



Madison, Wisconsin

CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

6502 MILWAUKEE ST AND 6501 TOWN CENTER DR

INDEX OF SHEETS

SHEET NO. 1	GENERAL NOTES
SHEET NO. 2-3	STREET DETAILS
SHEET NO. C - 1.8	UTILITY PLAN

CITY PROJECT NO. 12323
CONTRACT NO. 8403

**THIS PLAN IS FOR WORK
WITHIN THE PUBLIC
RIGHT-OF-WAY ONLY**

PROJECT LOCATION



REVISION HISTORY
REV1 - 8/10/22: SHEET NO. 2 & SHEET C-1.8

PUBLIC IMPROVEMENT PROJECT APPROVED

APPROVED DATE: 6/11/2019

BY THE COMMON COUNCIL OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN APPROVED BY:

[Signature] 11/15/19

City Engineer Date

STREET GRADES DESIGNED BY:



STORM & SANITARY SEWER DESIGNED BY:

SEE INDIVIDUAL SHEET FOR STAMP

STREET GEOMETRICS DESIGNED BY:



WATER DESIGNED BY:

SEE INDIVIDUAL SHEET FOR STAMP

PLOT SCALE: 1:1
PLOT NAME: ---
REV. DATE: 11/4/2019 11:09 AM
ORIGINATOR: CITY_OF_MADISON
FILE NAME: M:\DESIGN\Projects\12323\CAD\Plan Sheets\12323_T1.dwg
DATE: 11/4/2019 11:17 AM

GENERAL NOTES

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADES OF 0.5% TOWARD STORM SEWER INLETS.

ALL DITCHES SHALL DRAIN WITH A MINIMUM GRADES OF 0.5%

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL BE 1.5%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.03. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4.00:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENT FROM THE CITY ENGINEER PRIOR TO STAKING THIS PROJECT.

CURB STATION AND OFFSETS SHALL BE TO THE FACE OF CURB UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE TOP OF CURB (OR EXTENDED TOP OF CURB FOR DRIVEWAYS OR RAMPS) UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL BE RESPONSIBLE FOR COORDINATING AND SCHEDULING ANY REQUIRED RELOCATION OR ADJUSTING OF PRIVATE UTILITY FACILITIES, SUCH AS POLES, PEDESTALS, BOXES, STRUCTURES, CASTINGS OR MANHOLES, WITH PRIVATE UTILITY COMPANIES.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

CONTRACTOR/DEVELOPER SHALL SUBMIT A TRAFFIC CONTROL PLAN TO CITY TRAFFIC ENGINEERING AT LEAST 10 WORKING DAYS PRIOR TO THE START OF WORK. WORK WITHIN THE RIGHT-OF-WAY SHALL NOT BEGIN UNTIL THE TRAFFIC CONTROL PLAN IS APPROVED.

ANY PAVEMENT MARKINGS THAT ARE REMOVED OR DAMAGED SHALL BE REPLACED BY THE DEVELOPER/CONTRACTOR AS DIRECTED BY CITY TRAFFIC ENGINEERING.

ALL PAVEMENT SHALL BE PATCHED IN ACCORDANCE WITH CITY PAVEMENT PATCHING CRITERIA AND SHALL BE ACCOMPLISHED AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER.

ALL DISTURBED TERRACE AREAS SHALL BE RESTORED WITH 4-INCHES OF TOPSOIL, SEED, AND EROSION MAT UNLESS DIRECTED OTHERWISE ON THE PLAN OR BY THE CITY CONSTRUCTION ENGINEER.

CONVENTIONAL SIGNS	
FIELD VERIFY ALL UTILITY LOCATIONS	
GAS	— G —
STORM SEWER	— ST —
SANITARY SEWER	— SAN —
WATER	— W —
BURIED ELECTRIC	— E —
OVERHEAD ELECTRIC	— OH —
POWER POLE	
ADA COMPLIANT RAMP W/ DETECTABLE WARNING FIELD	
COMBUSTIBLE FLUIDS	

PAVEMENT PATCHING CRITERIA

CRITERIA USE ON:
STREETS WITH PAVEMENT RATING > 6
ARTERIAL STREETS

1. LENGTH OF PATCH

- 1.1. MINIMUM 50 FEET LONG
- 1.2. MINIMUM OF 15 FEET BEYOND THE EXCAVATION
- 1.3. WHERE MULTIPLE PATCHES ARE CREATED AND THE SEPARATION BETWEEN THEM IS LESS THAN 100 FEET, THE PATCHES SHALL BE COMBINED INTO A SINGLE PATCH.
- 1.4. THE PATCHES SHALL BE ADJUSTED IN THE FIELD TO MEET SPECIAL CONDITIONS SUCH AS PREVIOUS PAVING OR PATCHING LIMITS.

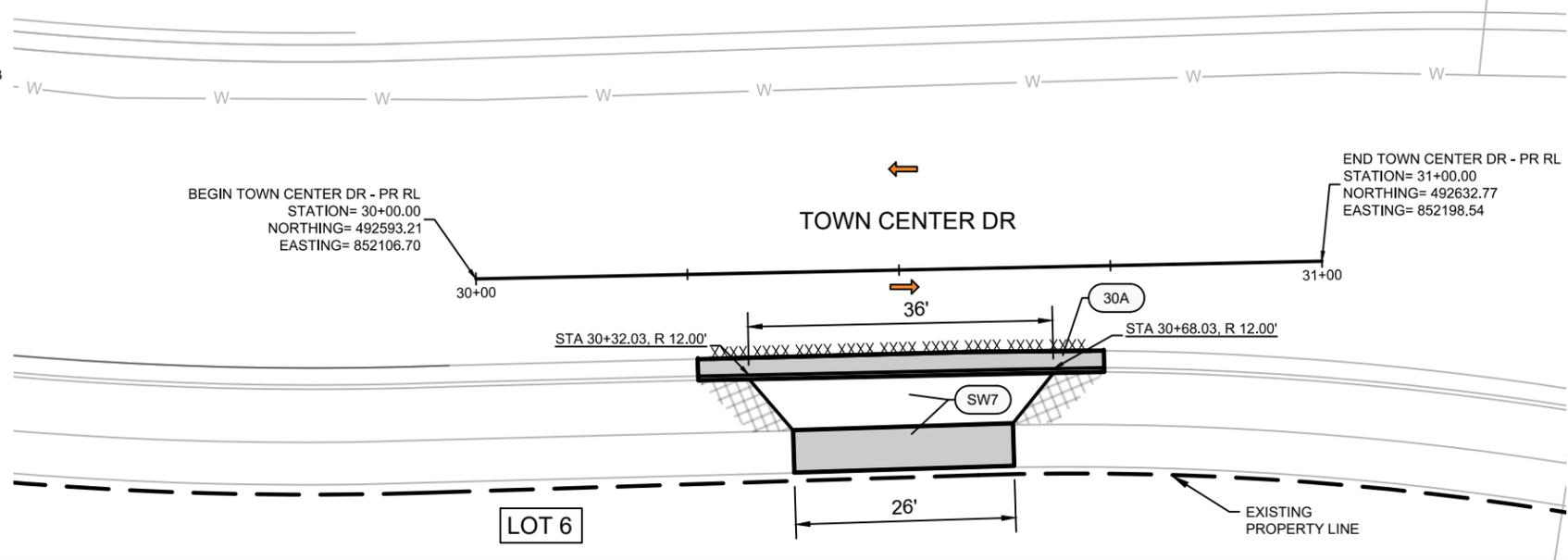
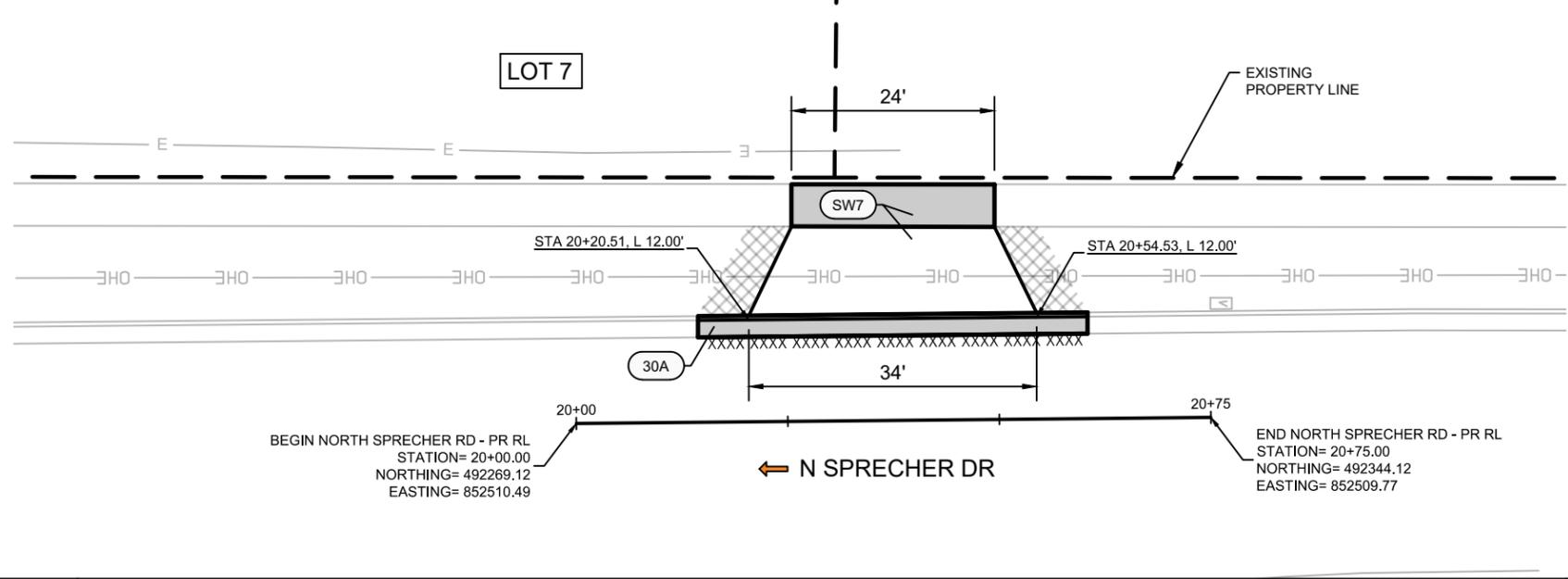
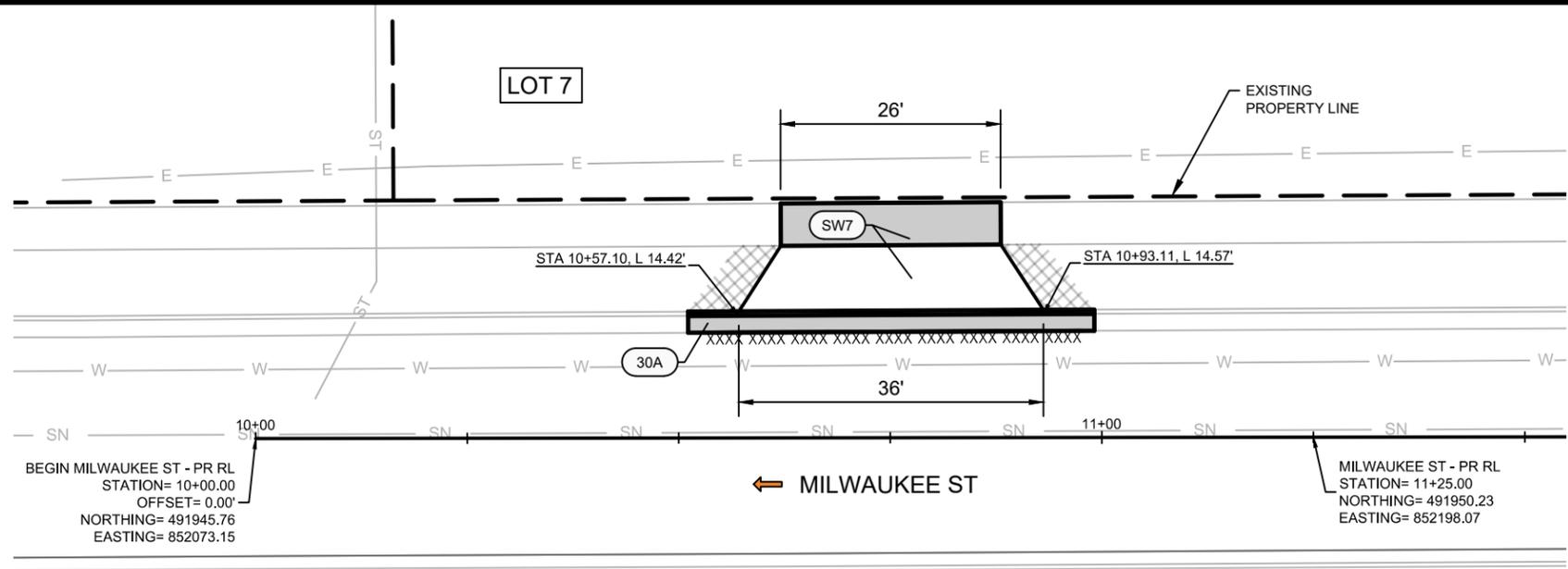
2. WIDTH OF PATCH (ALL DIMENSIONS ARE CURB FACE TO FACE)

- 2.1. ALL STREETS EXCEPT DIVIDED OR ONE-WAY ROADWAYS
 - 2.1.1. STREET WIDTH OF 0 - 24 FEET WIDE: PATCH ENTIRE STREET WIDTH
 - 2.1.2. STREET WIDTH OF 25 TO 37 FEET WIDE: PATCH ONE-HALF THE STREET WIDTH (CURB TO CENTERLINE OF ROADWAY).
NOTE: UTILITY ENGINEER MAY ADJUST PAVING LIMIT TO CORRESPOND WITH A PAINTED CENTERLINE IN SITUATIONS WHERE THE PAINTED CENTERLINE IS NOT IN THE CENTER OF THE STREET.
 - 2.1.3. STREET WIDTH 38 FEET AND UP: PATCH WIDTH OF ENTIRE LANE FOR EACH LANE WHICH WAS DISTURBED BY THE EXCAVATION.
 - 2.1.3.1. IF THE LANE IS ADJACENT TO A BIKE LANE, INCLUDE THE BIKE LANE (EXCEPT WHEN THERE IS A PARKING LANE BETWEEN THE BIKE LANE AND THE CURB).
 - 2.1.3.2. IF THE LANE IS A BIKE LANE AND ADJACENT TO A PARKING LANE, INCLUDE THE PARKING LANE.
 - 2.1.3.3. IF THE LANE IS A BIKE LANE AND NOT ADJACENT TO A PARKING LANE, INCLUDE THE ADJACENT TRAVEL LANE.
- 2.2. DIVIDED ROADWAYS AND ONE-WAY STREETS
 - 2.2.1. STREET 0 TO 19 FEET WIDE: PATCH ENTIRE STREET WIDTH.
 - 2.2.2. STREET WIDTH 20 FEET AND UP: PATCH WIDTH OF ENTIRE LANE FOR EACH LANE WHICH WAS DISTURBED BY THE EXCAVATION.
 - 2.2.2.1. IF THE LANE IS ADJACENT TO A BIKE LANE, INCLUDE THE BIKE LANE (EXCEPT WHEN THERE IS A PARKING LANE BETWEEN THE BIKE LANE AND THE CURB).
 - 2.2.2.2. IF THE LANE IS A BIKE LANE AND ADJACENT TO A PARKING LANE, INCLUDE THE PARKING LANE.
 - 2.2.2.3. IF THE LANE IS A BIKE LANE AND NOT ADJACENT TO A PARKING LANE, INCLUDE THE ADJACENT TRAVEL LANE.

STREET RATINGS		
STREET NAME	CLASSIFICATION	PAVEMENT RATING
MILWAUKEE ST	ARTERIAL	7
N SPRECHER RD	ARTERIAL	6
TOWN CENTER DR	LOCAL	7

CITY OF MADISON MINIMUM PAVEMENT DESIGN						
TYPE	CRUSHED AGGREGATE BASE COURSE		ASPHALTIC CONCRETE PAVEMENT			
	LOWER LAYER GRADATION 1	UPPER LAYER GRADATION 2	LOWER LAYER		UPPER LAYER	
			TYPE	THICKNESS	TYPE	THICKNESS
A	6"	6"	4 LT 58-28 S	1.75"	4 LT 58-28 S	1.75"
B	6"	6"	3 LT 58-28 S	2.50"	4 LT 58-28 S	2.00"
C	6"	6"	3 MT 58-28 S/H 3 HT 58-28 H	3.50"	4 MT 58-28 S/H 4 HT 58-28 H	2.00"

12323	MADISON, MADISON, WI	8403	CONTRACT NO:
GENERAL NOTES	6502 MILWAUKEE ST AND 6501 TOWN CENTER DR	12323	CITY OF MADISON
			
1			



NOTES:
1. SEE CITY COMMERCIAL DRIVEWAY PERMIT LNDUSE-2018-00123

LEGEND	
XXXX	SAWING PAVEMENT, FULL DEPTH
(Grey Shaded Area)	REMOVE CONCRETE
(30A)	TYPE "A" CONCRETE CURB & GUTTER
(24E)	TYPE "E" CONCRETE CURB & GUTTER
(SW5)	5 INCH CONCRETE SIDEWALK
(SW7)	7 INCH CONCRETE SIDEWALK & DRIVE
(PCC10)	10 INCH CONCRETE PAVEMENT
(Cross-hatched Area)	TOPSOIL, SEED, EROSION MAT
(-X-)	SILT FENCE
(X)	INLET PROTECTION TYPE D

12323	MADISON, MADISON, WI	8403	3
PLAN DETAILS - DRIVEWAYS	6502 MILWAUKEE ST AND 6501 TOWN CENTER DR	CITY OF MADISON	
MARK	DESIGNED BY: RES	DATE	DATE
REVISION	REVISION	DATE	DATE
12323	10-21-2019	11-50	3
Scale: 1"=50'			

LEGEND

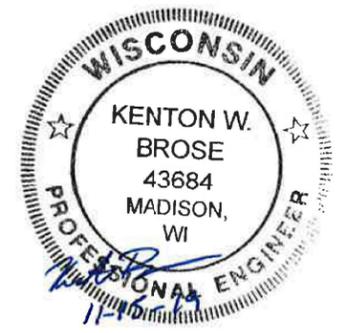
- PROPERTY LINE
- 18" CONCRETE CURB & GUTTER
- PROPOSED BUILDING
- PROPOSED RETAINING WALL
- PROPOSED STORM SEWER
- PROPOSED SANITARY SEWER
- PROPOSED WATER MAIN/LATERAL

SITE UTILITY NOTES

1. THE LOCATION OF EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE. PROTECTION OF EXISTING UTILITIES IS THE CONTRACTOR'S RESPONSIBILITY.
2. ALL SITE UTILITY WORK SHALL BE CONSTRUCTED PER THE CITY OF MADISON STANDARD SPECIFICATIONS.
3. CONTRACTOR TO COORDINATE ELECTRIC, GAS, PHONE & CABLE INSTALLATION WITH THE RESPECTIVE UTILITY COMPANIES.
4. UTILITY CONTRACTOR SHALL VERIFY EXISTING UNDERGROUND UTILITY GRADES AND NOTIFY THE PROJECT SUPERINTENDENT IF A CONFLICT ARISES WITH THE INSTALLATION OF NEW UTILITIES.
5. ALL PIPE AND INLET TYPES SHALL MEET CITY OF MADISON SPECIFICATIONS FOR SEWER CONSTRUCTION
6. ALL INLETS ACCEPTING STORMWATER RUNOFF FROM PARKING AREAS SHALL HAVE CATCHALL HR-I PERMANENT INLET INSERTS INSTALLED
7. PRIVATE WATER MAIN IS IN ACCORDANCE WITH MGO SECTION 34.507 FOR MINIMUM SIZE.

PUMP DESIGN NOTES

- GOULDS WS1538D4M PUMP OR APPROVED EQUAL TO BE USED TO PUMP REQUIRED FLOW AND HEAD
- GOULDS WS1538D4M PUMP WILL PUMP 360 GPM TOTAL HEAD UP TO 18 FEET.
- FINAL INTERIOR SUMP LAYOUT, PIPE ROUTING, AND OUTLET PIPE TO BE DESIGNED BY THE MASTER BUILDING PLUMBER DURING THE BUILDING PLUMBING DESIGN.

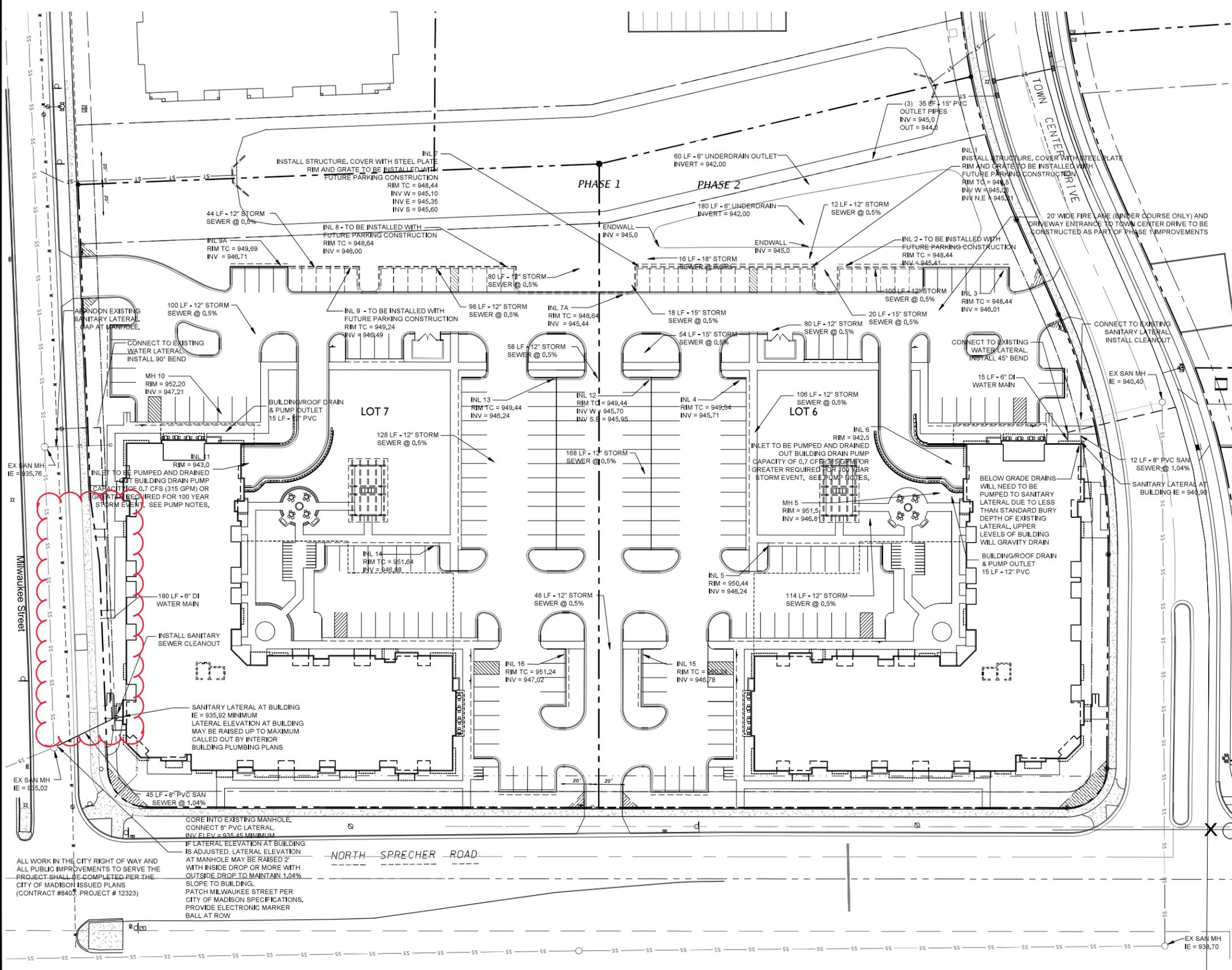


DATE: 06/03/19
 REVISED: 07/19/19
 09/05/19
 09/20/19
 10/24/19
 11/19/19
 08/18/22

DRAWN BY: KWB

FN: 18-03-101

Sheet Number:
C - 1.8



ALL WORK IN THE CITY RIGHT OF WAY AND ALL PUBLIC IMPROVEMENTS TO SERVE THE PROJECT SHALL BE COMPLETED PER THE CITY OF MADISON ISSUED PLANS (CONTRACT #8403, PROJECT # 12323)

CORE INTO EXISTING MANHOLE, CONNECT 8" PVC LATERAL. INV. ELEV. = 935.45 MINIMUM. IF LATERAL ELEVATION AT BUILDING IS ADJUSTED, LATERAL ELEVATION AT MANHOLE MAY BE RAISED 2' WITH INSIDE DROP OR MORE WITH OUTSIDE DROP TO MAINTAIN 1.04% SLOPE TO BUILDING. PATCH MILWAUKEE STREET PER CITY OF MADISON SPECIFICATIONS, PROVIDE ELECTRONIC MARKER BALL AT ROW