

Madison, Wisconsin

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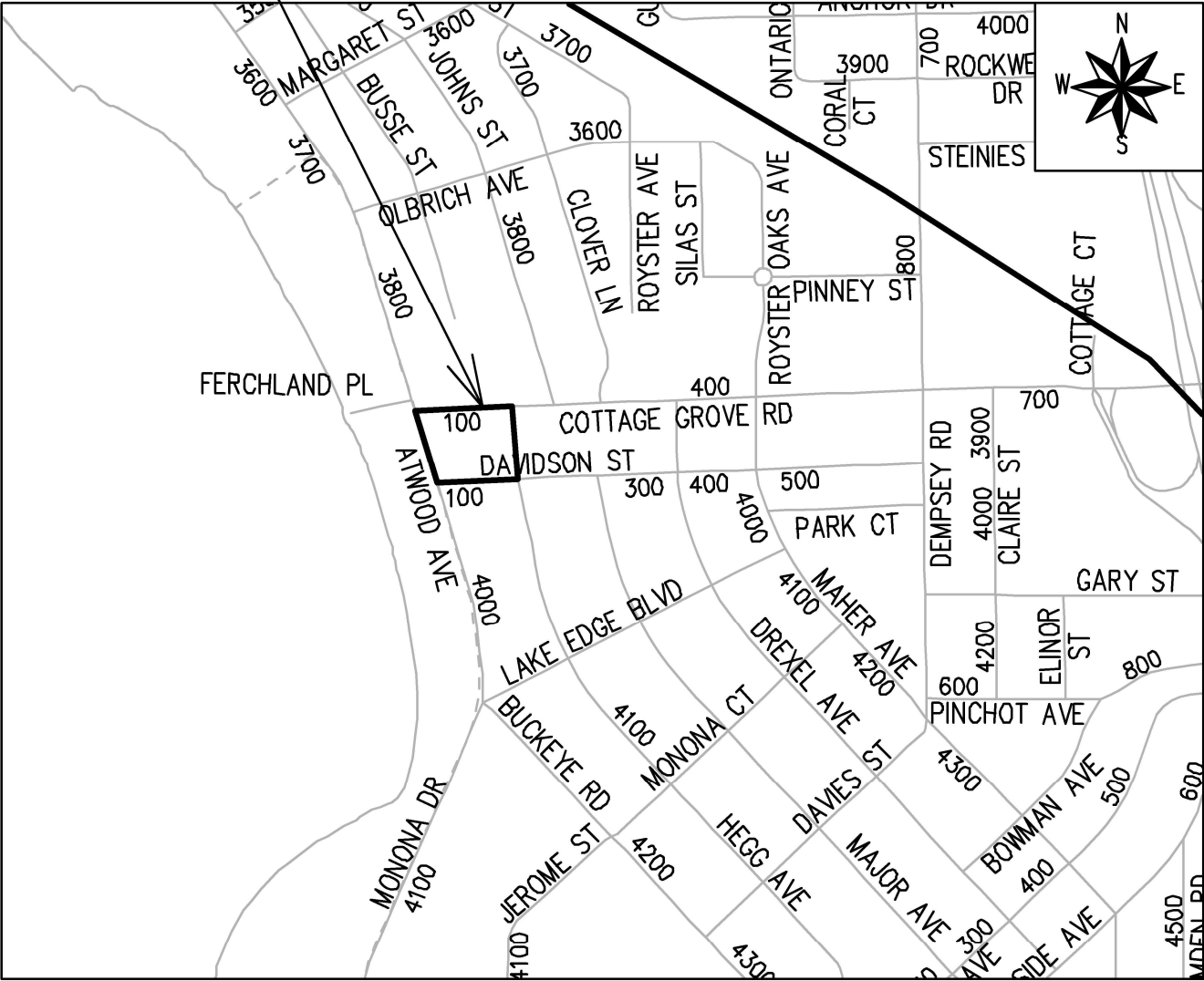
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CITY OF MADISON
CITY ENGINEERING DIVISION
DEPARTMENT OF PUBLIC WORKS
PLAN OF PROPOSED IMPROVEMENT

3914 MONONA DRIVE

PROJECT
LOCATION

CITY PROJECT NO. 14775
CONTRACT NO. 9242



PUBLIC IMPROVEMENT PROJECT
APPROVED

APPROVED DATE: SEPTEMBER 20, 2022

BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

Julie

Jul 5, 2023

City Engineer

Date

STREET
DESIGNED BY:



Jul 5, 2023

ELECTRICAL
DESIGNED BY:



Jul 5, 2023

WATER
DESIGNED BY:

LATERALS
ONLY

STORM SEWER
DESIGNED BY:

LATERALS
ONLY

SANITARY SEWER
DESIGNED BY:

LATERALS
ONLY

PLOT SCALE: 1 IN=1 FT_XREF

PLOT NAME: ---

REV. DATE: 7/5/2023 11:28 AM

ORIGINATOR: CITY OF MADISON

THE LOCATION AND INFORMATION FOR PROPOSED NEW TREES, IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS ARE APPROXIMATE AND ARE SHOWN FOR REFERENCE ONLY. THE LOCATIONS, SPECIFICATIONS AND PLANTING METHODS OF ALL PROPOSED NEW OR REPLACEMENT TREES IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE APPROVED BY THE CITY FORESTER PRIOR TO INSTALLATION.

NO TREES IN THE RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE TRIMMED, PRUNED, REMOVED OR ADVERSELY AFFECTED IN ANY WAY UNTIL THE DEVELOPER HAS RECEIVED WRITTEN PERMISSION FROM THE CITY ENGINEER OR CITY FORESTER. SAID WRITTEN PERMISSION SHALL INCLUDE LANGUAGE INDICATING THAT SECTION 10.101 OF THE MADISON GENERAL ORDINANCES AND ADMINISTRATIVE PROCEDURE MEMORANDUM NO. 6-2, REFERRING TO NOTIFICATION OF PROPERTY OCCUPANTS AND/OR OWNERS, HAS BEEN COMPLIED WITH.

DEVELOPER MUST SUBMIT A TRAFFIC CONTROL PLAN TO CITY TRAFFIC ENGINEERING AT LEAST 14 DAYS PRIOR TO THE START OF WORK. WORK SHALL NOT PROCEED UNTIL AN APPROVED TRAFFIC CONTROL PLAN IS IN PLACE.

CONTACT THE PROJECT ENGINEER AND DESIGNER, FADI EL MUSA, AT FELMUSAGONZALEZ@CITYOFMADISON.COM FOR CAD FILES AND ALIGNMENT DATA PRIOR TO STAKING.

CONVENTIONAL SIGNS

FIELD VERIFY ALL UTILITY LOCATIONS

GAS

STORM SEWER

SANITARY SEWER

WATER

BURIED ELECTRIC

OVERHEAD ELECTRIC

POWER POLE

ADA COMPLIANT RAMP W/
DETECTABLE WARNING FIELD

COMBUSTIBLE FLUIDS

— G —


— ST —


— SAN —


— W —

— E —

— OH —







UNDERDRAINS SHALL BE INSTALLED PER STANDARD DETAIL DRAWING 4.05 FOR 75' ON EACH SIDE OF THE LOW POINT, OR TO THE NEAREST CURB HIGH POINT. ALL UNDERDRAIN SHALL BE WRAPPED.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADE OF 0.5% TOWARD STORM SEWER INLETS.

ALL DITCHES SHALL DRAIN WITH A MINIMUM GRADE OF 0.5%

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL TYPICALLY BE 1.5%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.04. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENTS FROM THE CITY PROJECT ENGINEER PRIOR TO STAKING THIS PROJECT. THE CITY PROJECT ENGINEER IS FADI EL MUSA, EMAIL: FELMUSAGONZALEZ@CITYOFMADISON.COM

CURB STATION AND OFFSETS SHALL BE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK UNLESS OTHERWISE INDICATED ON THE PLANS.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS RIGHT-OF-WAY PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO S.D.D 3.05.

ALL WORK IN THE RIGHT OF WAY AND PUBLIC EASEMENTS SHALL BE IN ACCORDANCE WITH THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION LATEST EDITION.

ALL PROPOSED IMPROVEMENTS IN THE RIGHT-OF-WAY ARE BASED ON SURVEY DATA PROVIDED BY THE DEVELOPER OR ITS CONSULTANT. IN THE CASE THAT THE PROPOSED IMPROVEMENTS CAN NOT BE INSTALLED PER THESE PLANS OR THE CITY OF MADISON STANDARD SPECIFICATION FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION, THE DEVELOPER SHALL BE RESPONSIBLE FOR THE COST OF EXTRA WORK DUE TO INACCURACIES OF THE SURVEY.


ALL UTILITY VERIFICATIONS AND ACCURACY OF THE DRAWINGS ARE THE RESPONSIBILITY OF THE DEVELOPER. ANY CONFLICTS THAT ARISE FROM MISSING OR ERRONEOUS INFORMATION WILL BE AT THE EXPENSE OF THE DEVELOPER. NO PRECAST STRUCTURES WILL BE APPROVED FOR STORM OR SANITARY SEWER UNTIL ALL POTENTIAL UTILITY CONFLICTS ARE RESOLVED.

IN LOCATIONS WHERE PAVEMENT RESTORATION IS NOT IDENTIFIED ADJACENT TO CURB AND GUTTER REPLACEMENT, IT IS ASSUMED THAT CURB AND GUTTER WILL BE REPLACED WITHOUT DAMAGING ADJACENT PAVEMENT. IF DAMAGED, THE MILL AND OVERLAY LIMITS SHALL BE EXTENDED BY THE CITY CONSTRUCTION ENGINEER AS NECESSARY TO MEET THE STANDARD PATCHING CRITERIA.

THE DEVELOPER SHALL FOLLOW THE WDNr-APPROVED, NOVEMBER 17,2021 MATERIALS MANAGEMENT PLAN PREPARED BY SCS FOR THE HANDLING OF ANY CONTAMINATION ENCOUNTERED WHILE WORKING ON PRIVATE LAND OR IN THE PUBLIC RIGHT-OF-WAY.

STANDARD NOTES

3914 MONONA DRIVE



14775

D-1

14775

MADISON, WI

9242

CONTRACT NO:

MARK

REFUSION

DATE

BY

14775

7/19/2023 11:23 AM

14775

D-1

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*SEE X-SHEETS FOR CROSS SLOPES

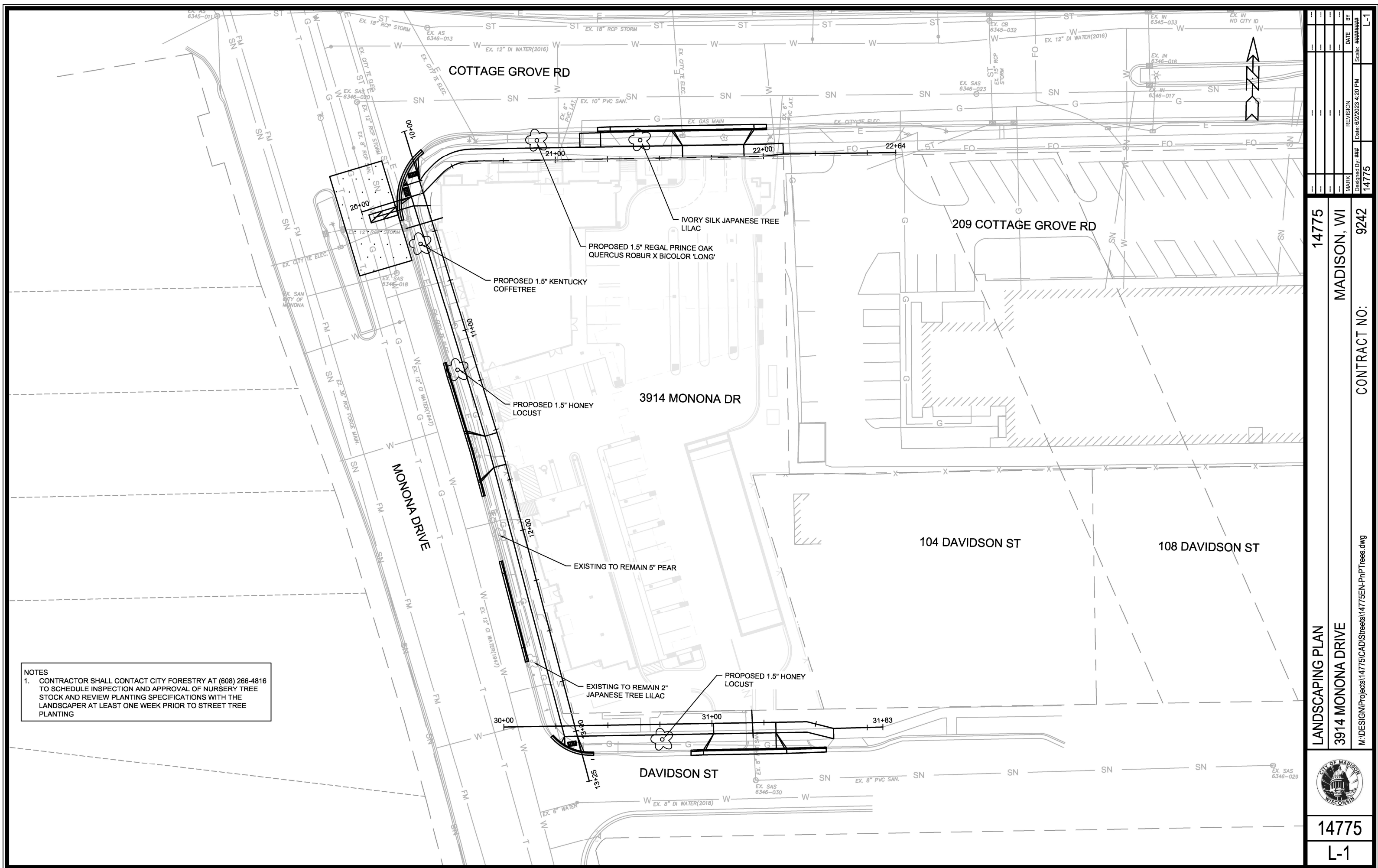


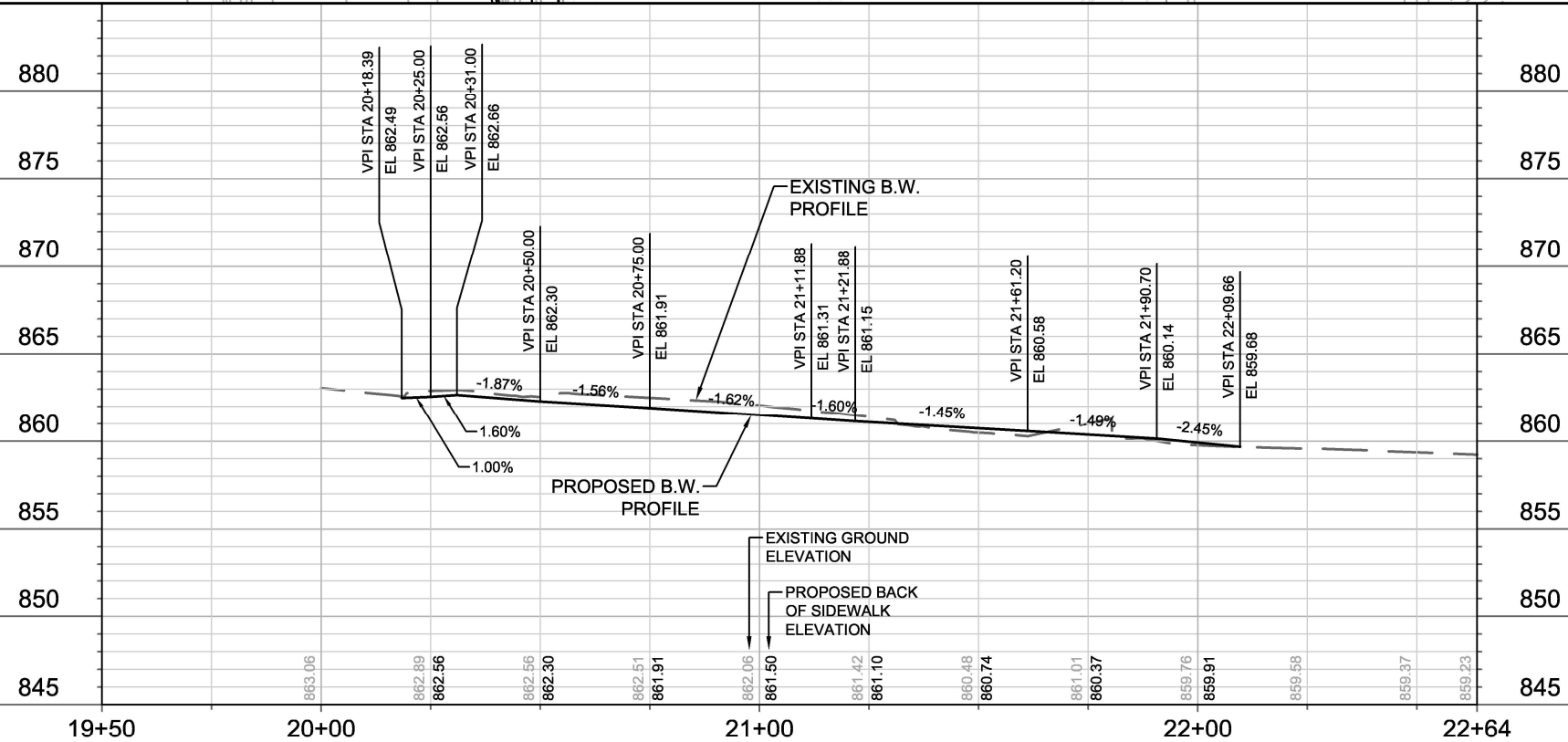
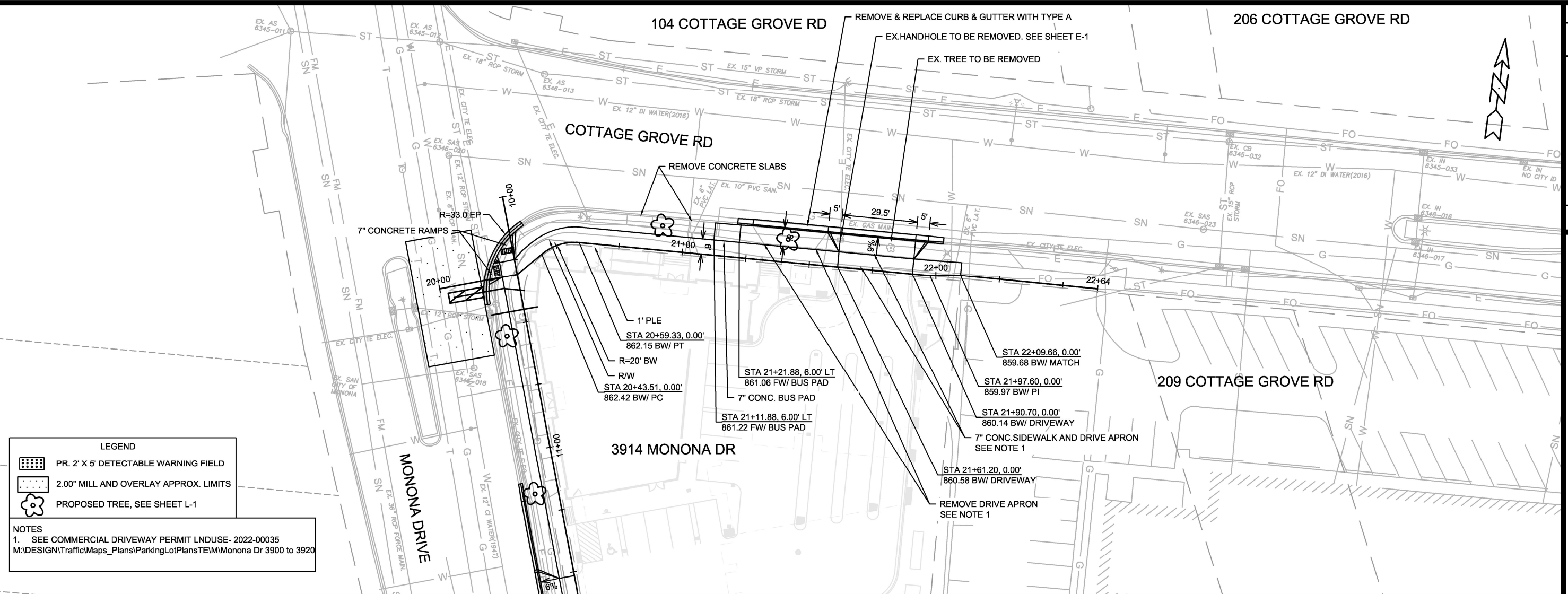
- ① POINT REFERRED TO ON PROFILE.
- ② 5" CONCRETE SIDEWALK, 7" AT DRIVEWAYS.
- ③ 6" TOPSOIL, SEED AND MAT.
- ④ EXISTING FILL TO REMAIN, INSTALL AT CURB REPLACEMENT LOCATIONS.
- ⑤ EXISTING CURB & GUTTER TYPE A, REPLACE AT LOCATIONS SHOWN IN P SHEETS OR AS DETERMINED BY THE ENGINEER.
- ⑥ EXISTING ASPHALTIC PAVEMENT TO REMAIN.
- ⑦ EXISTING ROAD BASE TO REMAIN.
- ⑧ EXISTING CURB & GUTTER TYPE G, REPLACE AT LOCATIONS SHOWN IN P SHEETS OR AS DETERMINED BY THE ENGINEER.

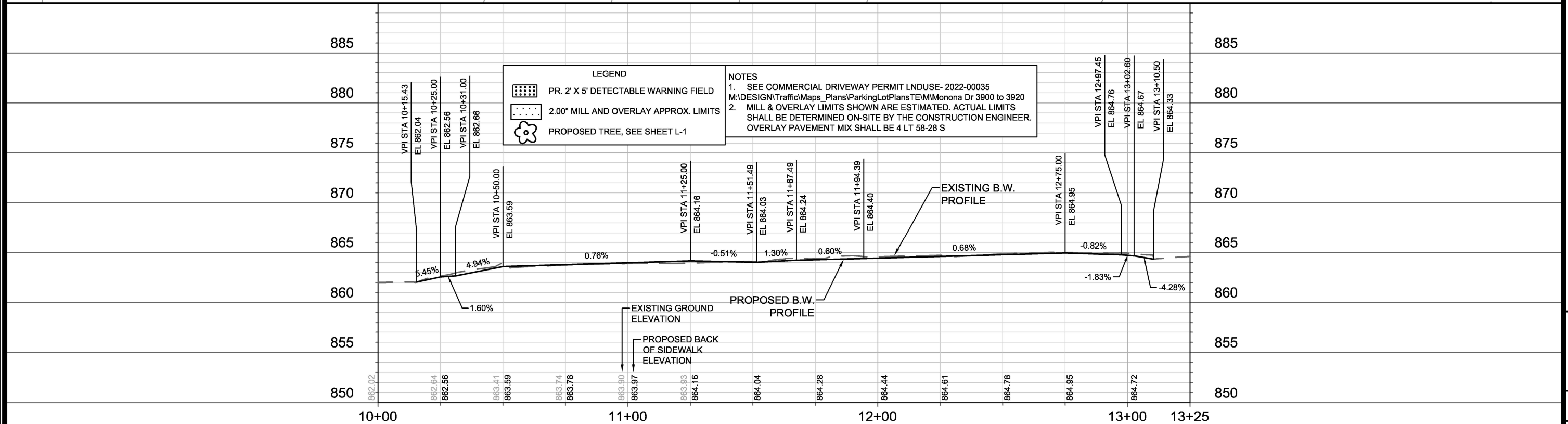
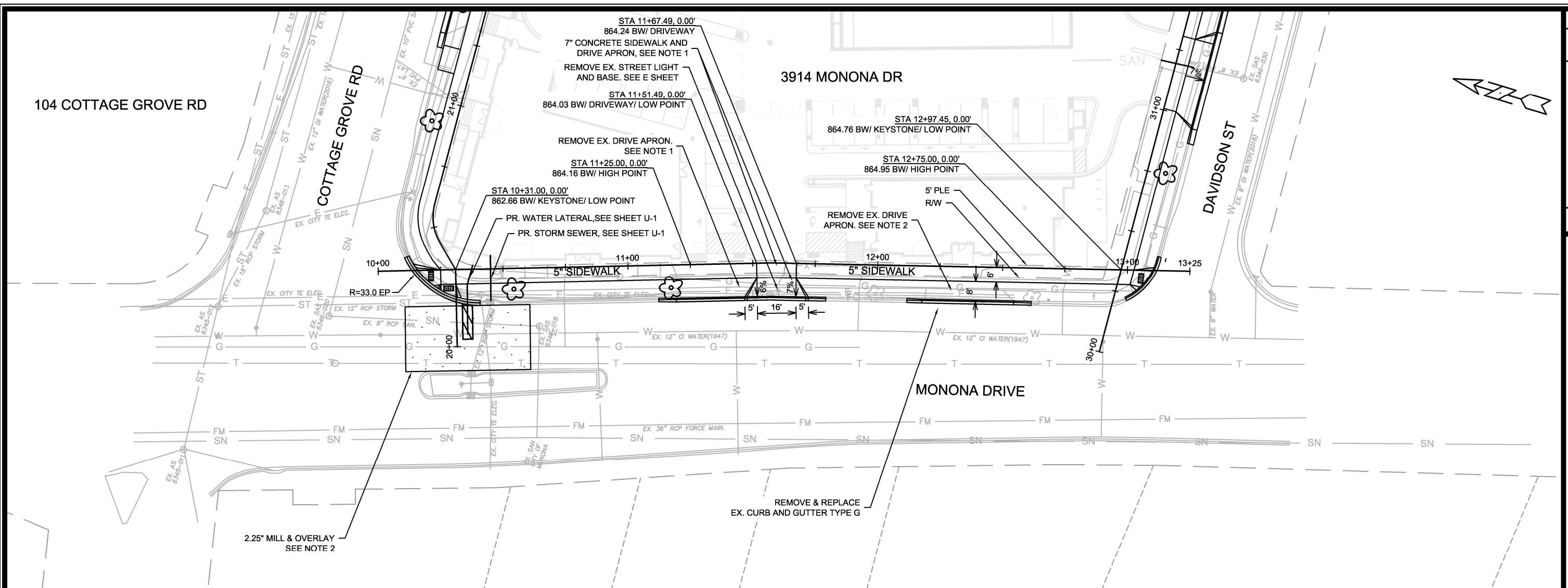
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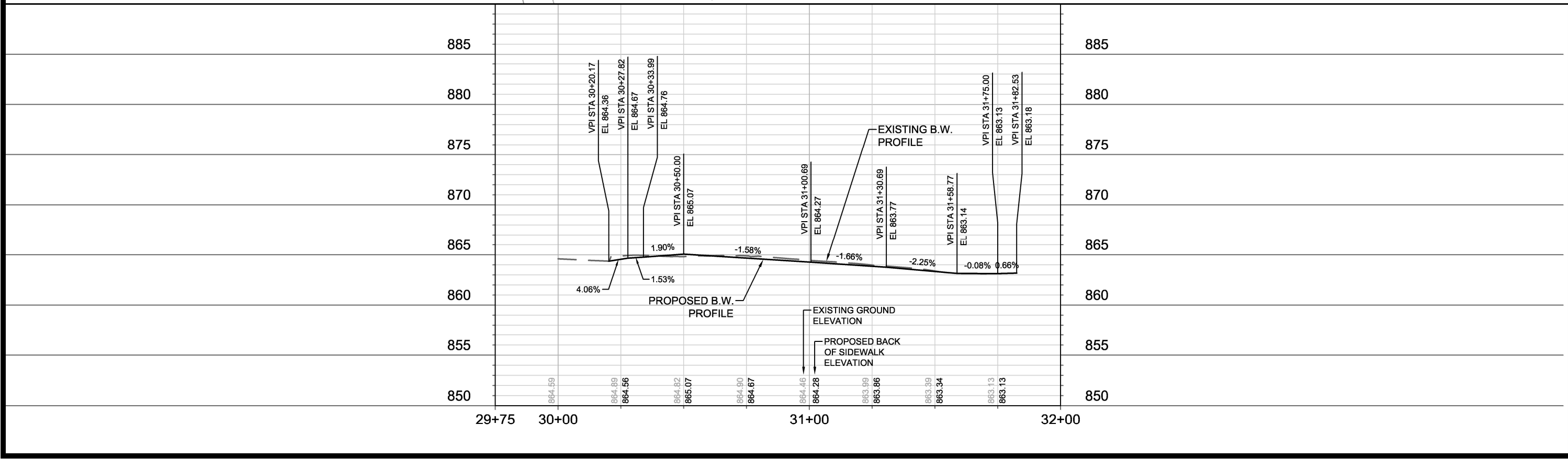
1. ALL LOCATIONS ARE APPROXIMATE. TRAFFIC ENGINEER SHALL APPROVE FINAL LOCATIONS INCLUDING SETBACK IN THE FIELD. AFTER CONTRACTOR SURVEYS STAKING, THE CONTRACTOR SHALL NOTIFY GRETCHEN AVILES PINEIRO (266-4899), AT LEAST 24-HOURS IN ADVANCE OF NEEDED BASE LOCATION MARKED.
2. THE CONTRACTOR SHALL DO ALL WORK IN ACCORDANCE WITH CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION 2023 EDITION AND ALL ADDENDUMS THERETO.
WWW.CITYOFMADISON.COM/ENGINEERING/DOCUMENTS/STANDARD-SPECIFICATIONS/COMPLETESPECBOOK.PDF
3. ALL CONDUIT SHALL BE PVC SCHEDULE 80 UNDER PAVEMENT OR SCHEDULE 40 OTHERWISE. PULL WIRE REQUIRED AS PER STANDARD SPECIFICATIONS.
4. THE CONTRACTOR SHALL CALL TROY VANT (395-1975) AT THE TRAFFIC ENGINEERING SHOP AT LEAST 24-HOURS IN ADVANCE OF POURING BASES AND BURYING CONDUIT TO ARRANGE FOR INSPECTION.
5. THE CONTRACTOR SHALL CALL TROY VANT (395-1975) PRIOR TO REMOVING OR CONNECTING TO EXISTING CONDUITS OR HANDHOLES. CONTRACTOR SHALL FIELD VERIFY CONDUIT MATERIAL & SIZE.
6. LOOPS TO BE INSTALLED BY CITY TE. COORDINATE WITH TE INSPECTOR TO SCHEDULE LOOP INSTALLATION.
7. THE CONTRACTOR SHALL ARRANGE FOR PICKUP OF THE FOLLOWING CITY MATERIALS:
1"X40" ANCHOR BOLTS: 1 SET OF 4 FOR LB-3R BASE




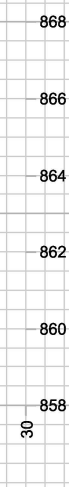
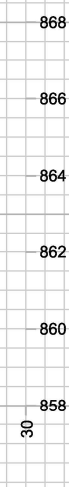
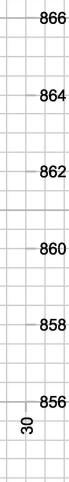
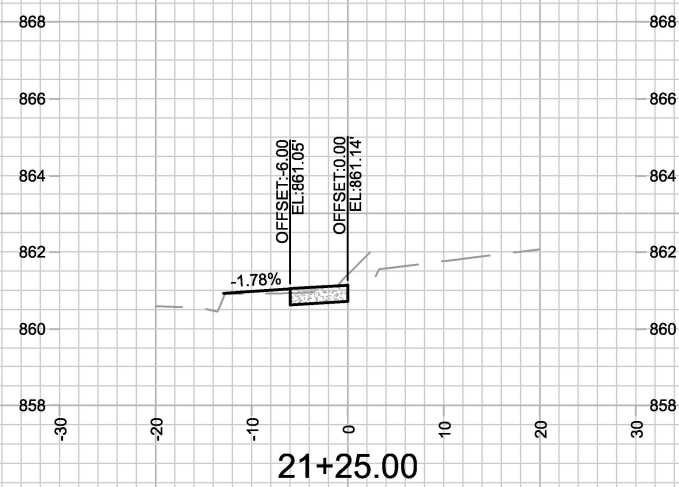
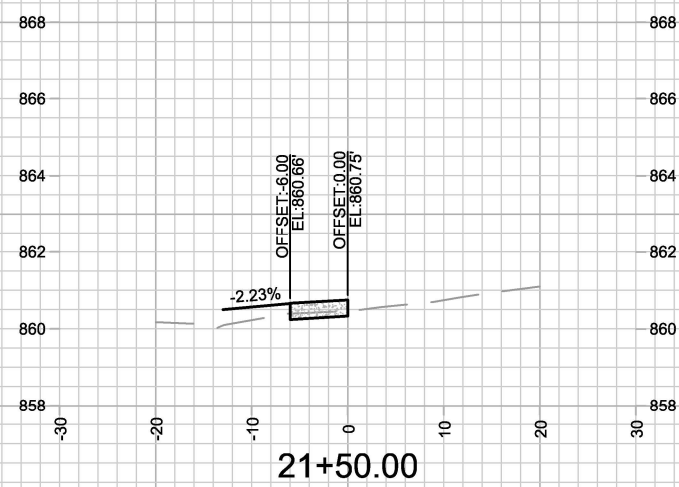
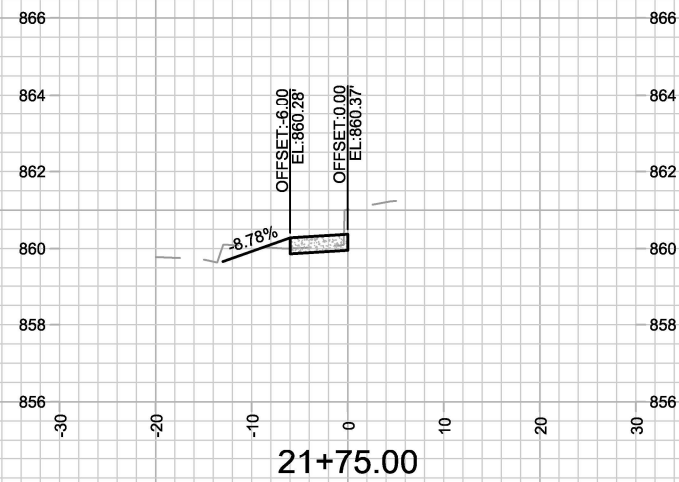
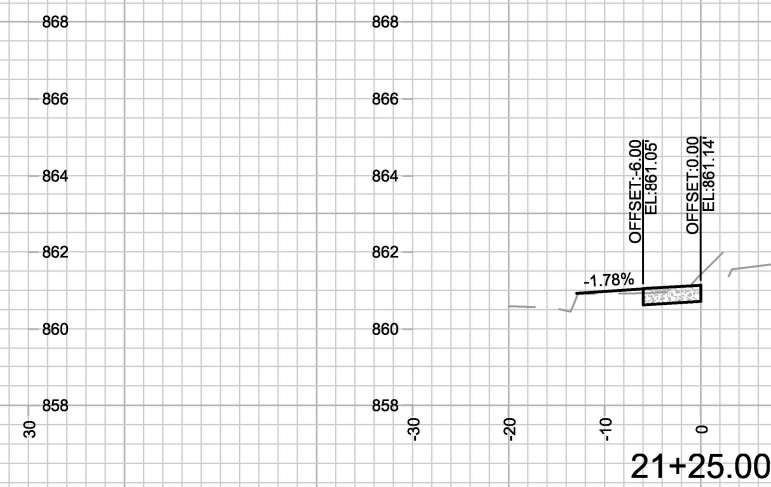
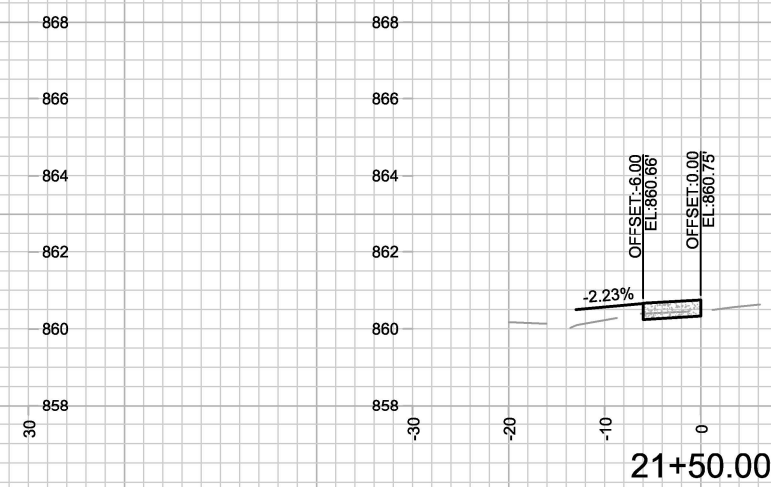
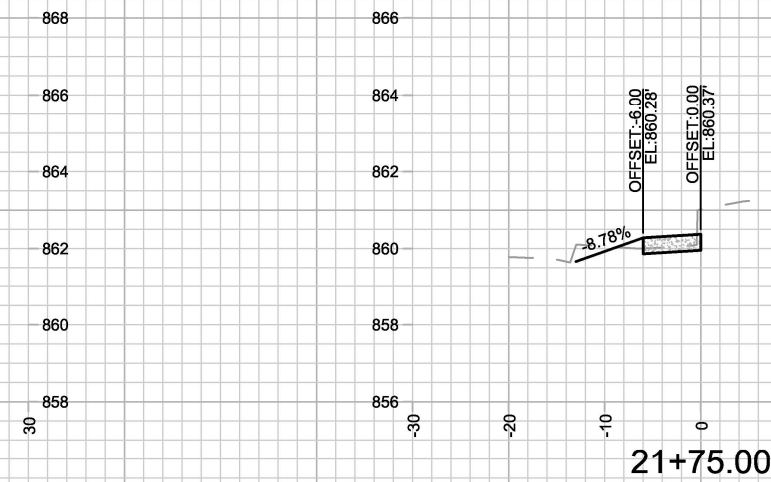
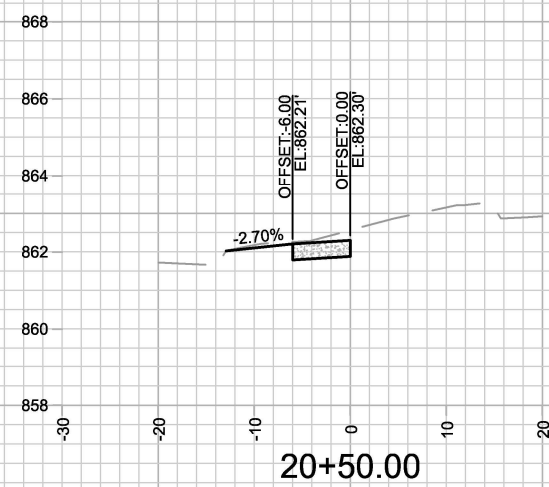
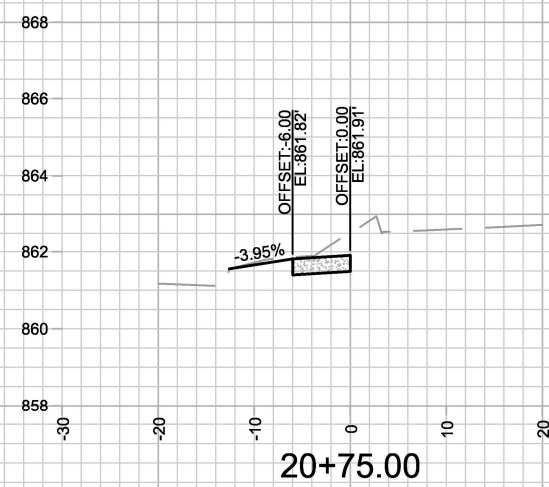
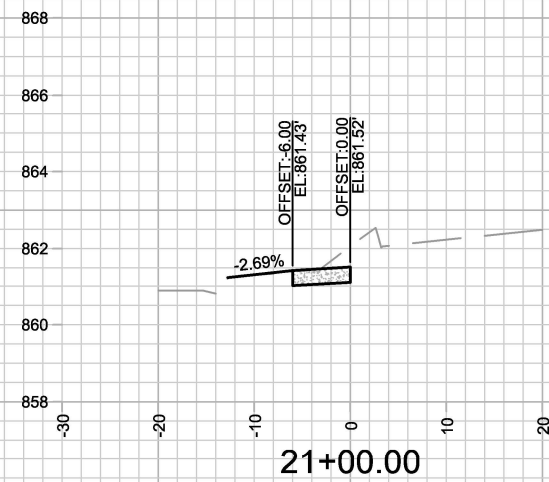




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		DATE	BY
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X-1

CROSS SECTIONS - COTTAGE GROVE ROAD

3914 MONONA DRIVE

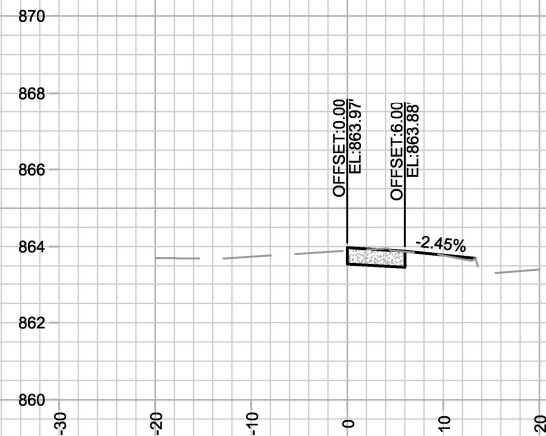
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MADISON, WI

CONTRACT NO: 9242

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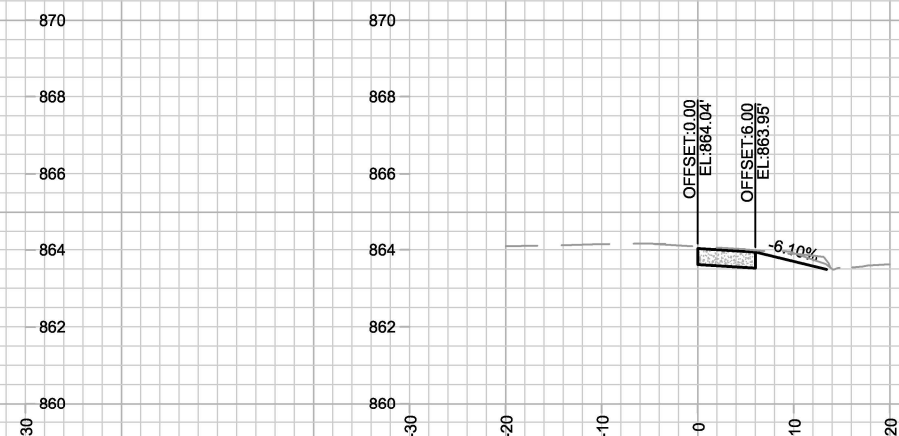
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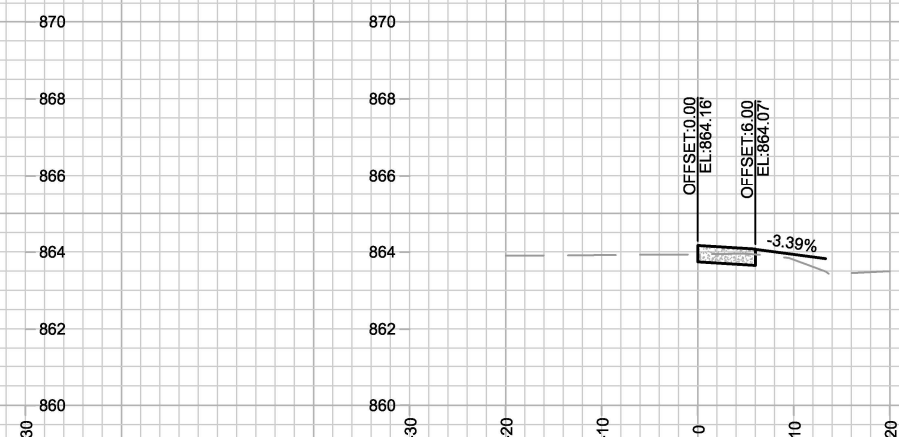
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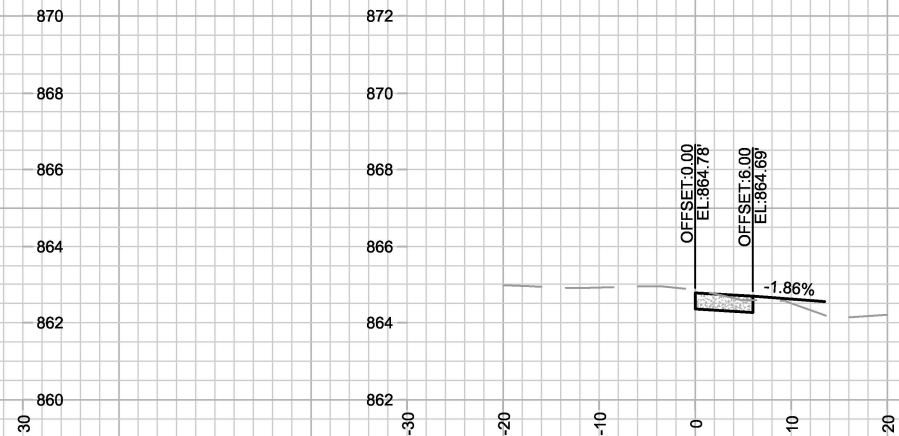
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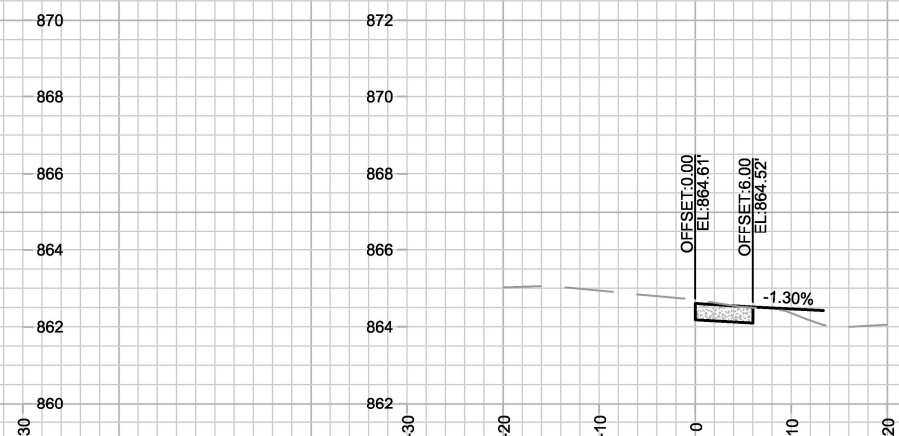
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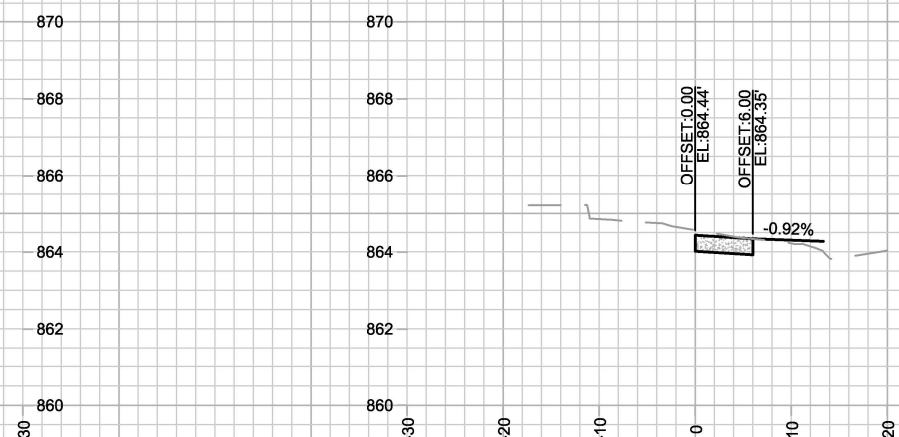
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	CROSS SECTIONS - MONONA DRIVE		14775		
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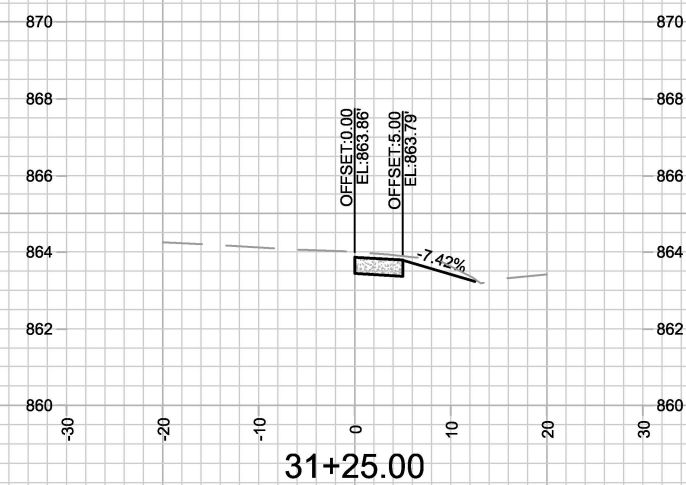
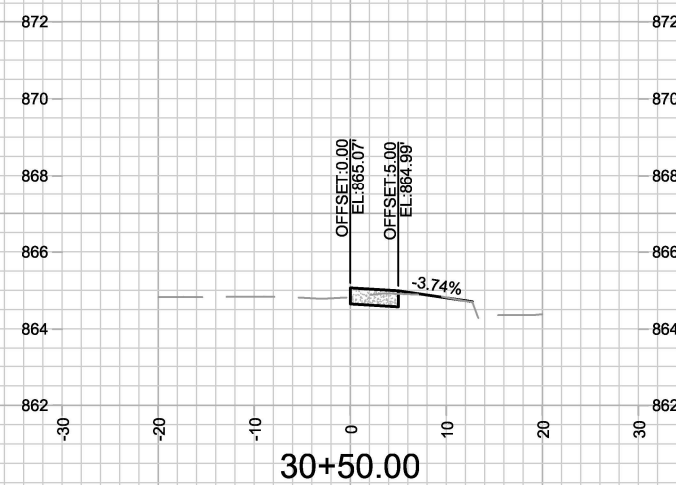
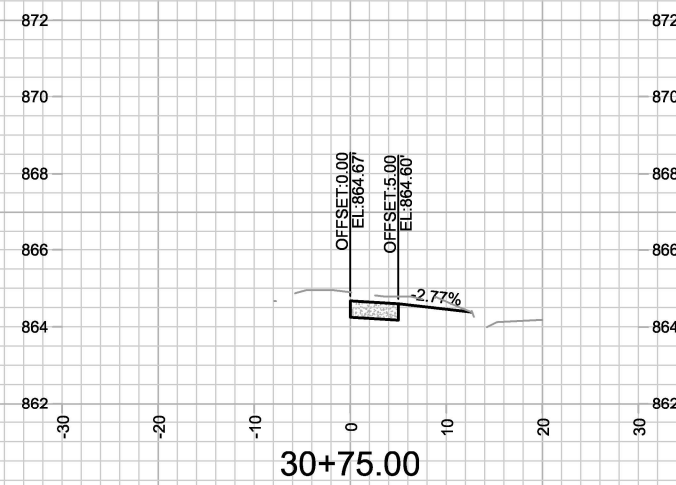
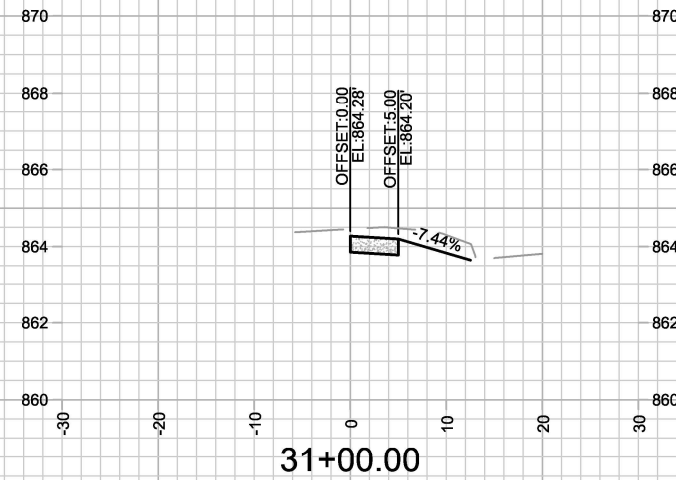


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