

S. HANCOCK STREET

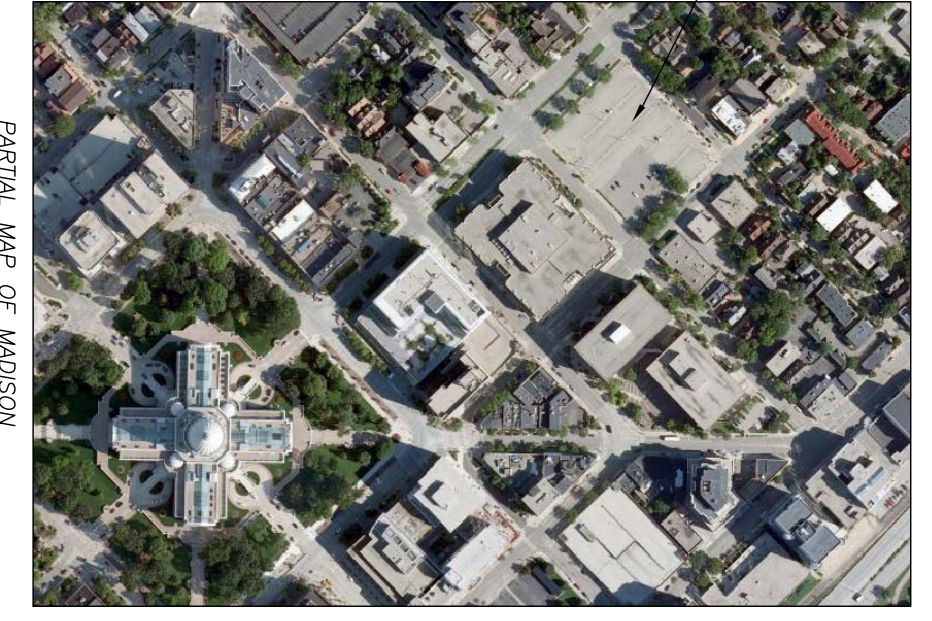
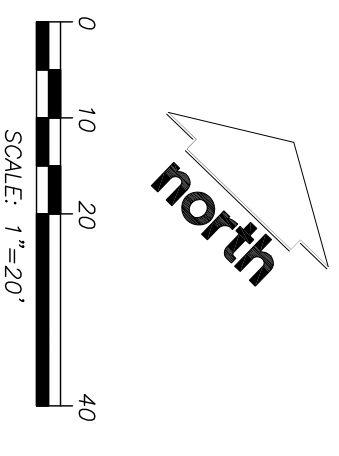
E. MAIN STREET

S. BUTLER STREET

PHASE I PHASE II

LEGEND (PROPOSED)

- PROPERTY LINE
- ===== 18" CONCRETE CURB & GUTTER
- ===== 18" CONCRETE CURB & GUTTER - REFLECT
- ===== EDGE OF CONCRETE
- ===== EDGE OF PAVEMENT
- ===== ASPHALT PAVEMENT
- ===== CONCRETE PAVEMENT
- ===== CONSTRUCTION FENCING



PARTIAL MAP OF MADISON

GENERAL NOTES

1. DO NOT DISTURB EXISTING CONCRETE PADS AND MOUNTED EQUIPMENT.
2. WORK SHALL BE PHASED TO ALLOW ALL PERMIT PARKERS TO PARK DURING THE WEEK. COORDINATE PHASING AND REQUIRED STALLS WITH THE CITY PARKING UTILITY.
3. EXISTING SIGNS AND PARKING METERS TO BE REMOVED BY THE CITY OF MADISON PARKING UTILITY PRIOR TO RESURFACING AND REPLACED BY THE CITY OF MADISON PARKING UTILITY ONCE RESURFACING IS COMPLETE.
4. PROVIDE A MINIMUM OF 48 HOURS NOTICE FOR THE PARKING UTILITY TO REMOVE THE SIGNS.
5. ALL DIMENSIONS TO FACE OF CURB UNLESS OTHERWISE NOTED.
6. ALL RADII TO FACE OF CURB UNLESS OTHERWISE NOTED.
7. ALL WORK IS TO BE COMPLETED IN ACCORDANCE WITH THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
8. GRINDING SHALL CONSIST OF "MILLING" OR "TOUD PLANNING" THE EXISTING PAVEMENT SURFACE TO ESTABLISH A GROUNDED SURFACE. THE SURFACE SHALL BE FINISHED TO A MINIMUM OF ONE-FOURTH (1/4) INCH (+/-1/8"). THE EXISTING SURFACE TO BE GROUND SHALL INCLUDE ASPHALT PAVEMENT, CONCRETE UTILITY PATCHES AND SMALL AREAS OF CONCRETE PAVEMENT.
9. THE CONTRACTOR SHALL USE ONLY APPROVED GRINDERS OR MILLING MACHINES FOR REMOVING EXISTING PAVEMENT FAILURES SO AS NOT TO DISTURB THE EXISTING SUBBASE.
10. GRINDING AROUND UTILITY CASTINGS TO THE DEPTH OF CUT BEFORE AND AFTER ENCOUNTERING THE CASTINGS SHALL BE INCLUDED IN THE AREA OF THE PAVEMENT SURFACE GROUND. THE CONTRACTOR MAY CHOOSE TO REMOVE THE ENTIRE EXISTING ASPHALT PAVEMENT AROUND THE CASTINGS WHERE GRINDING IS NOT COMPLETED AND REPLACE IT WITH AN ASPHALT MAKEUP PLACED AND COMPACTED IN THREE (3) INCH LIFTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER PLACEMENT AND COMPACTING OF THE ASPHALT MAKEUP. ALL COSTS FOR THE ALTERNATE TO GRINDING AROUND CASTINGS SHALL BE CONSIDERED INCIDENTAL TO GRINDING.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACEMENT WITH 1-1/2" CRUSHED STONE MECHANICALLY UNDERLAYED BY ANY MATERIAL NECESSARY TO BRING SURFACE TO GRADE WHERE THE CONTRACTOR HAS UNDERCUT THE SURFACE WITHOUT THE DIRECTION OF THE ENGINEER.
12. UNLESS OTHERWISE SPECIFIED, CURB AND GUTTER SHALL BE INSTALLED IN MINIMUM LENGTHS OF SIX (6) FEET AND MAXIMUM LENGTHS OF FIFTEEN (15) FEET.
13. THE CONTRACTOR SHALL INSTALL A HEADER AT THE END OF EACH POUR. AT NO TIME SHALL THE CONTRACTOR BE ALLOWED TO SPREAD EXCESS CONCRETE AS A BASE FOR THE NEXT OR ANY SUCCEEDING POUR.
14. WHEREVER DIFFERENT TYPES OF CURB AND GUTTER ARE EMPLOYED, THE CONTRACTOR SHALL TAKE CARE THAT TRANSITIONS FROM ONE TYPE OF CURB AND GUTTER TO ANOTHER TYPE ARE DONE SMOOTHLY WITHOUT LOSS OF FLOW LINE GRADE OR CURB HEAD SHAPE.
15. FULL CONTRACTOR JOINTS SHALL BE A MINIMUM OF THREE (3) INCHES IN DEPTH, AND SHALL BE UNFORMALLY SPACED AND NOT LESS THAN SIX (6) FEET NOR MORE THAN FIFTEEN (15) FEET APART UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
16. IF MACHINE METHODS ARE USED FOR FORMING AND FINISHING CURB AND GUTTER, THE CONTRACTOR MAY SAW DEPTH SEPARATOR PLATES HAVING A MINIMUM DEPTH OF THREE (3) INCHES. THE DEPTH OF CUT AND EQUIPMENT USED IN SAWING SHALL MEET THE APPROVAL OF THE ENGINEER. THE SAWING SHALL BE DONE AS SOON AS IS PRACTICABLE AFTER THE CONCRETE HAS SET SUFFICIENTLY TO PRECLUDE RAVELING DURING THE SAWING AND BEFORE ANY SHIMMADE CONCRETE PAGES PLACE IN THE CONCRETE. IF THIS METHOD RESULTS IN RANDOM CRACKING, THE CONTRACTOR SHALL BE REQUIRED TO USE THE PROPER DEPTH SEPARATOR PLATES.
17. TRANSVERSE EXPANSION JOINTS SHALL BE ONE-HALF (1/2) INCH IN WIDTH AND SHALL BE PLACED ACROSS THE CURB AND GUTTER PERPENDICULAR TO THE CURB LINE AT ALL RAISED POINTS OF CURBS PAVING A RADUS OF TWO HUNDRED (200) FEET OR LESS AND ON BOTH SIDES OF METERS INSTALLED IN CURB AND GUTTER. ALL EXPANSION JOINTS SHALL BE FINISHED TO THE SURFACE AND SHALL BE PROTECTED BY A PROTECTIVE STRIP. ALL EXPANSION JOINTS SHALL BE FORMED BY INSERTING DURING CONSTRUCTION AND LEAVING IN PLACE THE REQUIRED THICKNESS OF JOINT FILLER WHICH SHALL EXTEND THROUGH THE ENTIRE THICKNESS OF BOTH CURB AND GUTTER.
18. THE JOINT FILLER IN TRANSVERSE JOINTS SHALL BE FINISHED WITH A WOODEN FLOAT WHICH IS DIVIDED THROUGH THE CENTER AND WHICH WILL PERMIT FINISHING ON BOTH SIDES OF THE FILLER AT THE SAME TIME. BEFORE THE CURB AND GUTTER IS OPENED TO TRAFFIC, EXCESS JOINT FILLER SHALL BE CUT OFF LEVEL WITH THE FINISHED SURFACE.
19. THE CURB AND GUTTER MUST BE PROTECTED FROM INJURY BY TRAFFIC OR OTHER CAUSES, AND ALSO FROM THE RAYS OF THE SUN UNTIL COMPLETELY SET.
20. ALL CONCRETE, ASPHALT, AND GRAVEL PAVEMENTS; STONE FLAGGING OR PAVING; SIDEWALKS; CURBS AND GUTTERS; CURBS; FENCES; OR OTHER STRUCTURES WHICH MAY HAVE BEEN DAMAGED OR DISPLACED BY THE CONTRACTOR SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION. ALL REPAIRS SHALL BE APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND MAINTENANCE OF ALL OTHER STRUCTURES DISBURBED BY THE CONSTRUCTION. ALL OF THE ABOVE WORK SHALL BE IN ACCORDANCE WITH THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
21. ALL STRUCTURES AND CONDUIT SHALL BE CLEANED OF ANY ACCUMULATIONS OF SILT, DEBRIS OR OTHER FOREIGN MATTER. CONDUIT SHALL BE CLEANED BY USE OF THE PROPER SIZE MANDREL. THE AREA ALONG THE ENTIRE INSTALLATION SHALL BE LEFT CLEAN AND GRADED IN A CONDITION SATISFACTORY TO THE ENGINEER.
22. THE ELECTRICAL CODE OF THE CITY OF MADISON AND SHALL BE SUBJECT TO INSPECTION BY THE CITY BUILDING INSPECTION AND REHABILITATION UNIT. COORDINATE ELECTRICAL UTILITY RELOCATION WITH MADISON GAS AND ELECTRIC. THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL NECESSARY PERMITS AND CERTIFICATES OF INSPECTION.

PAVEMENT MARKING NOTES

1. STRIPING AS SHOWN IS INTENDED TO MATCH THE INTENT OF THE EXISTING STRIPING. CONTRACTOR TO FIELD VERIFY EXISTING STRIPING AND REPORT AND DISCREPANCIES PRIOR TO CONSTRUCTION.
2. USE 4" WIDE, HIGH-VISIBILITY YELLOW PAINT FOR STALL LINES. PAINT SHALL BE LATEX, WATERBORNE EMULSION, LEAD AND CHROMIATE FREE, READY MIXED, COMPLYING WITH FS T-1-P-1952, WITH DRYING TIME OF LESS THAN 45 MIN. APPLY AT MANUFACTURER'S RECOMMEND RATES TO PROVIDE A MINIMUM WET FILM THICKNESS OF 15 MILS.
3. PAVEMENT MARKING SHALL ONLY BE ON CLEAN, DRY SURFACES, AND A MINIMUM AHEAD OR SURFACE TEMPERATURE OF 40° F FOR OIL-BASED MATERIALS, 50° F FOR WATER-BASED MATERIALS, AND NOT EXCEEDING 95° F.
4. ALLOW PAVING TO AGE OVERNIGHT BEFORE STARTING PAVEMENT MARKING UNLESS OTHERWISE APPROVED BY OWNER.
5. APPLY PAINT WITH MECHANICAL EQUIPMENT TO PRODUCE PAVEMENT MARKINGS, OF DIMENSIONS INDICATED, WITH UNIFORM, STRAIGHT EDGES.
6. MARK AND STRIKE ADA PARKING SPACES APPROPRIATELY.

PROJECT: BRAYTON LOT RESURFACING

PROJECT LOCATION: MADISON DANE COUNTY, WI

JSD PROJECT NO.: 09-3884

SEAL/SIGNATURE:

DESIGN:	DATE:
DOS	1/25/12
DOS	1/25/12
APPROVED: JE	

PLAN MODIFICATIONS:	DATE:
CLIENT REVIEW	07-18-12
CLIENT REVIEW	03-18-13

ALTHOUGH EVERY EFFORT HAS BEEN MADE IN PREPARING THESE PLANS AND CHECKING FOR ACCURACY, THE USER SHALL BE RESPONSIBLE FOR THE PROPER INTERPRETATION AND USE OF THESE PLANS AND DIMENSIONS OF THEIR TRADE AND BE RESPONSIBLE FOR THE SAME.

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SITE PLAN

SHEET NUMBER: C-1.0

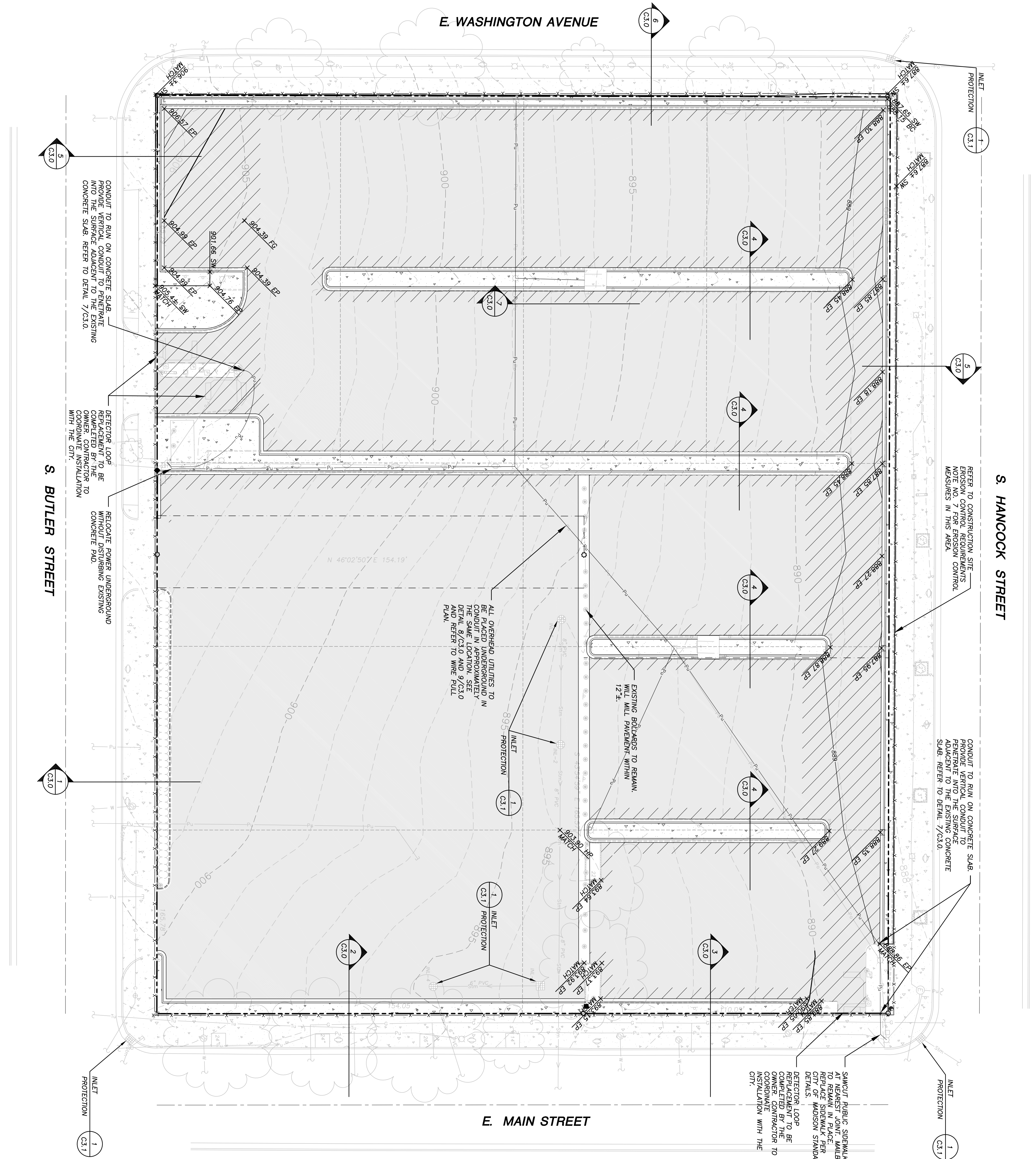
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CITY OF MADISON

210 Martin Luther King Jr. Blvd.
City County Building, Rm. 513



S. HANCOCK STREET

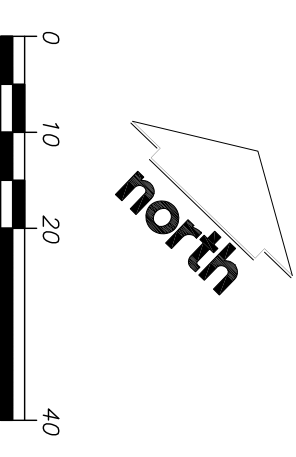
E. WASHINGTON AVENUE

E. MAIN STREET

S. BUTLER STREET

LEGEND (PROPOSED)

- PROPERTY LINE
- ==== 18" CONCRETE CURB & GUTTER
- ===== 18" CONCRETE CURB & GUTTER - REFLECT
- EDGE OF CONCRETE
- CONSTRUCTION FENCING
- INDEX CONTOUR
- INTERMEDIATE CONTOUR (COORDINATE WITH MARK)
- ASPHALT PAVEMENT (REMOVE AND REGRADE)
- ASPHALT PAVEMENT (MILL/PULVERIZE AND REPLACE)
- CONCRETE PAVEMENT
- SPCL ELEVATION OF CURB
- EP - EDGE OF PAVEMENT
- SW - SIDEWALK
- FG - FINAL GRADE



GENERAL NOTES

1. REFER TO THE EXISTING CONDITIONS SURVEY FOR EXISTING CONDITIONS NOTES AND LEGENDS.
2. ALL WORK IN THE ROW SHALL BE IN ACCORDANCE WITH THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
3. NO SITE GRADING OUTSIDE OR DIMINISHING OF PROPOSED GRADING LIMITS. NO LAND DISTURBANCE BEYOND PROPERTY LINES.
4. JSD SHALL BE HELD HARMLESS AND DOES NOT WARRANT ANY DEVIATIONS BY THE OWNER/CONTRACTOR FROM THE APPROVED CONSTRUCTION PLANS THAT MAY RESULT IN DISCIPLINARY ACTIONS BY ANY OR ALL REGULATORY AGENCIES.
5. DRAWING FOR REVIEW - NOT FOR CONSTRUCTION UNLESS OTHERWISE NOTED IN THE TITLE BLOCK.

CONSTRUCTION SITE EROSION CONTROL REQUIREMENTS

1. ALL EROSION CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED BY THE CONTRACTOR IN ACCORDANCE WITH THE MDWR CONSTRUCTION SITE EROSION & SEDIMENT CONTROL TECHNICAL STANDARDS (FORMERLY REFERRED TO AS BMP-5). IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN A COPY OF THESE STANDARDS.
2. INSTALL EROSION CONTROL MEASURES PRIOR TO ANY SITE WORK, INCLUDING GRADING OR DISTURBANCE OF EXISTING SURFACE MATERIALS AS SHOWN ON PLAN. MODIFICATIONS TO SEDIMENT CONTROL DESIGN MAY BE CONDUCTED TO MEET UNFORSEEN FIELD CONDITIONS IF MODIFICATIONS CONFORM TO WORKER TECHNICAL STANDARDS.
3. ALL EROSION CONTROL MEASURES AND STRUCTURES SERVING THE SITE MUST BE INSPECTED AT LEAST WEEKLY AND WHENEVER 0.5 INCHES OF RAIN IS PRODUCED WITHIN 24 HOURS.
4. EROSION CONTROL MEASURES SHALL BE REMOVED ONLY AFTER SITE CONSTRUCTION IS COMPLETE WITH ALL SOIL SURFACES HAVING AN ESTABLISHED VEGETATIVE COVER.
5. EXISTING GRAVEL TO BE USED AS TRACKING PADS. 6\"/>

UTILITY NOTES

1. IF ANY ERRORS, DISCREPANCIES, OR OMISSIONS WITHIN THE PLAN BECOME APPARENT, IT SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO CONSTRUCTION SO THAT CORRECTIONS OR REVISIONS MAY OCCUR.
2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF MADISON AND WISCONSIN DEPARTMENT OF TRANSPORTATION REQUIREMENTS.
3. THE CONTRACTOR SHALL INSTALL A PEDESTRIAN FENCE AROUND ALL EXCAVATIONS TO BE LEFT OPEN OVER NIGHT AS REQUIRED.
4. THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH CITY OF MADISON STANDARD SPECIFICATIONS.
5. PRIOR TO CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR:
 - EXAMINING ALL SITE CONDITIONS RELATIVE TO THE CONDITIONS INDICATED ON THE ENGINEERING DRAWINGS. ANY DISCREPANCIES ARE TO BE REPORTED TO THE ENGINEER AND RESOLVED PRIOR TO THE START OF CONSTRUCTION.
 - OBTAINING ALL NECESSARY PERMITS, BONDS, AND ALL OTHER FEES REQUIRED FOR PROPOSED WORK.
 - VERIFYING UTILITY ELEVATIONS AND NOTIFYING ENGINEER OF ANY DISCREPANCY. NO WORK SHALL BE PERFORMED UNTIL THE DISCREPANCY IS RESOLVED.
 - NOTIFYING ALL UTILITIES PRIOR TO THE DISRUPTION OF CONSTRUCTION IMPROVEMENTS.
 - NOTIFYING THE DESIGN ENGINEER AND MUNICIPALITY 48 HOURS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE FOR APPROPRIATE CONSTRUCTION OBSERVATION.
6. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING THE CITY OF MADISON WITH AS-BUILT CONDITIONS OF THE DESIGNATED IMPROVEMENTS IN ORDER THAT THE APPROPRIATE DRAWINGS CAN BE PREPARED IF REQUIRED. ANY CHANGES TO THE DRAWINGS OR ADDITIONAL ITEMS MUST BE REPORTED TO THE CONTRACTING ENGINEER AS WORK PROGRESSES.
7. THE FIRM CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL CONSTRUCTION WITH OTHER CONTRACTORS INVOLVED WITH CONSTRUCTION OF THE PROPOSED DEVELOPMENT AND FOR REPORTING ANY ERRORS OR DISCREPANCIES BETWEEN THESE PLANS AND PLANS PREPARED BY OTHERS.
8. ANY SANITARY SEWER, SANITARY SEWER SERVICES, WATER MAIN, WATER SERVICES, STORM SEWER, OR OTHER UTILITIES, WHICH ARE DAMAGED BY THE CONTRACTORS, SHALL BE REPAIRED TO THE OWNER'S SATISFACTION AT THE CONTRACTORS' EXPENSE.
9. CONTRACTOR IS RESPONSIBLE FOR SITE SAFETY DURING THE CONSTRUCTION OF IMPROVEMENTS.

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PROJECT:
 BRAYTON LOT
 RESURFACING

PROJECT LOCATION:
 1 S. BUTLER ST., MADISON
 DANNE COUNTY, WI

JSD PROJECT NO.: 09-3884
SEAL/SIGNATURE:

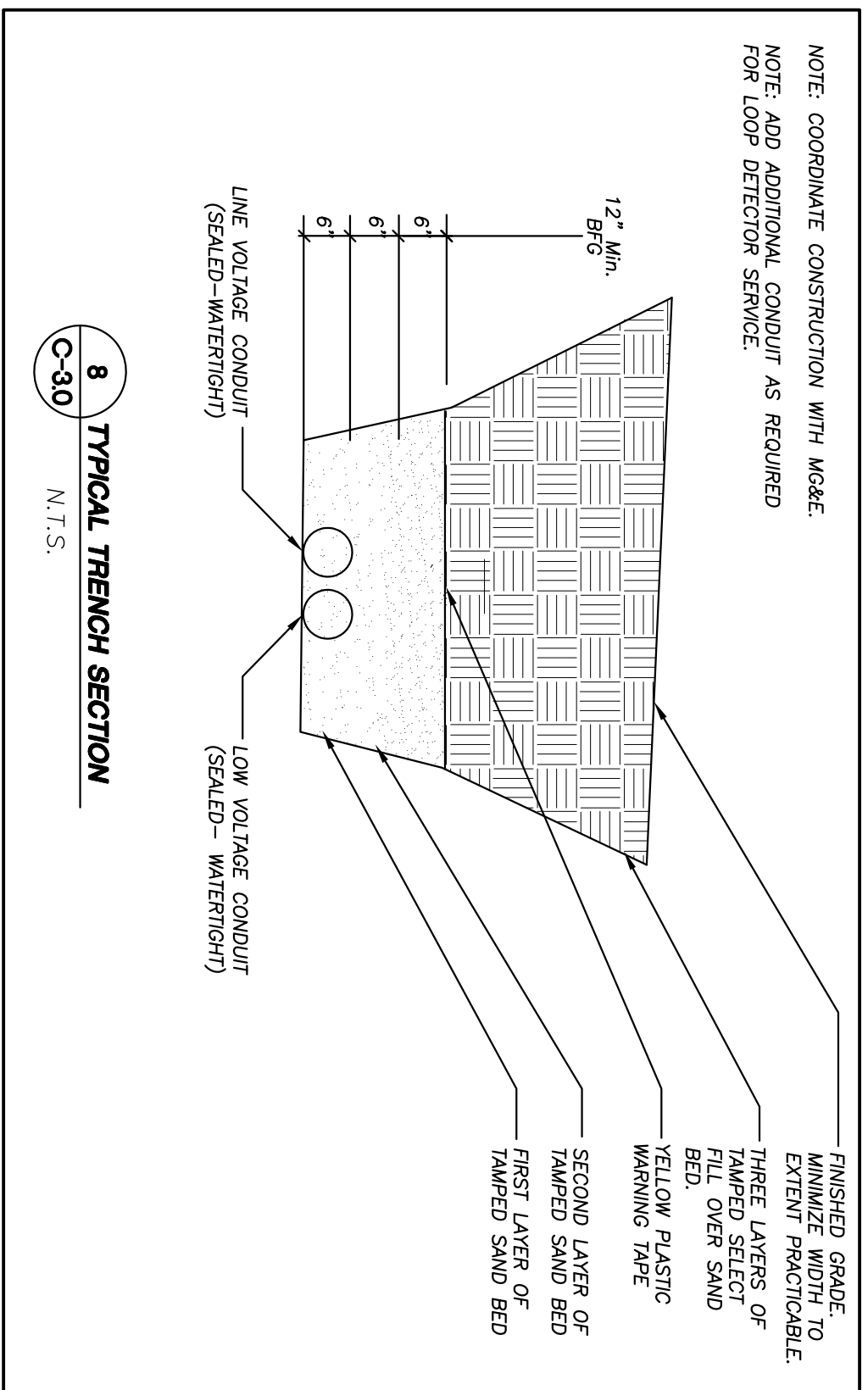
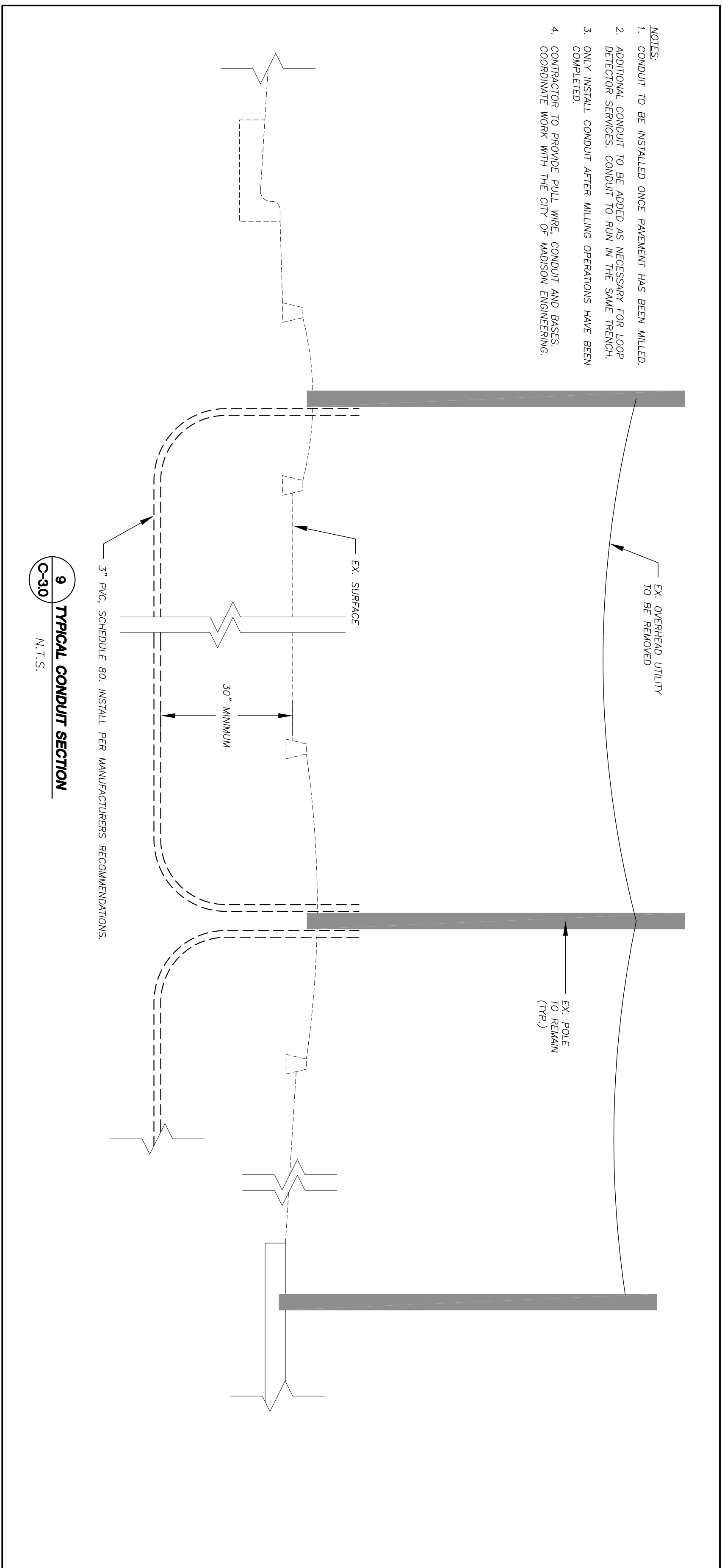
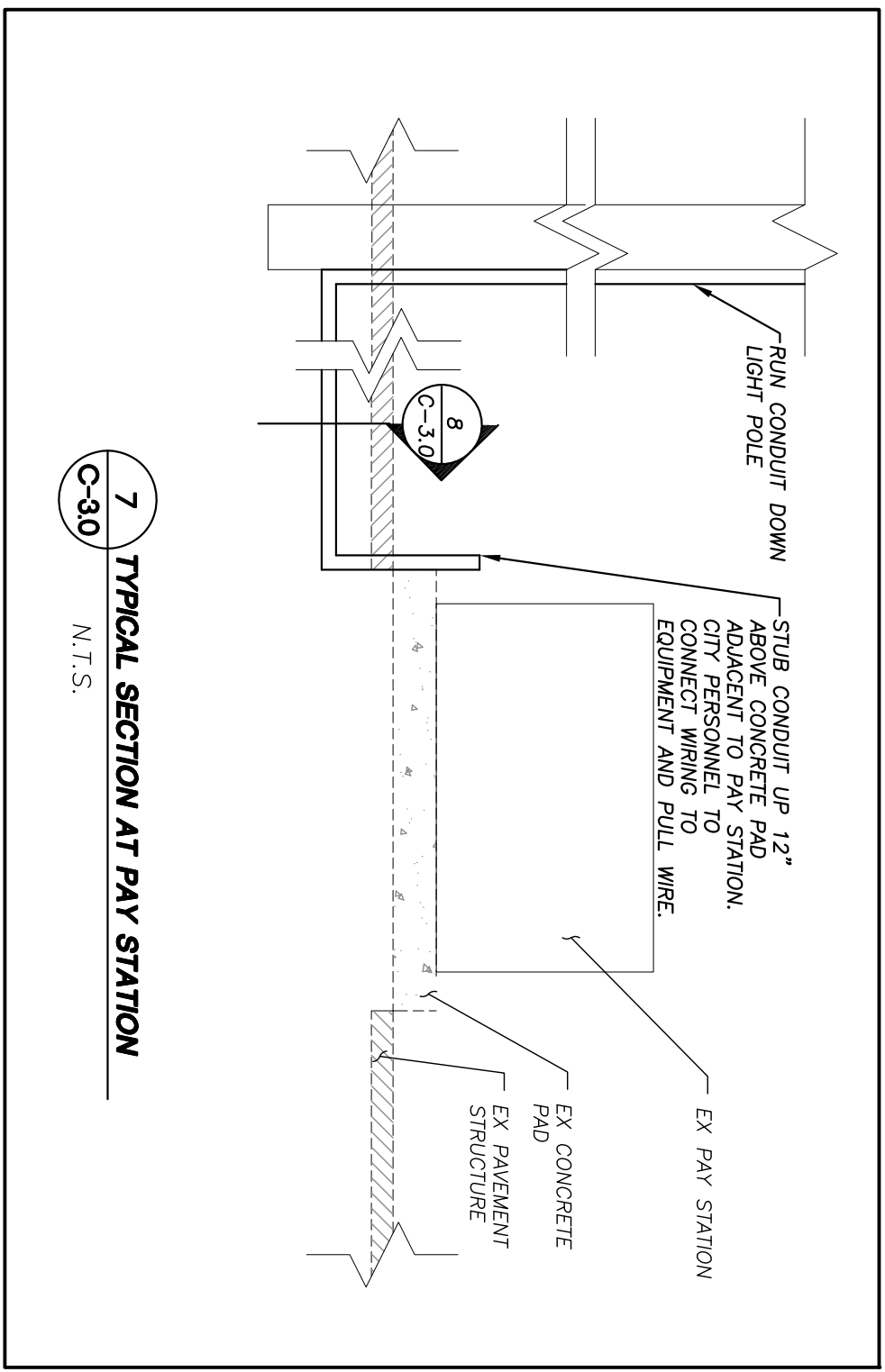
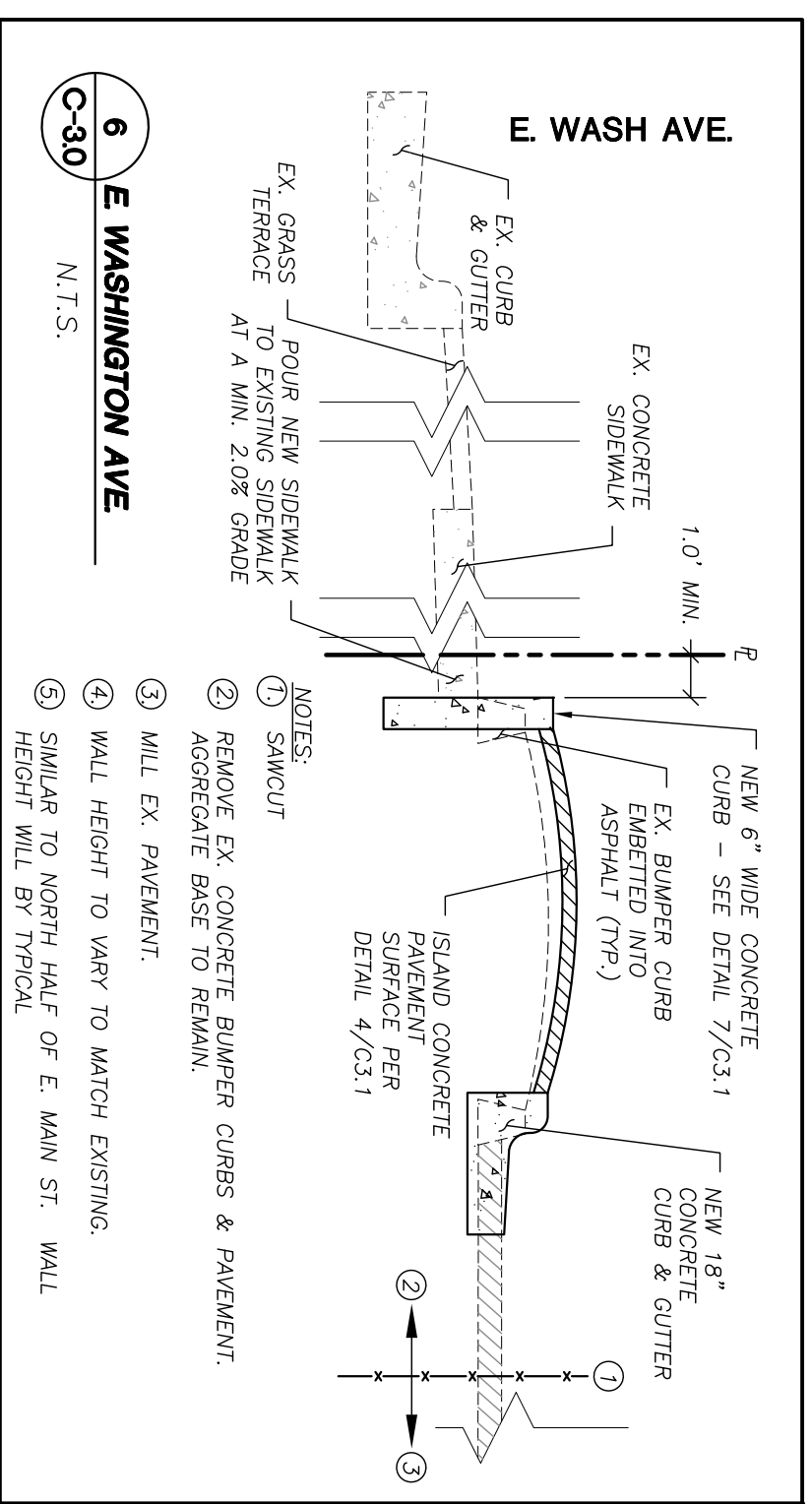
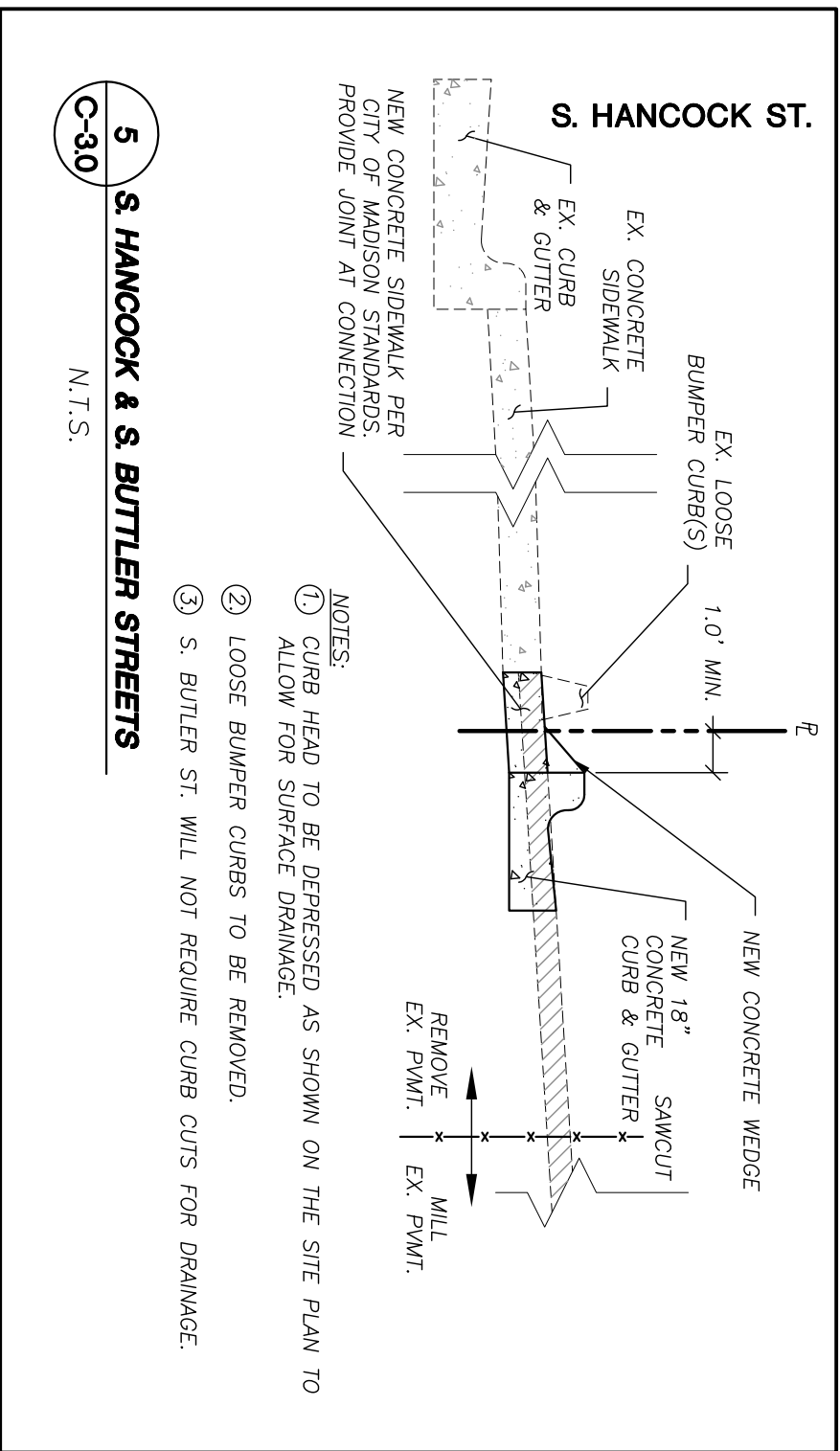
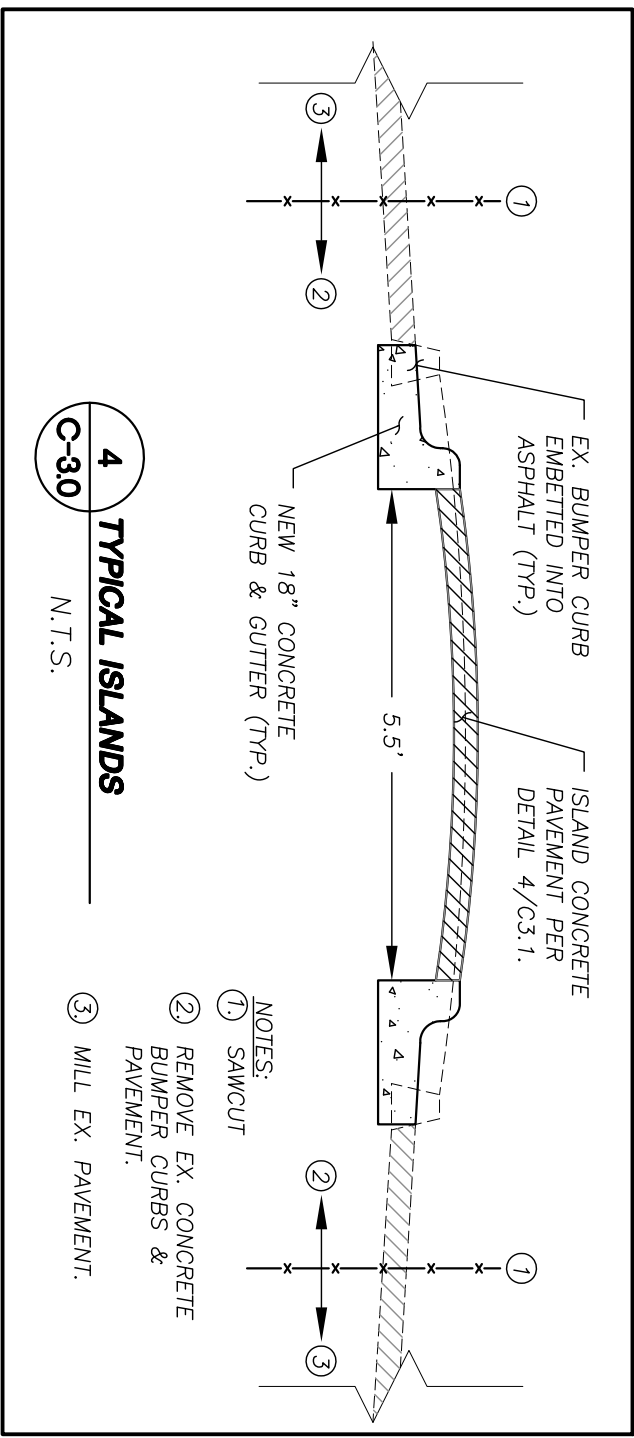
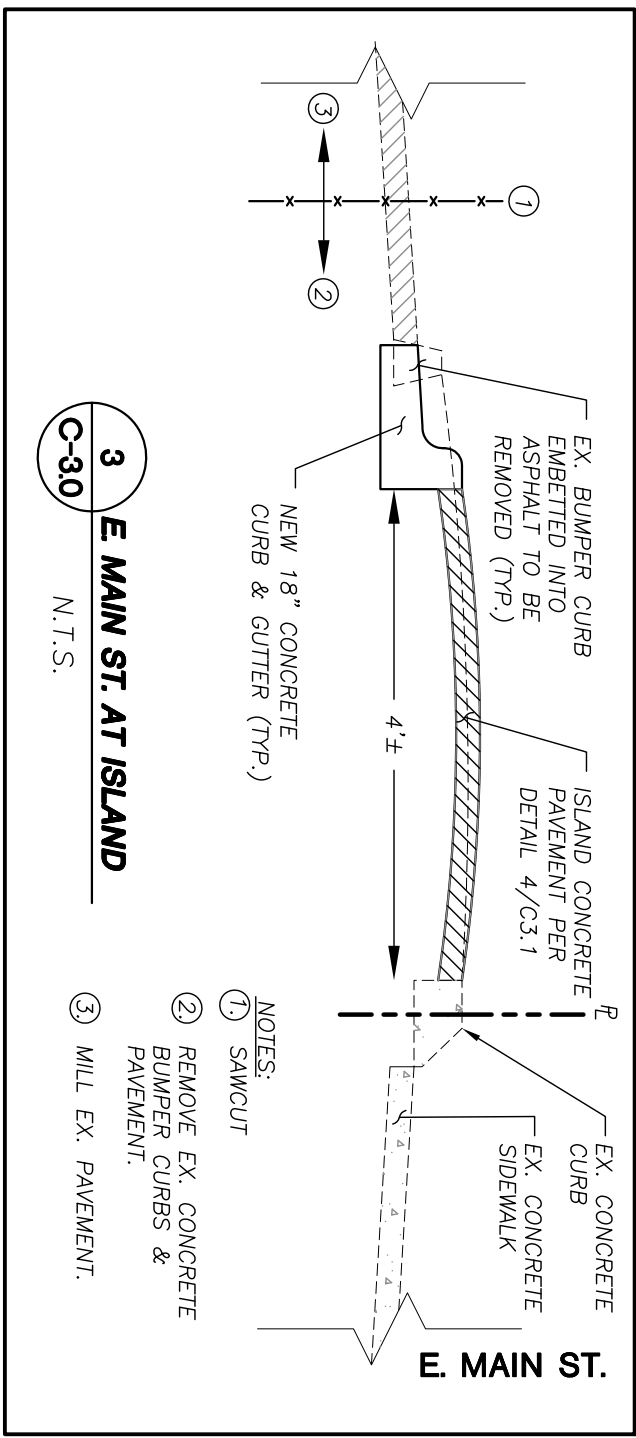
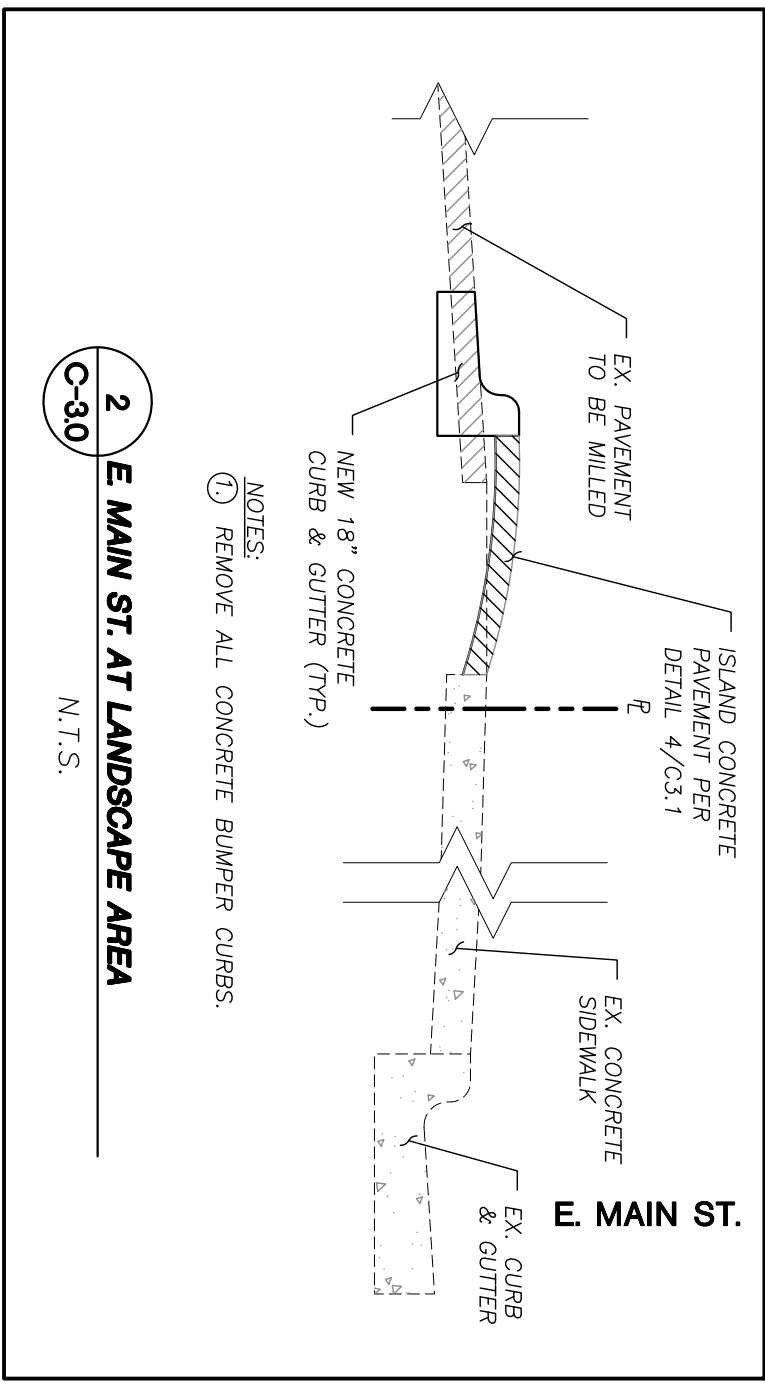
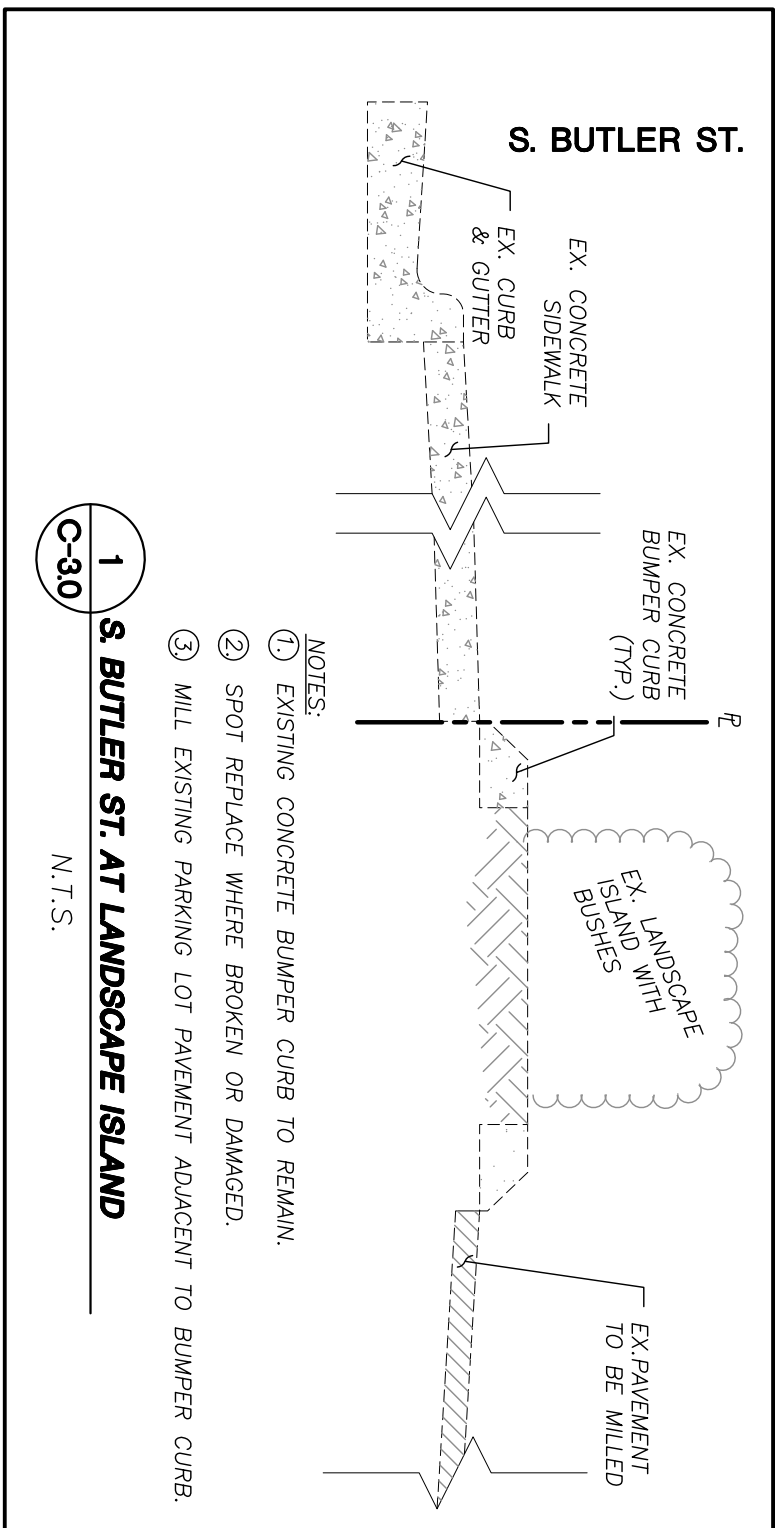
DESIGN: DOS 01/23/12
DRAWN: DOS 01/23/12
APPROVED: JE

PLAN MODIFICATIONS:

CLIENT REVIEW	DATE
	07-18-12
	03-13-13

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SHEET NUMBER:
 GRADING, EROSION
 CONTROL, UTILITY &
 PAVING PLAN
C-2.0



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PROJECT:
BRAYTON LOT RESURFACING

PROJECT LOCATION:
 MADISON
 DANE COUNTY, WI

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APPROVED:	JE	

PLAN MODIFICATIONS:

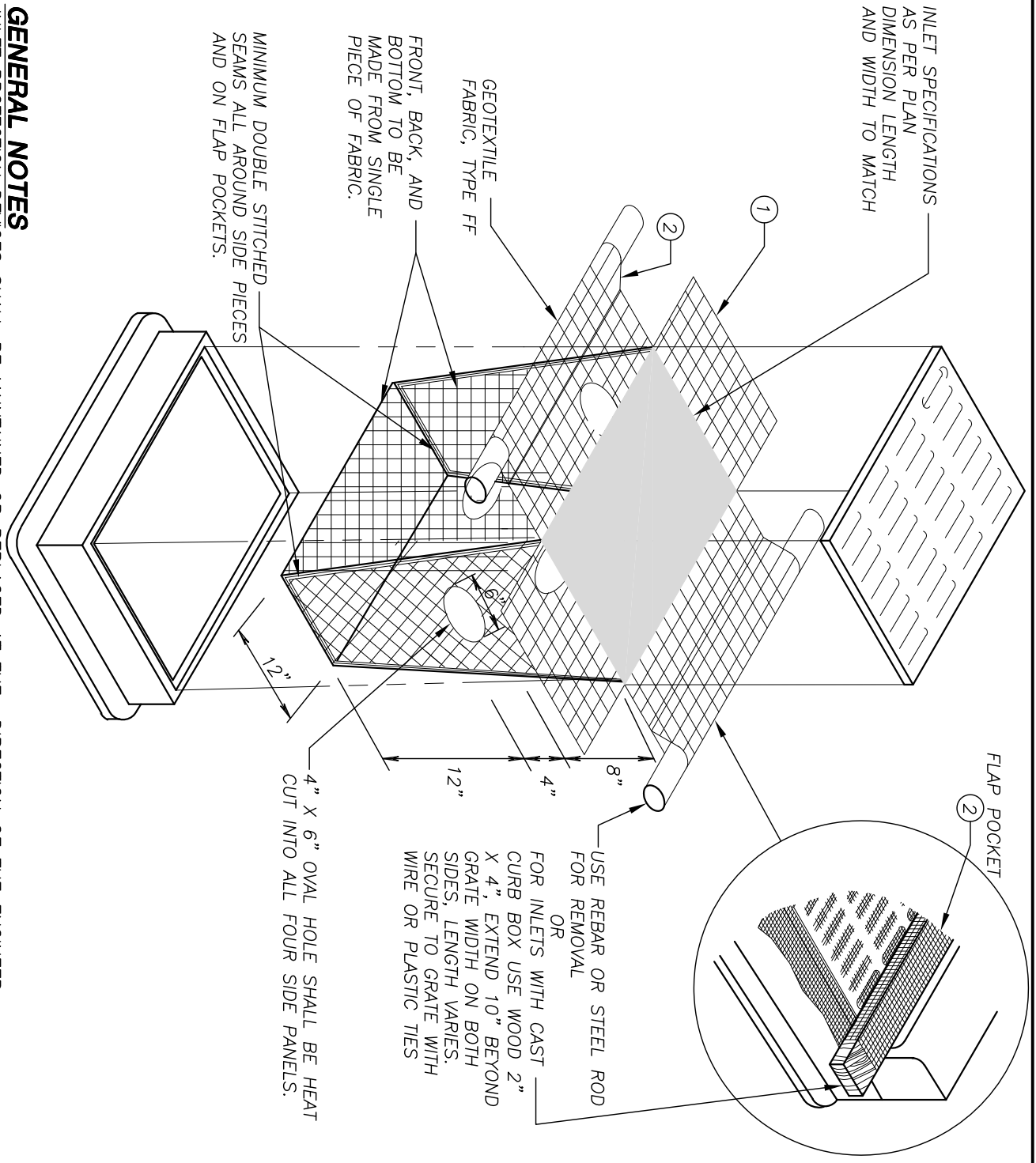
CLIENT REVIEW	DATE
CLIENT REVIEW	07-18-12
CITY OF MADISON	03-11-13
	04-16-13

ALTHOUGH EVERY EFFORT HAS BEEN MADE IN PREPARING THESE PLANS AND CHECKING FOR CONFLICTS, THE USER OF THESE PLANS AND SUBCONTRACTORS MUST CHECK ALL DETAIL AND DIMENSIONS OF THEIR TRADE AND BE RESPONSIBLE FOR THE SAME.

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SHEET TITLE:
DETAILS

SHEET NUMBER:
C-3.0



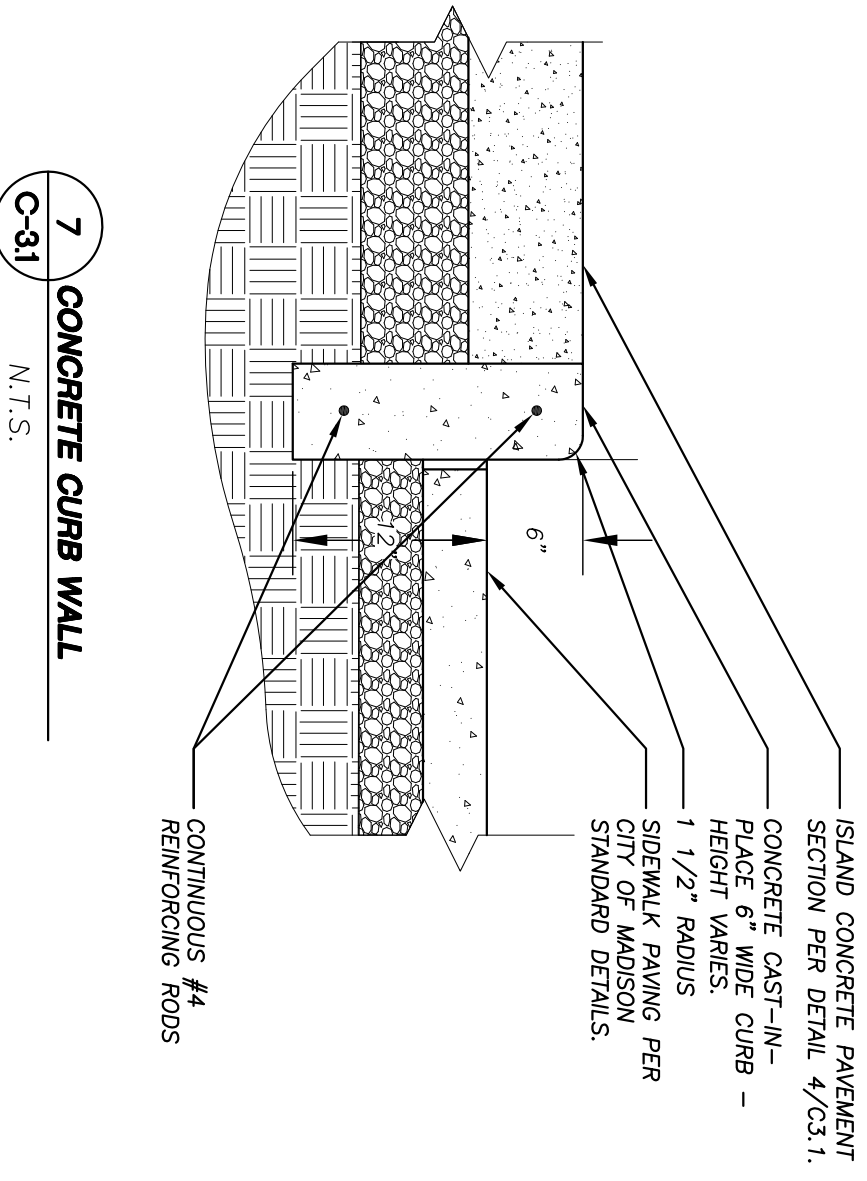
GENERAL NOTES
 INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER. MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.
 WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.
 FINISHED SIZE INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
 ② FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2x4.

① RINGED SIZE INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

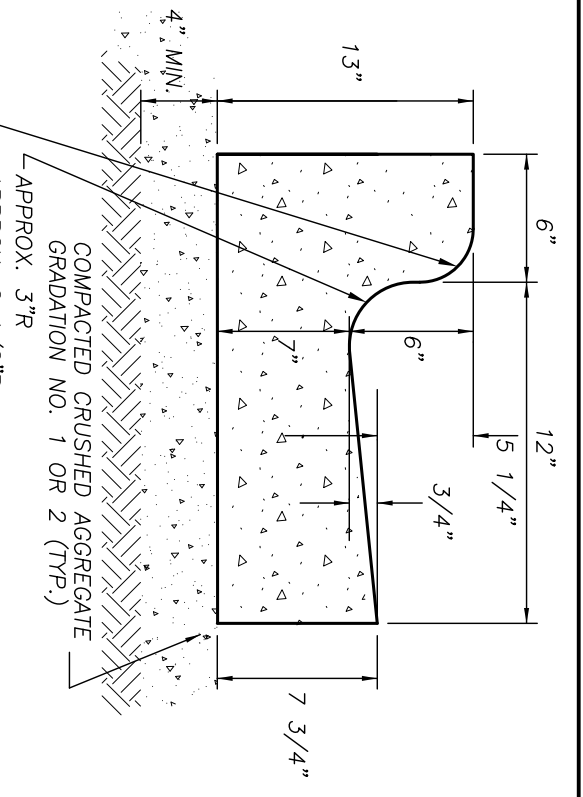
INSTALLATION NOTES

INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE. IF LESS THAN 30", USE TYPE C.
 THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CATCH THE BAG USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

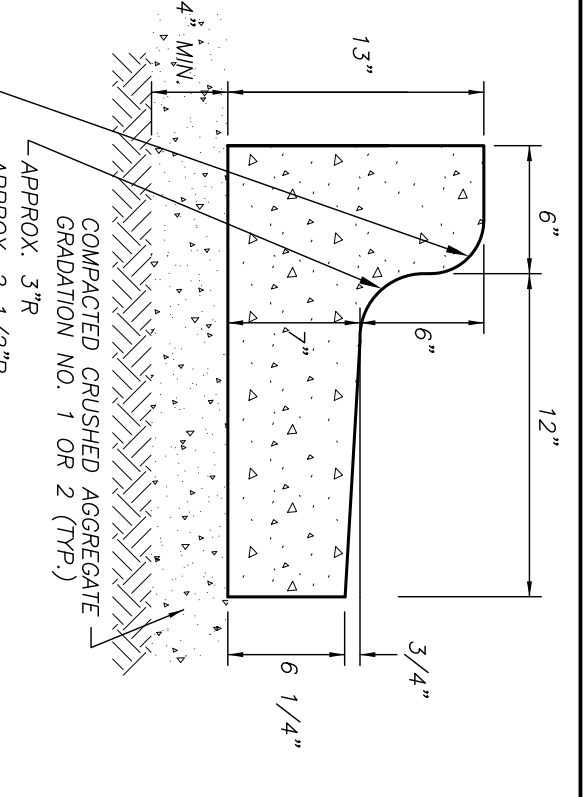
1 INLET PROTECTION, TYPE D
 C-31 N.T.S.



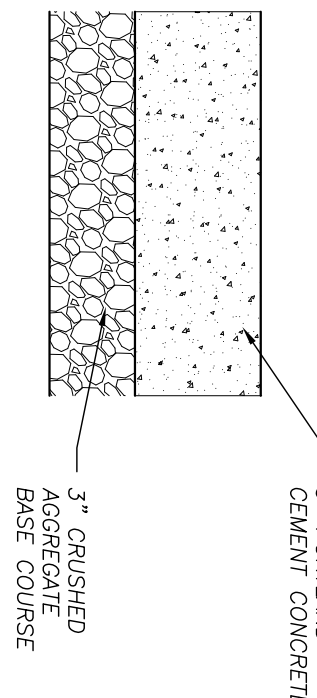
7 CONCRETE CURB WALL
 C-31 N.T.S.



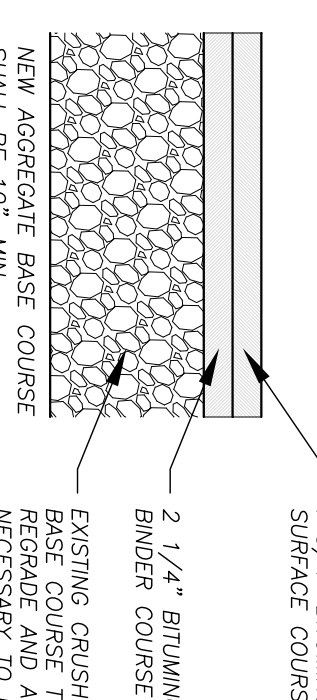
2 18" STANDARD CURB AND GUTTER
 C-31 N.T.S.



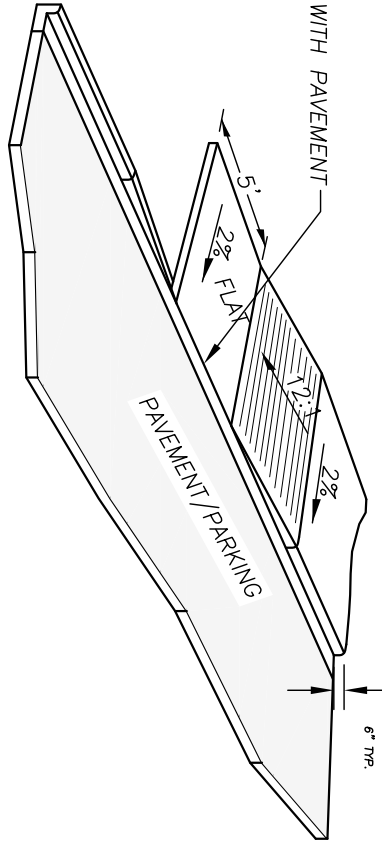
3 18" REJECT CURB AND GUTTER
 C-31 N.T.S.



4 ISLAND CONCRETE SURFACE SECTION
 C-31 N.T.S.



5 DRIVE SURFACE PAVEMENT SECTION
 C-31 N.T.S.



6 ACCESSIBLE WALK CROSS SECTION
 C-31 N.T.S.

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