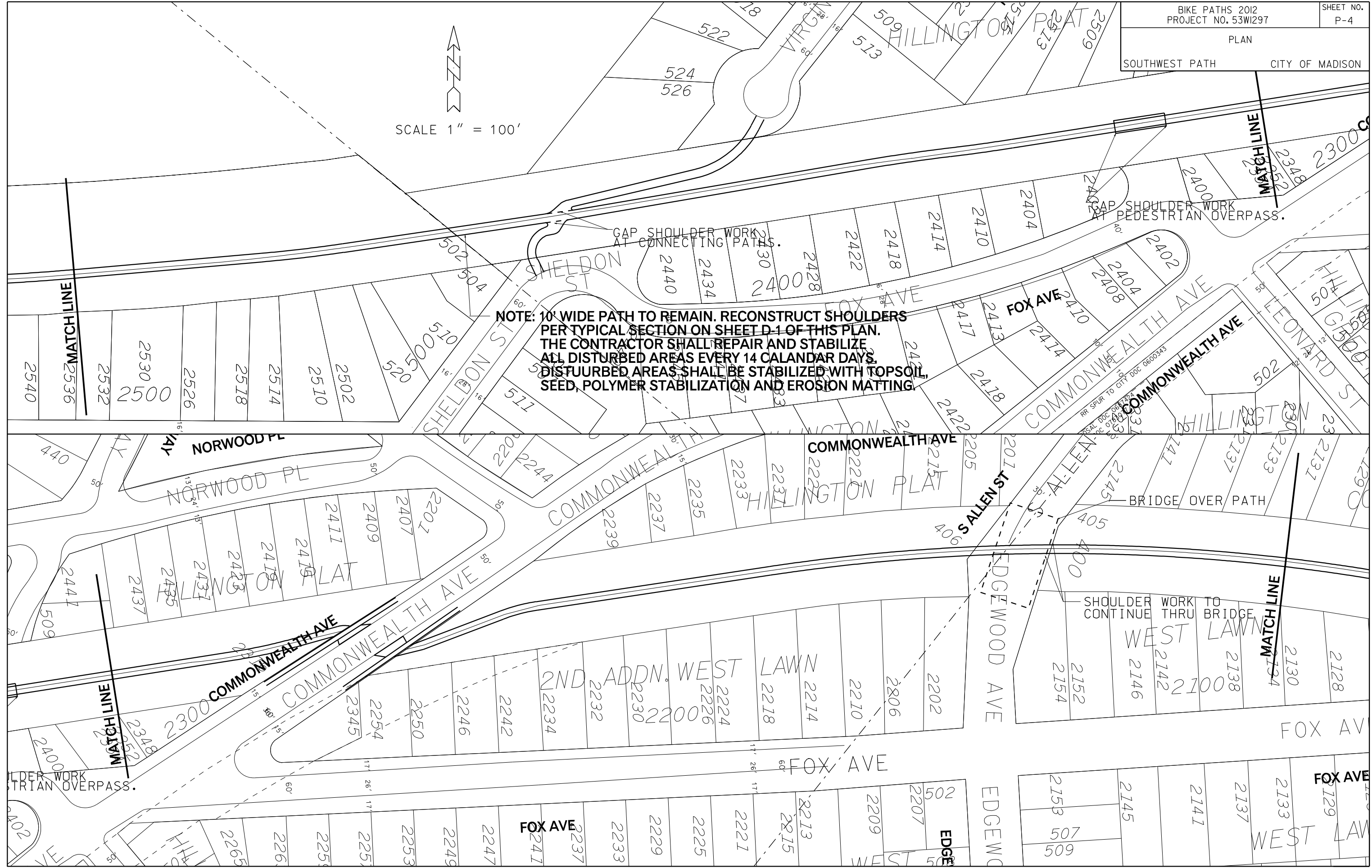
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



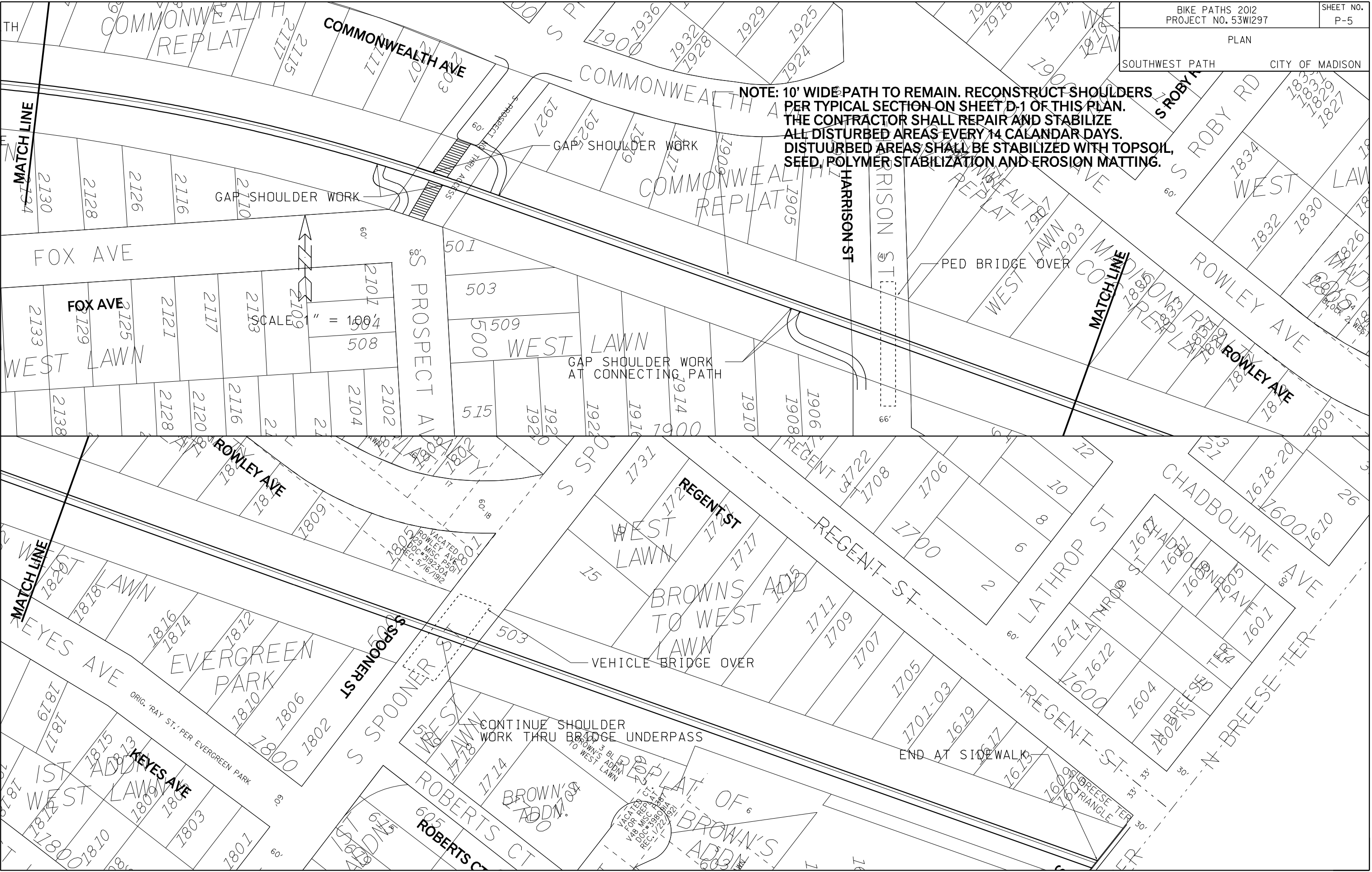
NOTE: 10' WIDE PATH TO REMAIN. RECONSTRUCT SHOULDERS PER TYPICAL SECTION ON SHEET D-1 OF THIS PLAN. THE CONTRACTOR SHALL REPAIR AND STABILIZE ALL DISTURBED AREAS EVERY 14 CALENDAR DAYS. DISTURBED AREAS SHALL BE STABILIZED WITH TOPSOIL, SEED, POLYMER STABILIZATION AND EROSION MATTING.



PLOT SCALE:
PLOT NAME:
REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

NOTE: 10' WIDE PATH TO REMAIN. RECONSTRUCT SHOULDERS PER TYPICAL SECTION ON SHEET D-1 OF THIS PLAN. THE CONTRACTOR SHALL REPAIR AND STABILIZE ALL DISTURBED AREAS EVERY 14 CALANDAR DAYS. DISTURBED AREAS SHALL BE STABILIZED WITH TOPSOIL, SEED, POLYMER STABILIZATION AND EROSION MATTING.

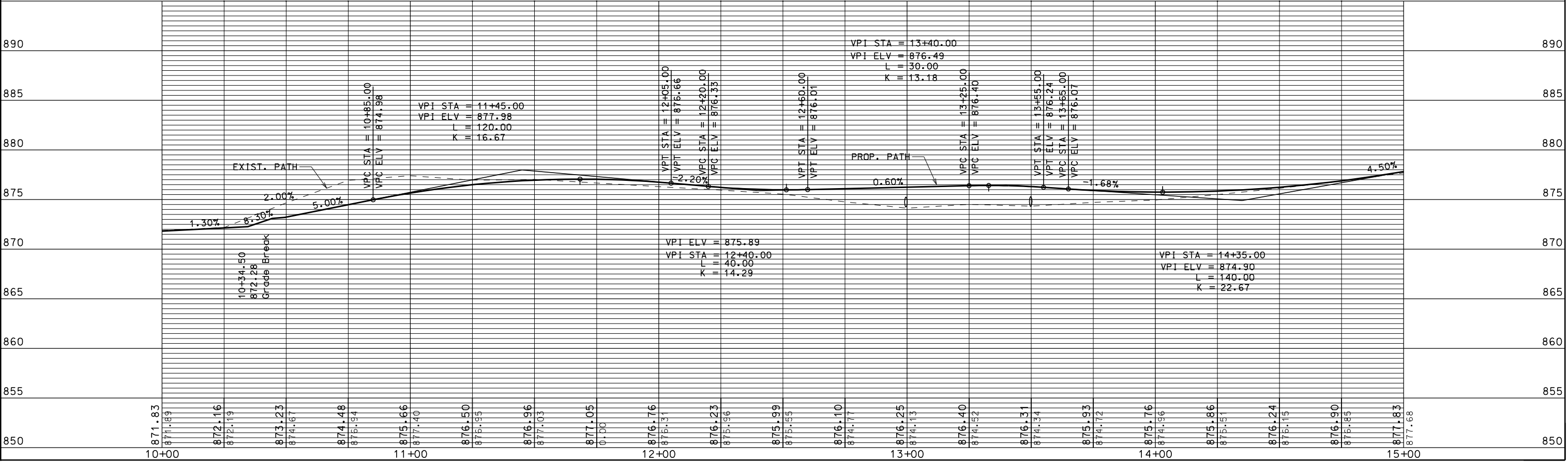
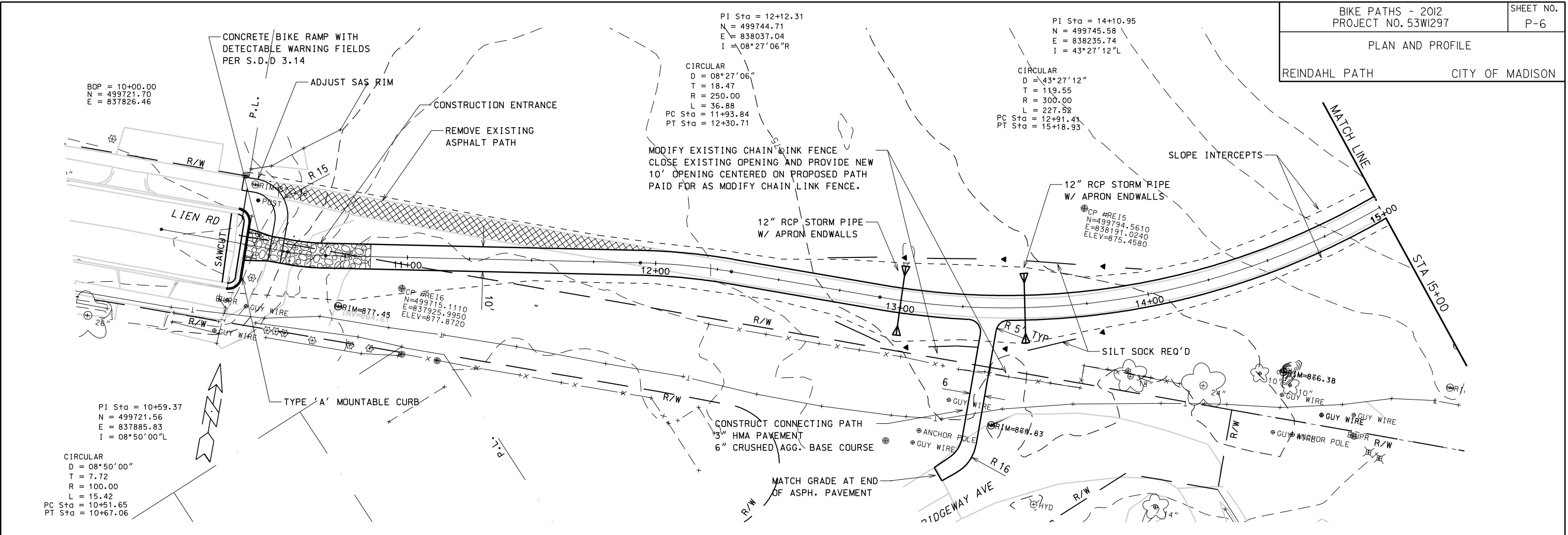


PLOT SCALE: 1" = 100'

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



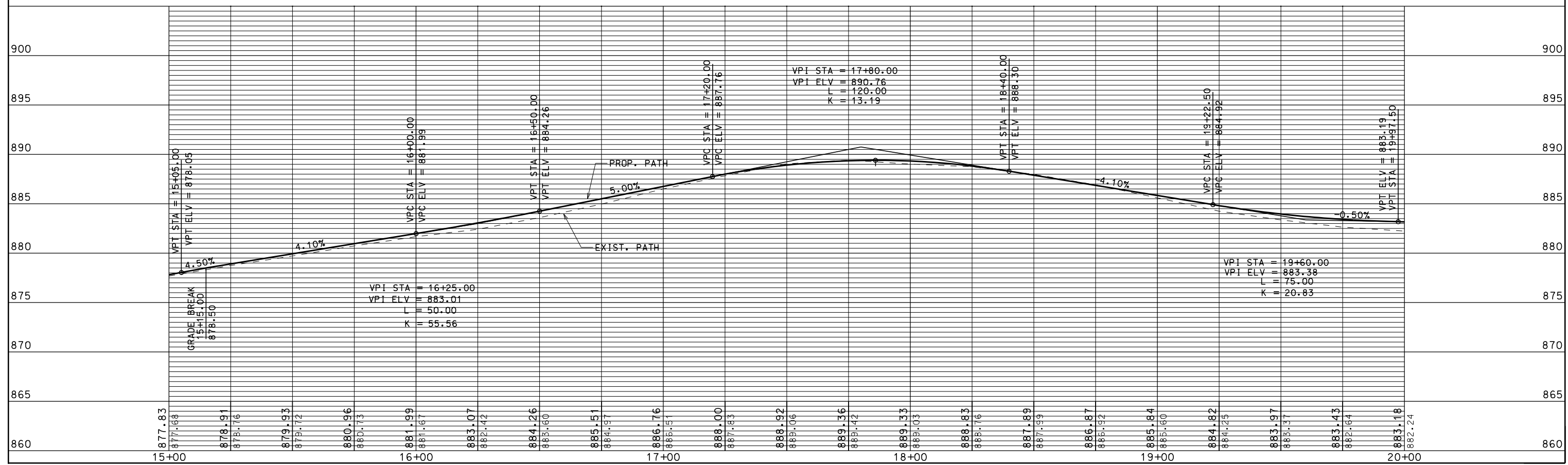
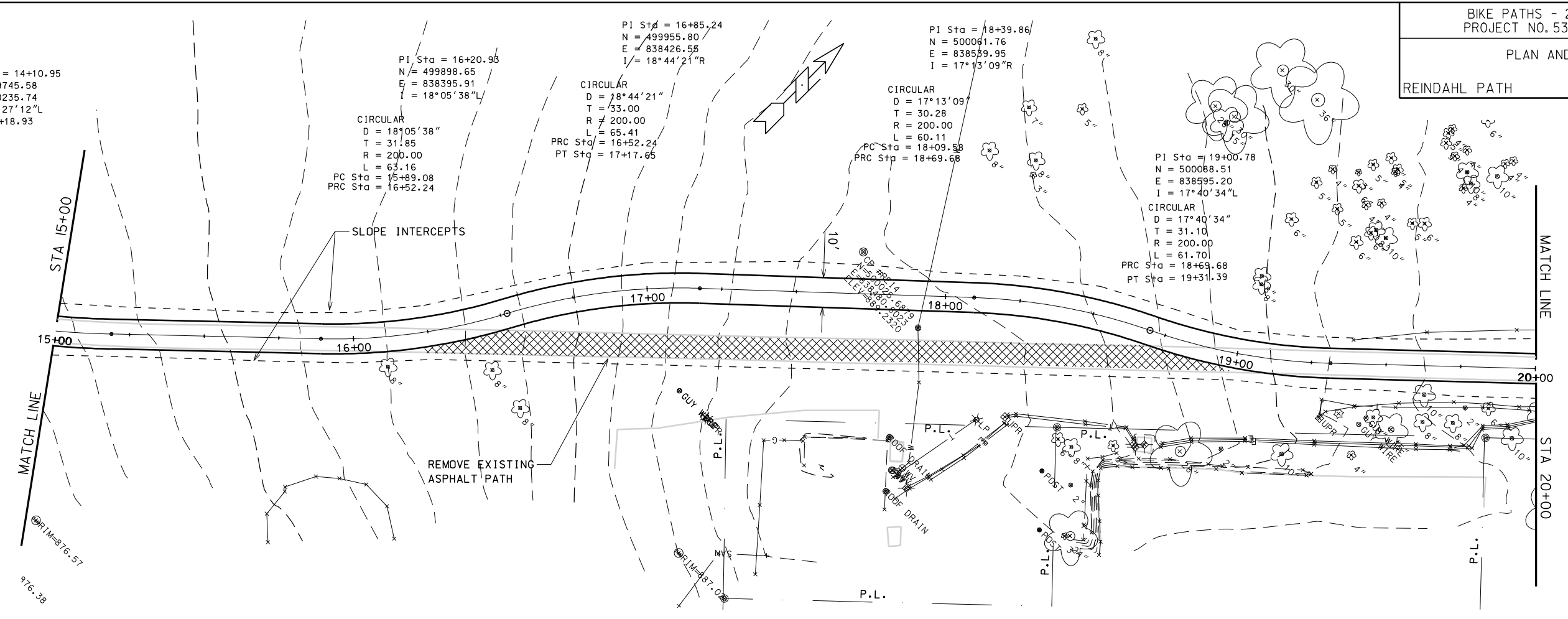
PI Sta = 14+10.95
N = 499745.58
E = 838235.74
I = 43°27'12"L
PT Sta = 15+18.93

PI Sta = 16+20.95
N = 499898.65
E = 838395.91
I = 18°05'38"L
CIRCULAR
D = 18105'38"
T = 31.85
R = 200.00
L = 63.16
PC Sta = 15+89.08
PT Sta = 16+52.24

PI Sta = 16+85.24
N = 499955.80
E = 838426.55
I = 18°44'21"R
CIRCULAR
D = 18°44'21"
T = 33.00
R = 200.00
L = 65.41
PC Sta = 16+52.24
PT Sta = 17+17.65

PI Sta = 18+39.86
N = 500061.76
E = 838559.95
I = 17°13'09"R
CIRCULAR
D = 17°13'09"
T = 30.28
R = 200.00
L = 60.11
PC Sta = 18+09.58
PT Sta = 18+69.68

PI Sta = 19+00.78
N = 500088.51
E = 838995.20
I = 17°40'34"L
CIRCULAR
D = 17°40'34"
T = 31.10
R = 200.00
L = 61.70
PC Sta = 18+69.68
PT Sta = 19+31.39

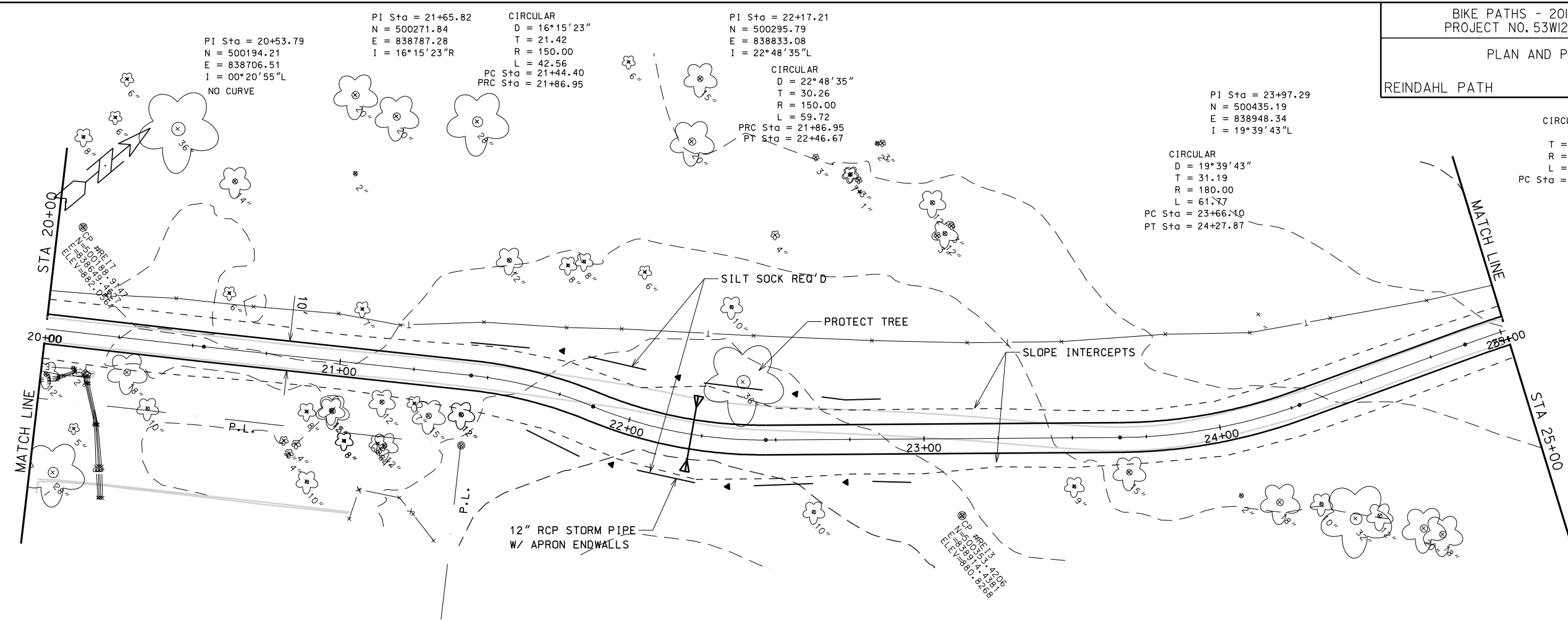


PLOT SCALE: 1"=40'

PLOT NAME: REINDAHL PATH

REV. DATE: 7/2/2012

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PI Sta = 20+53.79
N = 500194.21
E = 838706.51
I = 00°20'55"L
NO CURVE

PI Sta = 21+65.82
N = 500271.84
E = 838787.28
I = 16°15'23"R

CIRCULAR
D = 16°15'23"
T = 21.42
R = 150.00
L = 42.56
PC Sta = 21+44.40
PRC Sta = 21+86.95

PI Sta = 22+17.21
N = 500295.79
E = 838833.08
I = 22°48'35"L

CIRCULAR
D = 22°48'35"
T = 30.26
R = 150.00
L = 59.72
PRC Sta = 21+86.95
PT Sta = 22+46.67

PI Sta = 23+97.29
N = 500435.19
E = 838948.34
I = 19°39'43"L

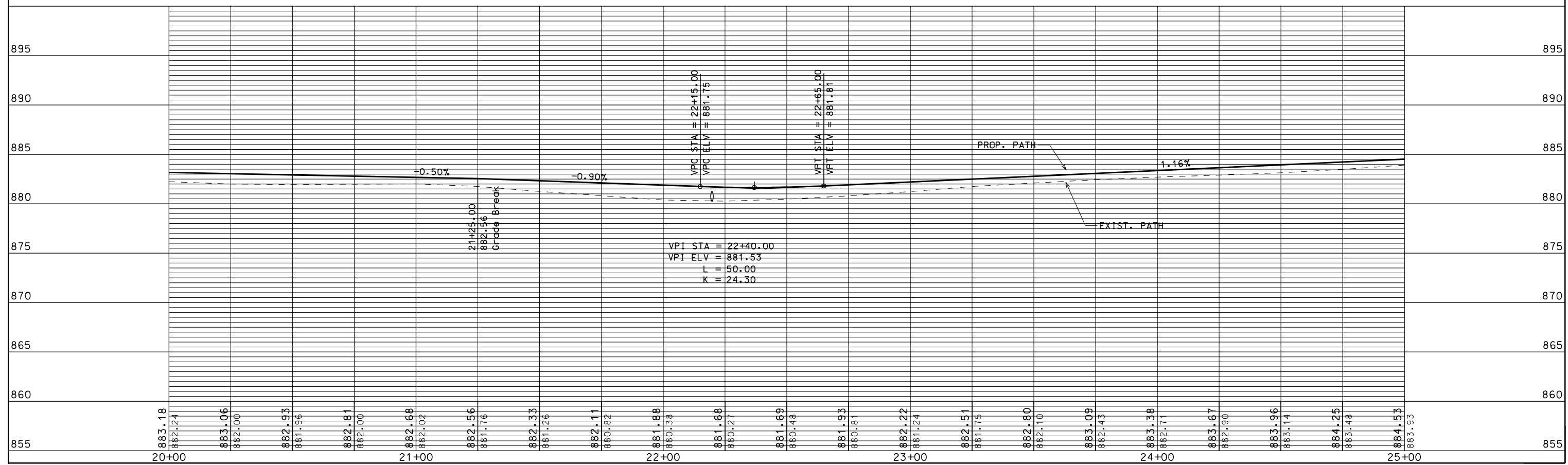
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D = 19°39'43"
T = 31.19
R = 180.00
L = 61.77
PC Sta = 23+66.40
PT Sta = 24+27.87

CIRCULAR
T = 72.65
R = 400.00
L = 143.74
PC Sta = 24+87.32

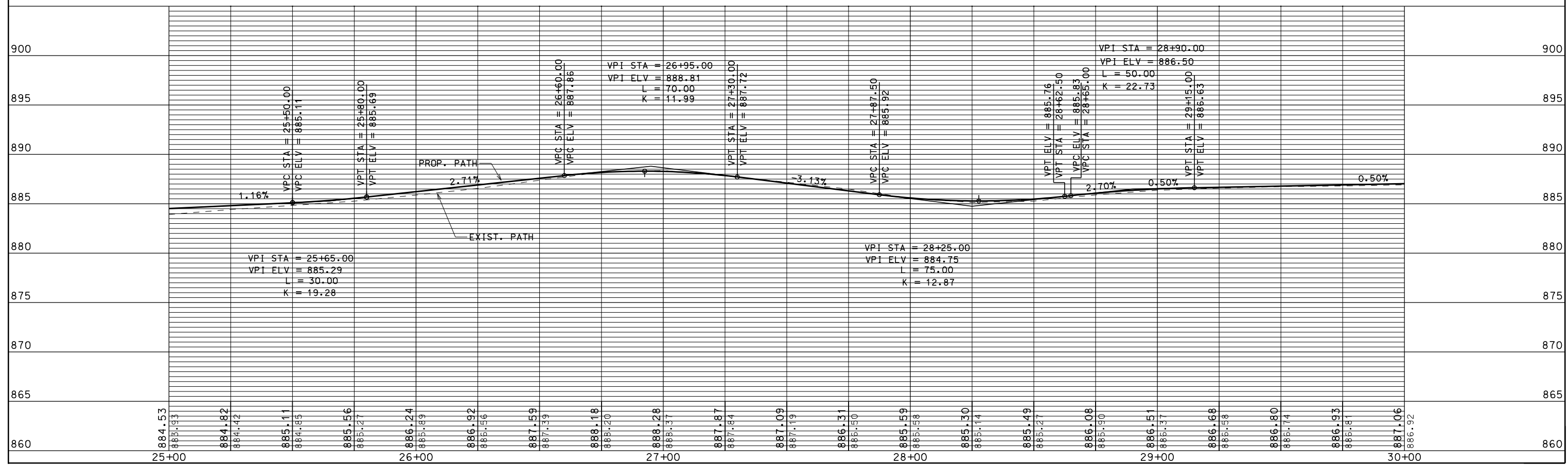
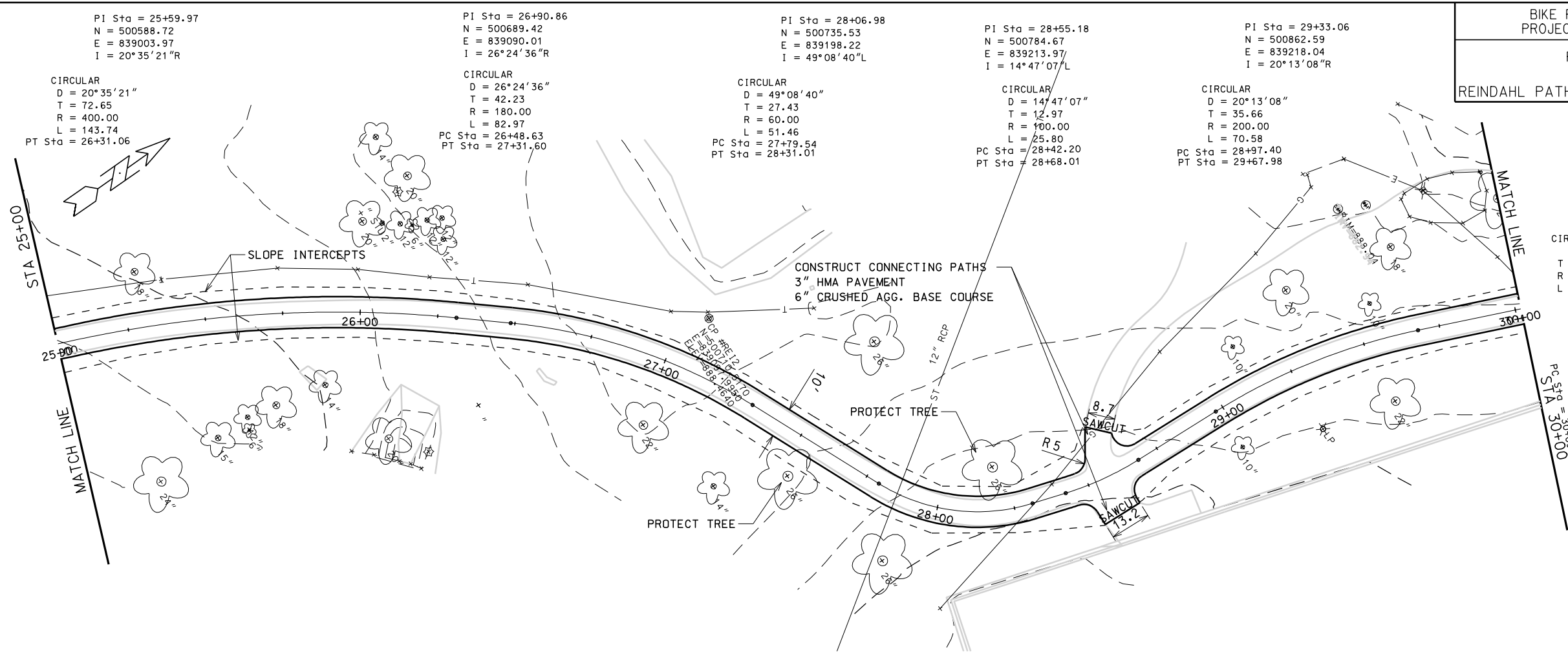
PLOT SCALE:

PLOT NAME:

REV. DATE:



ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PLOT SCALE:
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PI Sta = 30+47.09
N = 500968.07
E = 839263.27
I = 21°13'12"R

PI Sta = 31+10.36
N = 501013.87
E = 839308.16
I = 13°15'16"R

PI Sta = 32+49.48
N = 501088.30
E = 839425.82
I = 57°41'05"L

PI Sta = 33+96.70
N = 501244.98
E = 839425.82
I = 90°00'06"R

CIRCULAR
D = 21°13'12"
T = 37.46
R = 200.00
L = 74.07
PC Sta = 30+09.62
PT Sta = 30+83.69

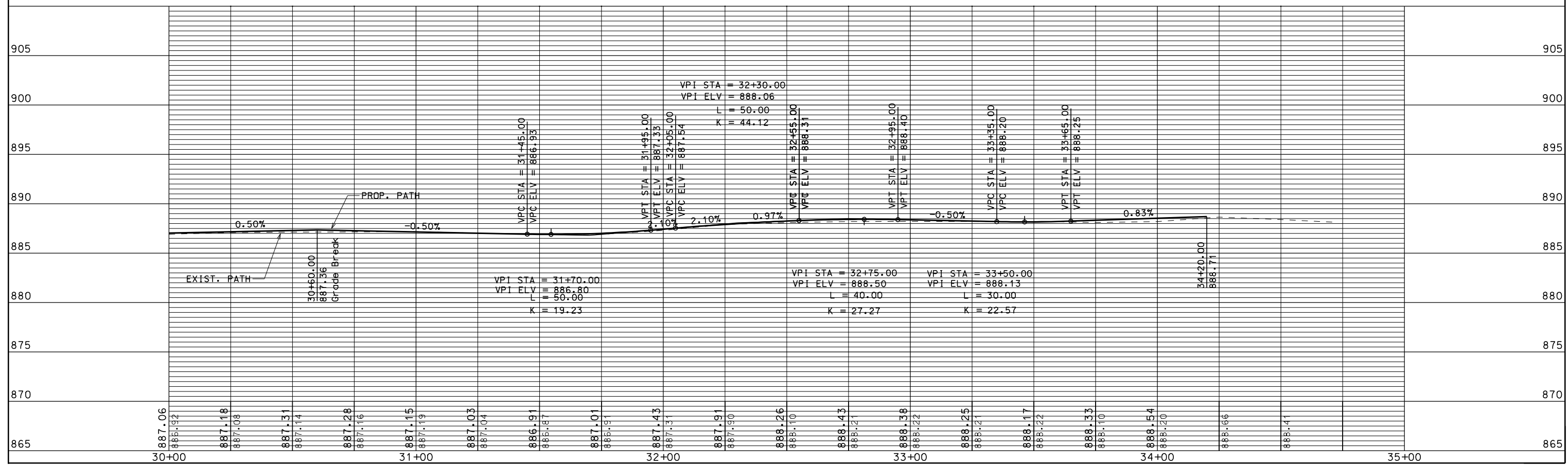
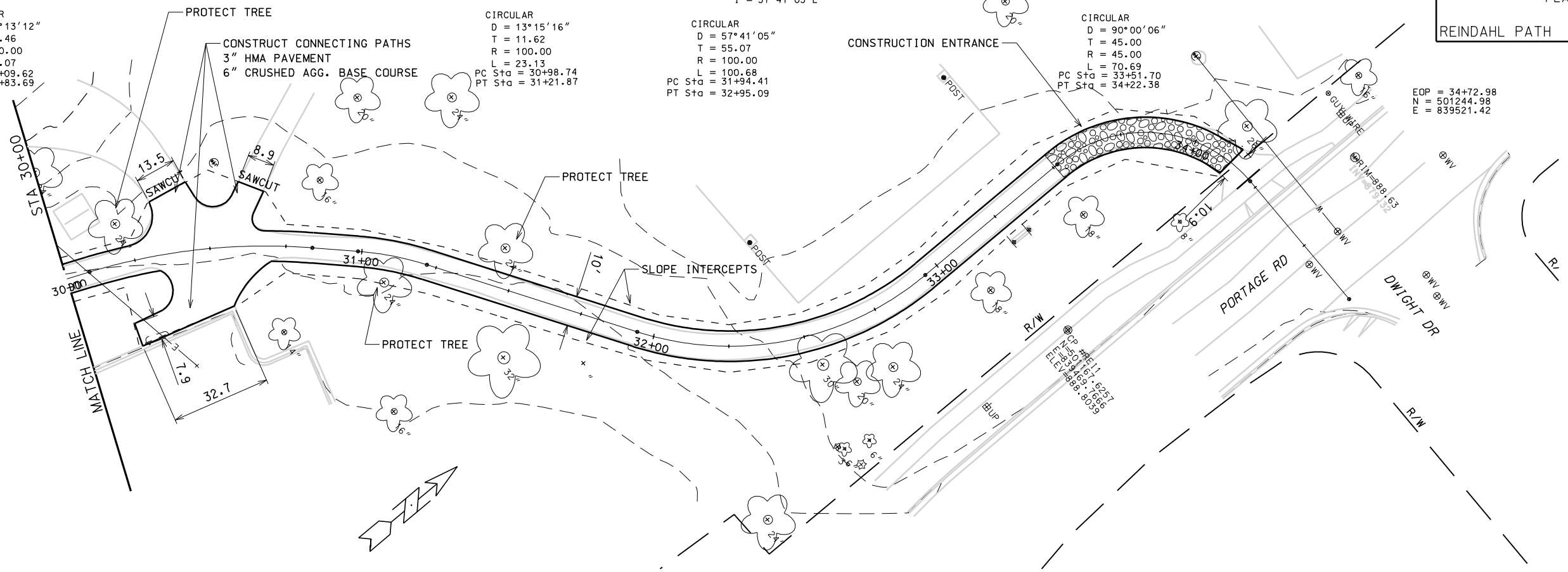
CIRCULAR
D = 13°15'16"
T = 11.62
R = 100.00
L = 23.13
PC Sta = 30+98.74
PT Sta = 31+21.87

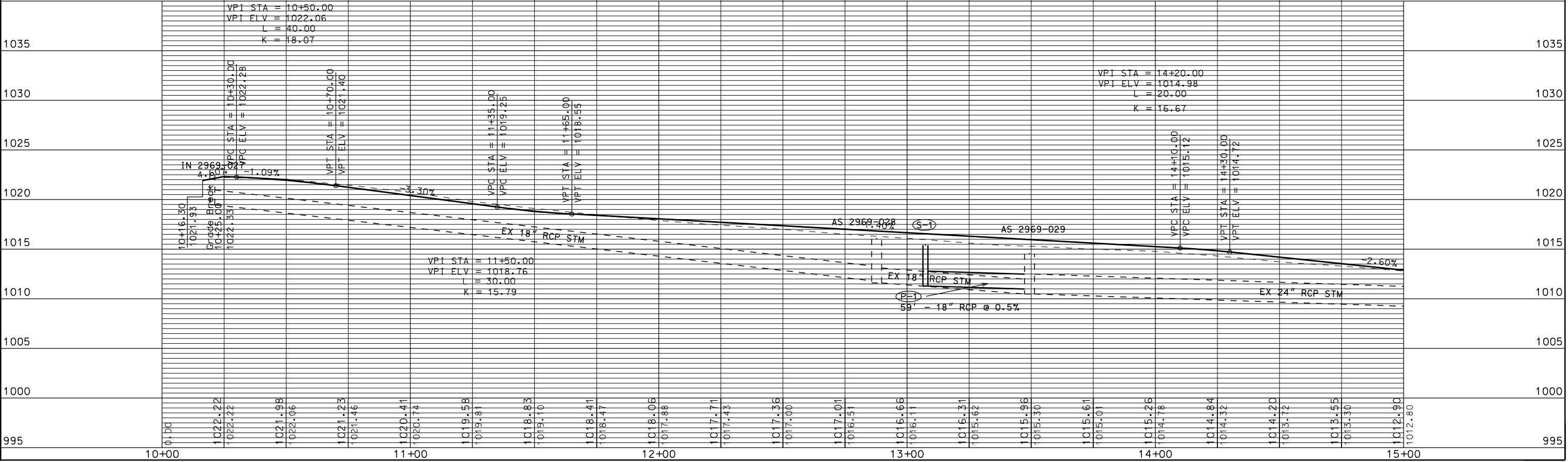
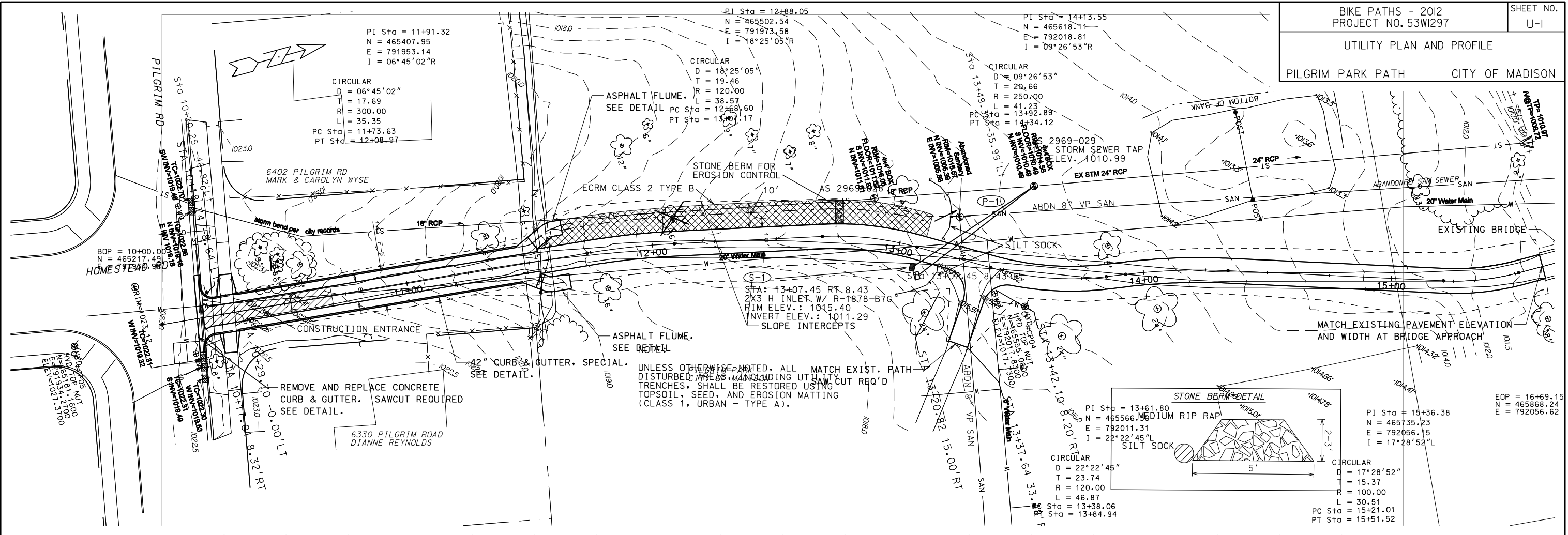
CIRCULAR
D = 57°41'05"
T = 55.07
R = 100.00
L = 100.68
PC Sta = 31+94.41
PT Sta = 32+95.09

CIRCULAR
D = 90°00'06"
T = 45.00
R = 45.00
L = 70.69
PC Sta = 33+51.70
PT Sta = 34+22.38

EOP = 34+72.98
N = 501244.98
E = 839521.42

PLOT SCALE:
PLOT NAME:
REV. DATE:





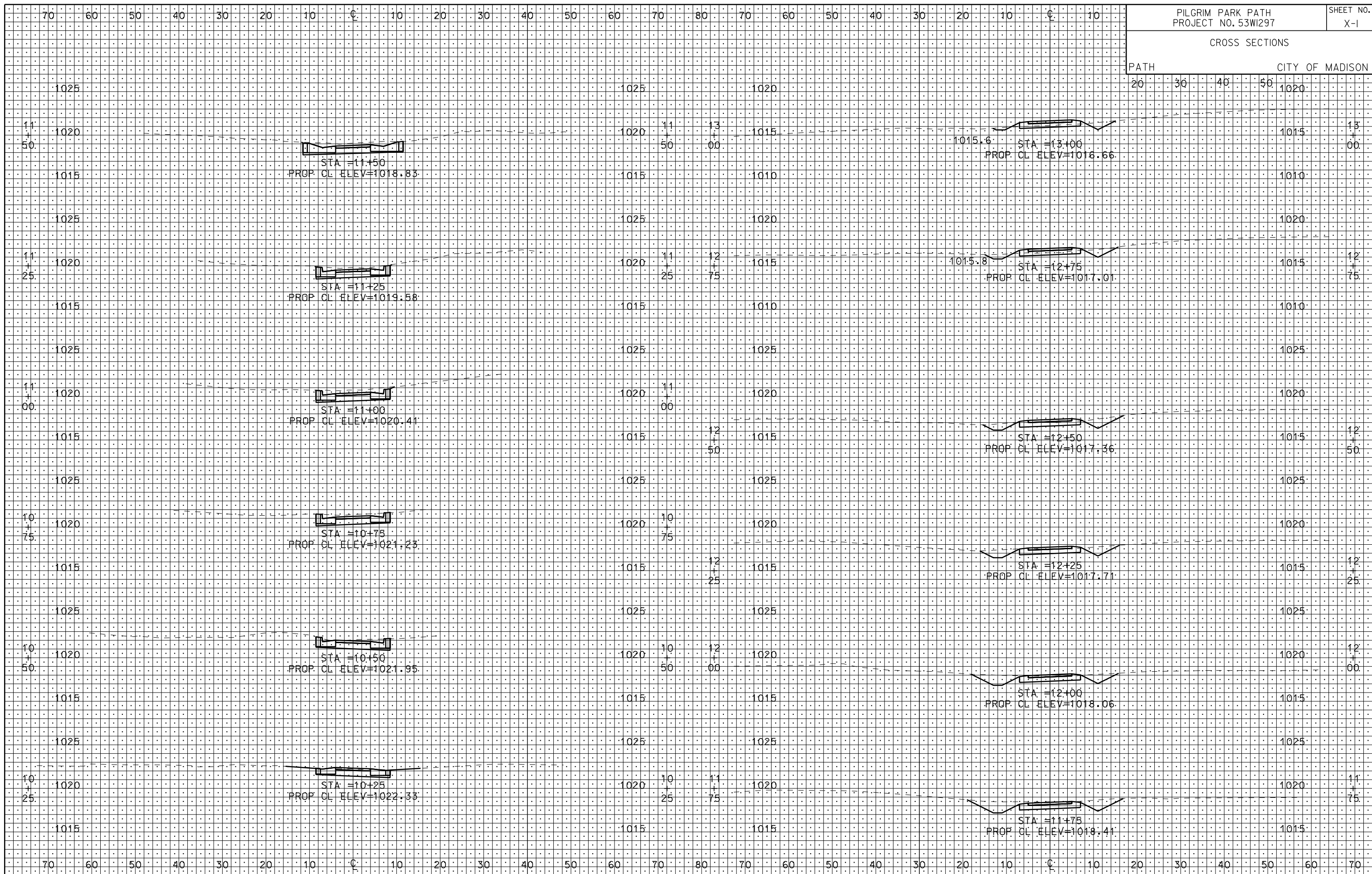
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLOT SCALE: 1" = 40' (PLAN), 1" = 10' (PROFILE)

REV. DATE:

CROSS SECTIONS

PATH CITY OF MADISON



PLOT SCALE:

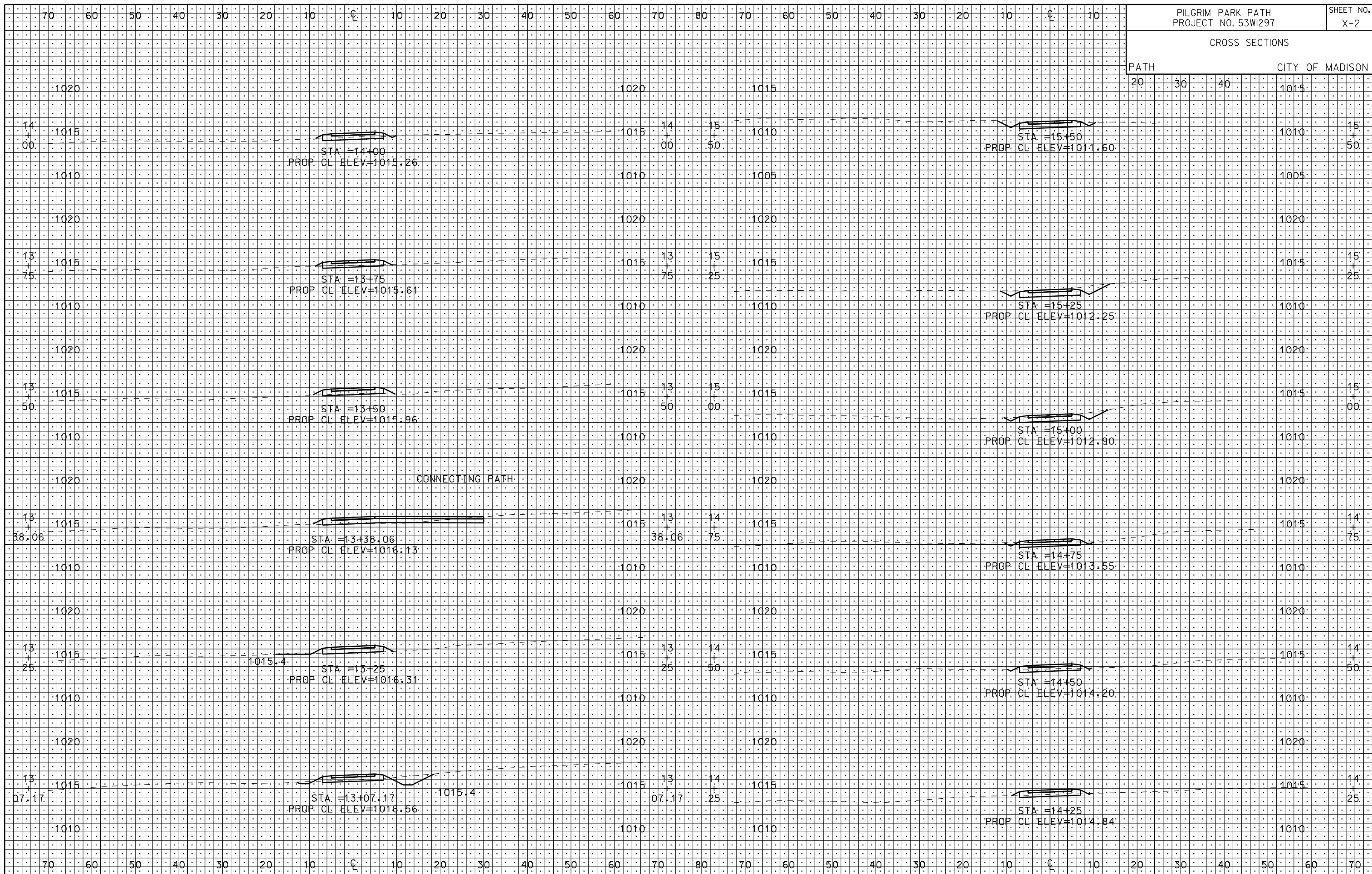
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CROSS SECTIONS

PATH CITY OF MADISON



PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

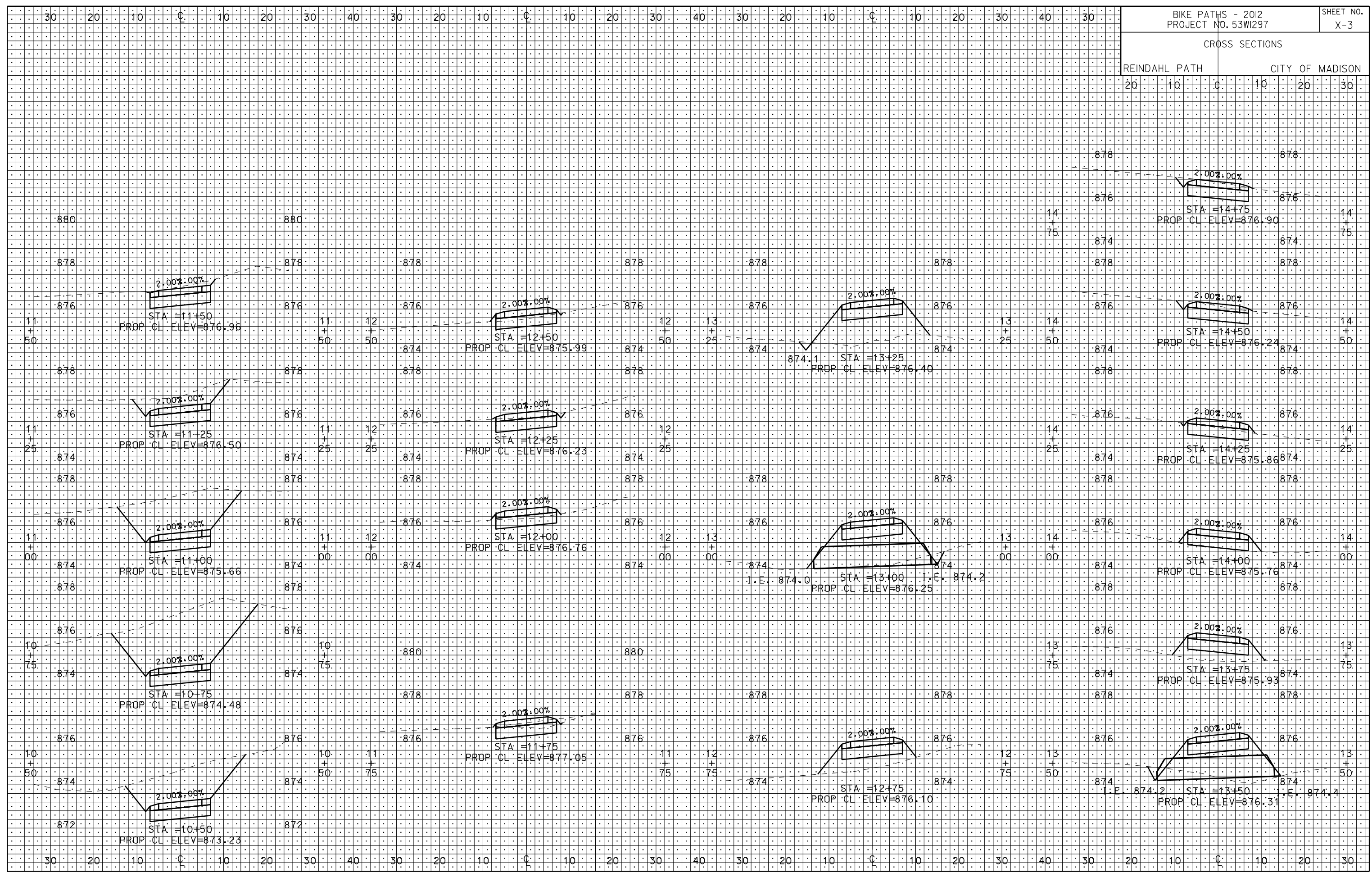
CROSS SECTIONS

REINDAHL PATH CITY OF MADISON

PLOT SCALE:

PLOT NAME:

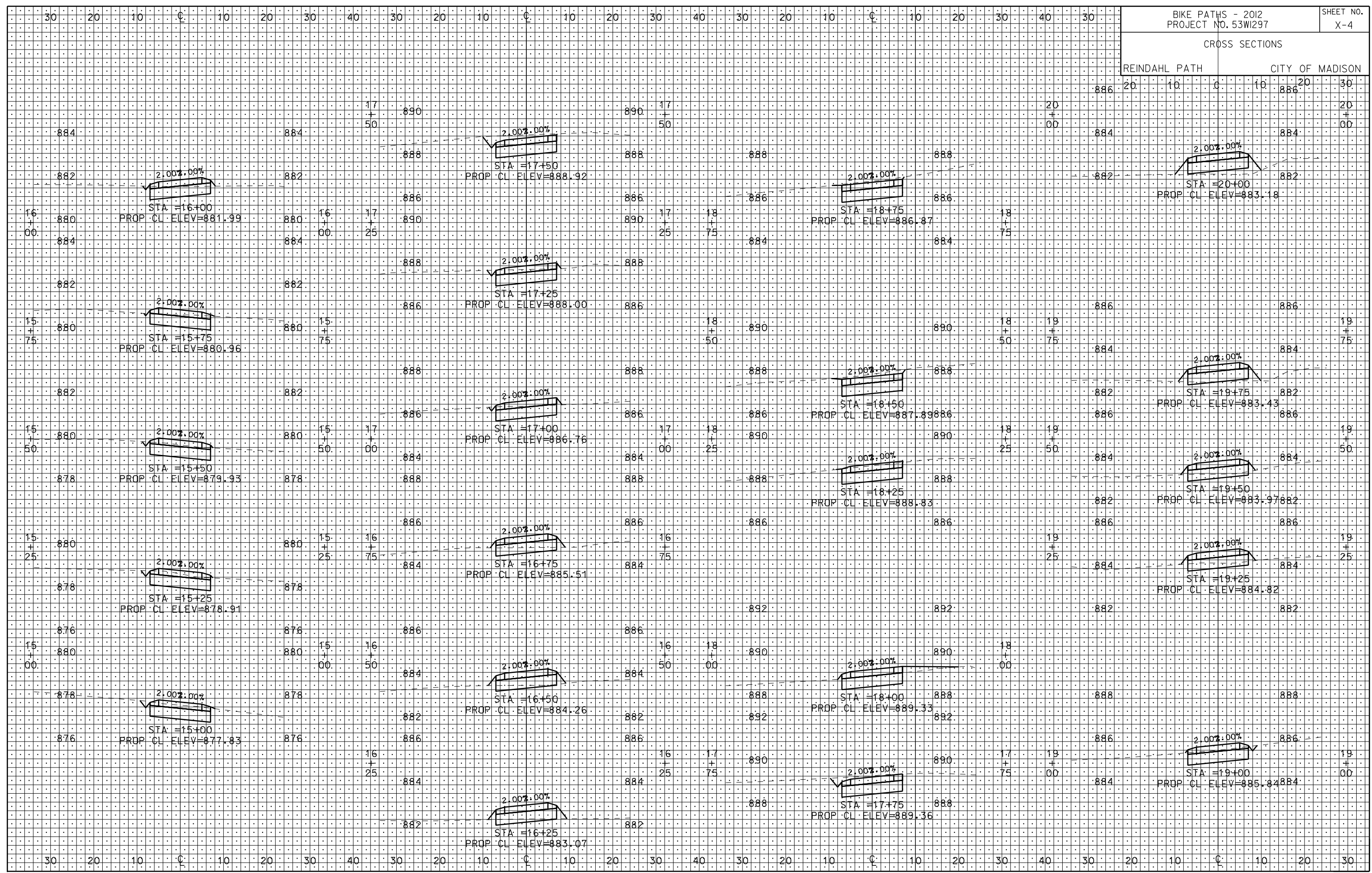
REV. DATE:



ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CROSS SECTIONS

REINDAHL PATH CITY OF MADISON



PLOT SCALE:

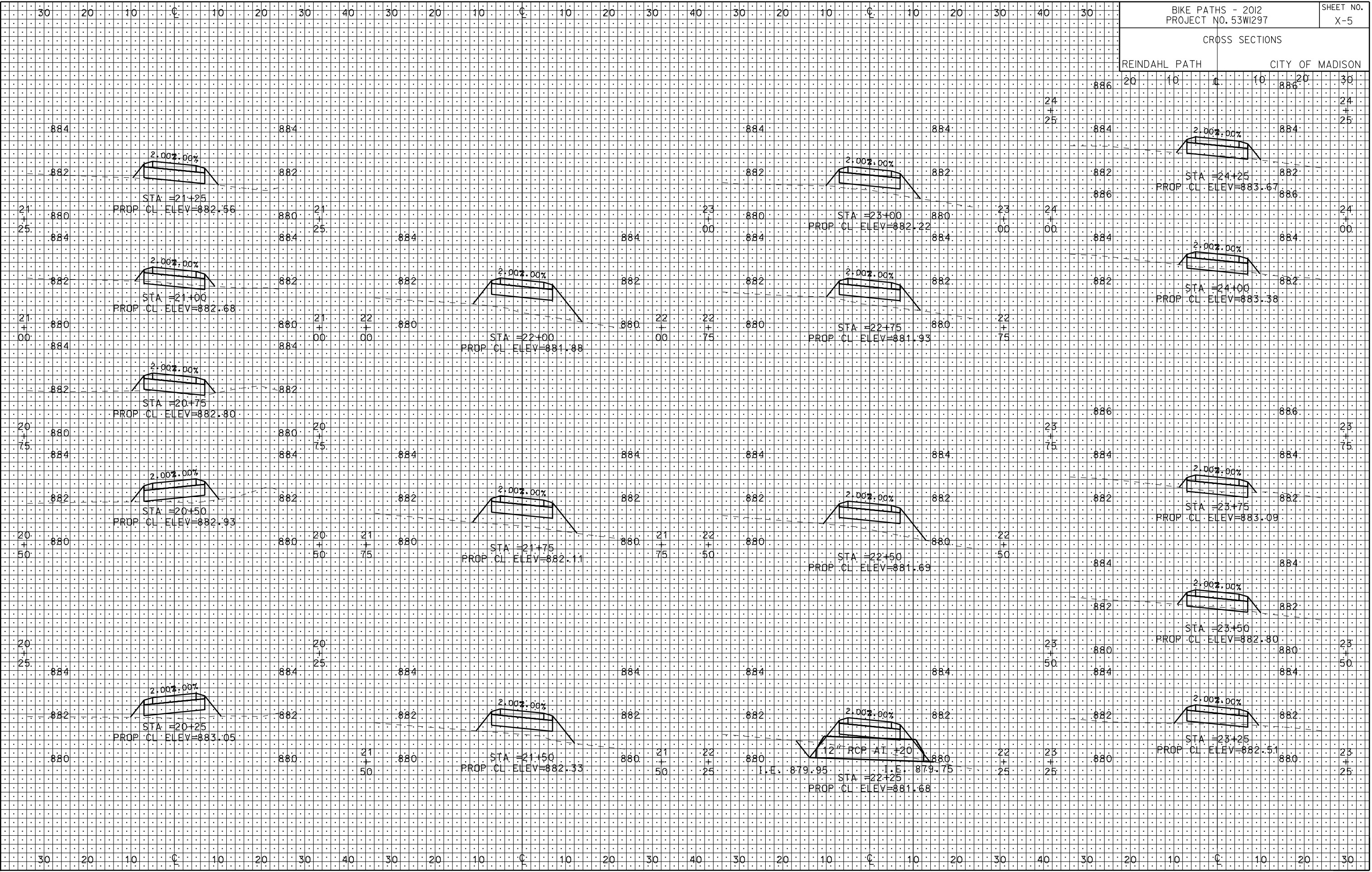
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CROSS SECTIONS

REINDAHL PATH CITY OF MADISON



PLOT SCALE:

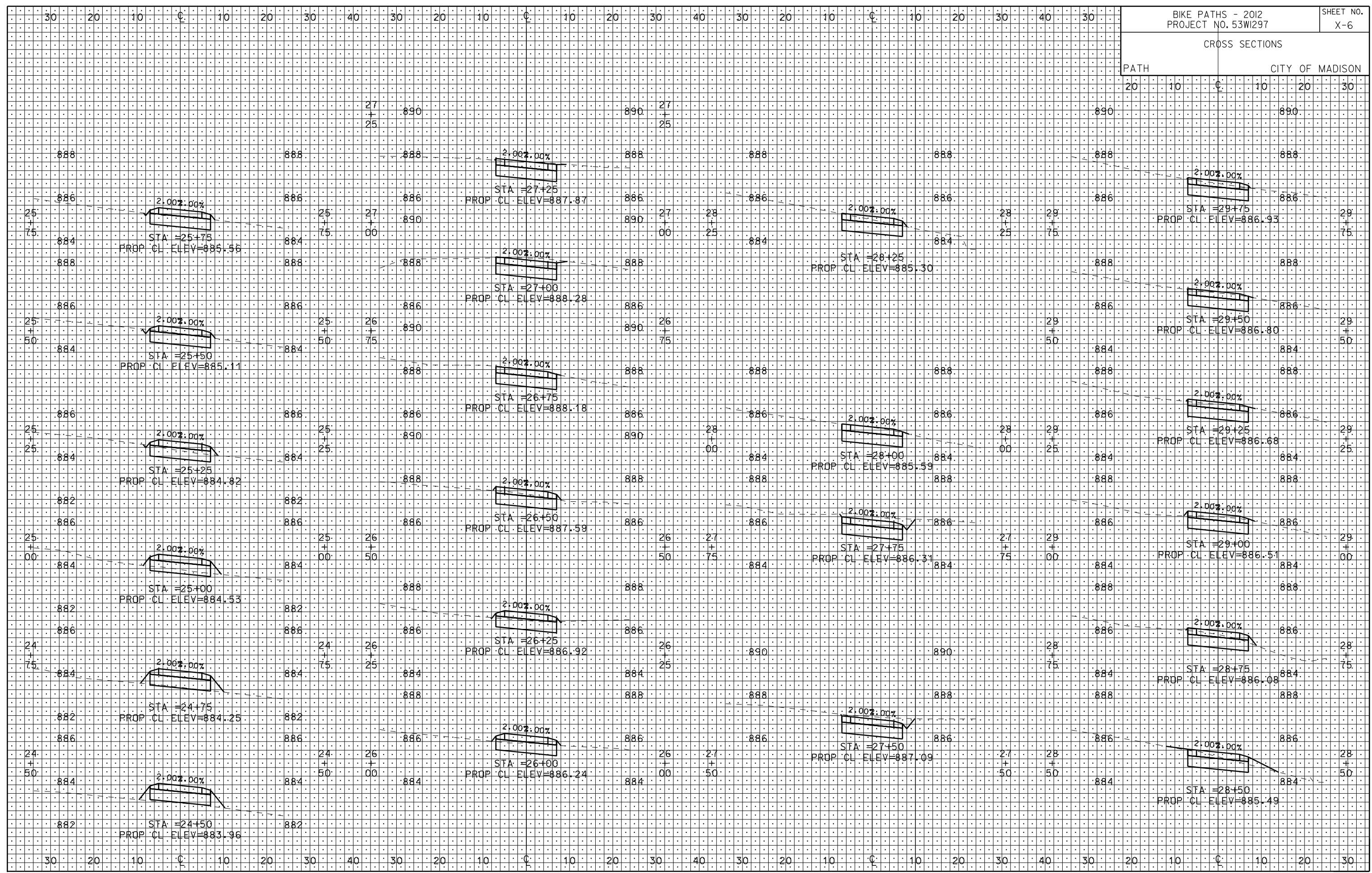
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CROSS SECTIONS

PATH CITY OF MADISON



PLOT SCALE:

PLOT NAME:

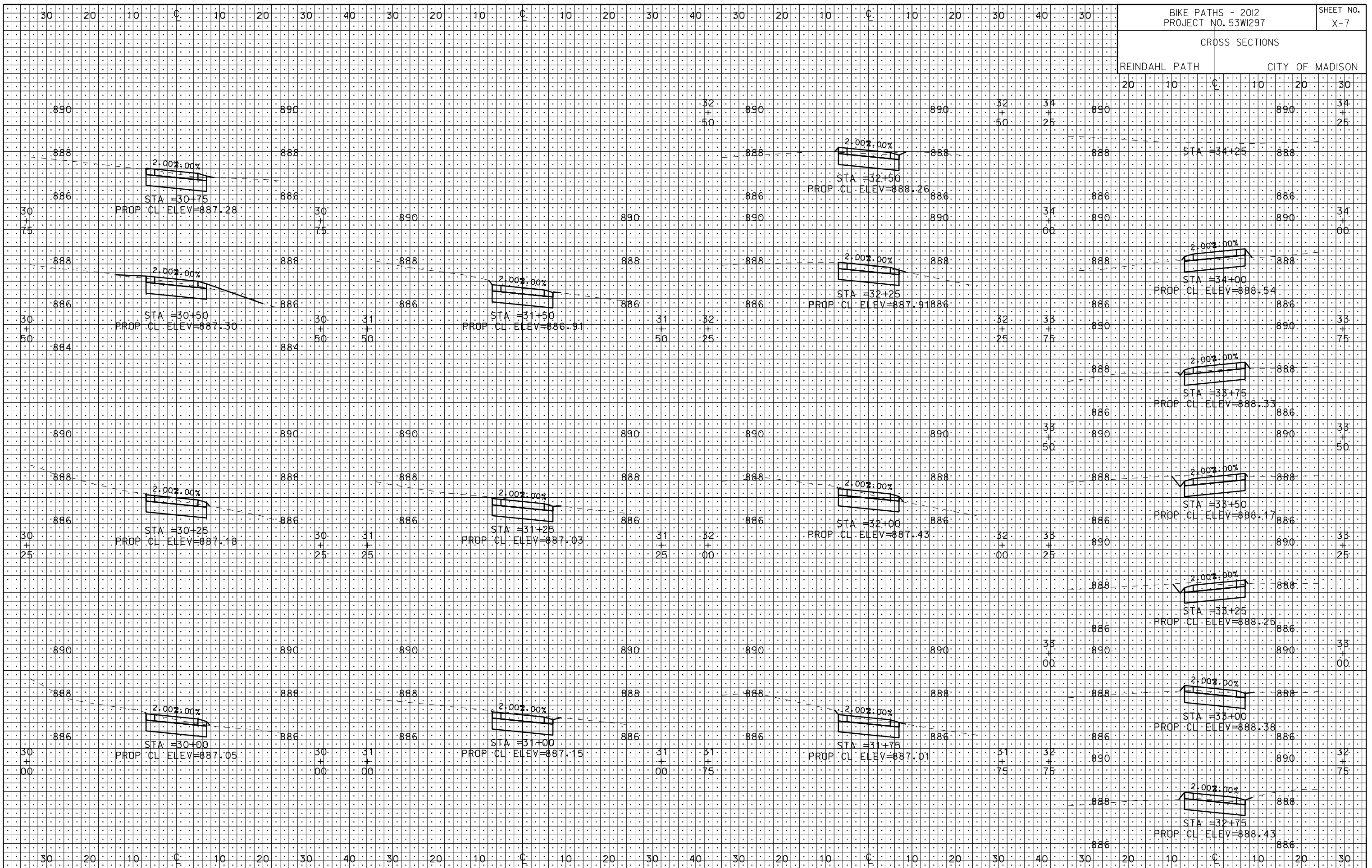
REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CROSS SECTIONS

REINDAHL PATH

CITY OF MADISON



PLOT SCALE:

PLOT NAME:

REV. DATE: