



Madison, Wisconsin

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NO MAINTENANCE PLAN REQUIRED		

CITY OF MADISON

CITY ENGINEERING DIVISION

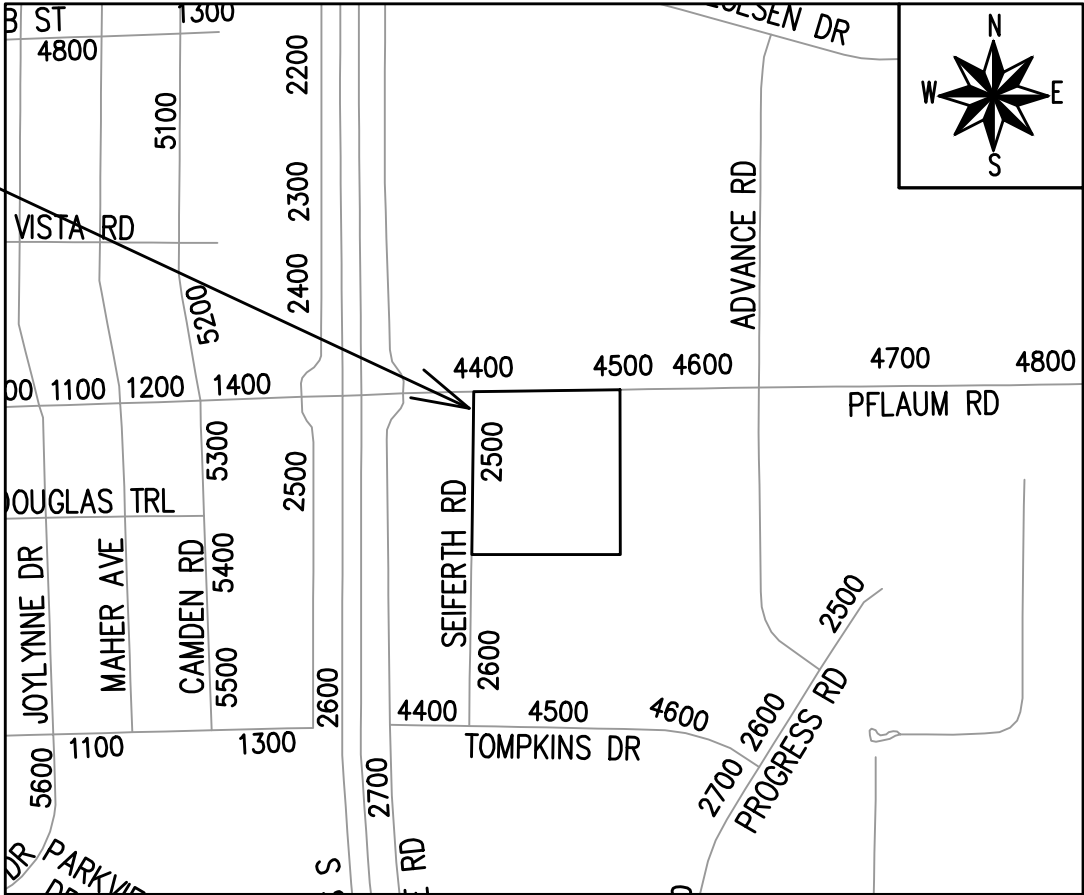
DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

2503 SEIFERTH ROAD

CITY PROJECT NO. 14925  
CONTRACT NO. 9376

PROJECT  
LOCATION



REVISION 1: 10/29/2024  
- REVISED SHEETS: U1-U3

PUBLIC IMPROVEMENT PROJECT  
APPROVED

APPROVED DATE: NOVEMBER 21, 2023

BY THE COMMON COUNCIL  
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN  
APPROVED BY:

Chris Potykowski Feb 27, 2024

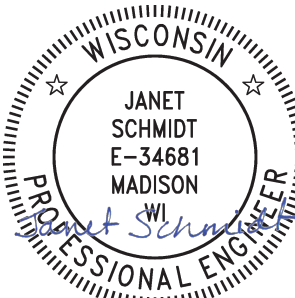
City Engineer Date

STREET  
DESIGNED BY:



Feb 22, 2024

STORM SEWER  
DESIGNED BY:



Feb 22, 2024

WATER  
DESIGNED BY:

LATERAL ONLY

THE LOCATION AND INFORMATION FOR PROPOSED NEW TREES, IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS ARE APPROXIMATE AND ARE SHOWN FOR REFERENCE ONLY. THE LOCATIONS, SPECIFICATIONS AND PLANTING METHODS OF ALL PROPOSED NEW OR REPLACEMENT TREES IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE APPROVED BY THE CITY FORESTER PRIOR TO INSTALLATION.

NO TREES IN THE RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE TRIMMED, PRUNED, REMOVED OR ADVERSELY AFFECTED IN ANY WAY UNTIL THE DEVELOPER HAS RECEIVED WRITTEN PERMISSION FROM THE CITY ENGINEER OR CITY FORESTER. SAID WRITTEN PERMISSION SHALL INCLUDE LANGUAGE INDICATING THAT SECTION 10.101 OF THE MADISON GENERAL ORDINANCES AND ADMINISTRATIVE PROCEDURE MEMORANDUM NO. 6-2, REFERRING TO NOTIFICATION OF PROPERTY OCCUPANTS AND/OR OWNERS, HAS BEEN COMPLIED WITH.

DEVELOPER MUST SUBMIT A TRAFFIC CONTROL PLAN TO CITY TRAFFIC ENGINEERING AT LEAST 14 DAYS PRIOR TO THE START OF WORK. WORK SHALL NOT PROCEED UNTIL AN APPROVED TRAFFIC CONTROL PLAN IS IN PLACE.

CONTACT THE PROJECT ENGINEER AND DESIGNER, GRECIA IZQUIERDO, AT GIZQUIERDO@CITYOFMADISON.COM FOR CAD FILES AND ALIGNMENT DATA PRIOR TO STAKING.

CONVENTIONAL SIGNS

FIELD VERIFY ALL UTILITY LOCATIONS

GAS

STORM SEWER

SANITARY SEWER

WATER

BURIED ELECTRIC

OVERHEAD ELECTRIC

POWER POLE

ADA COMPLIANT RAMP W/  
DETECTABLE WARNING FIELD

COMBUSTIBLE FLUIDS

— G —

— ST —

— SAN —

— W —

— E —

— OH —

UNDERDRAINS SHALL BE INSTALLED PER STANDARD DETAIL DRAWING 4.05 FOR 75' ON EACH SIDE OF THE LOW POINT, OR TO THE NEAREST CURB HIGH POINT. ALL UNDERDRAIN SHALL BE WRAPPED.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADE OF 0.5% TOWARD STORM SEWER INLETS.

ALL DITCHES SHALL DRAIN WITH A MINIMUM GRADE OF 0.5%

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL TYPICALLY BE 1.5%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.04. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENTS FROM THE CITY PROJECT ENGINEER PRIOR TO STAKING THIS PROJECT. THE CITY PROJECT ENGINEER IS GRECIA IZQUIERDO, EMAIL: GIZQUIERDO@CITYOFMADISON.COM

CURB STATION AND OFFSETS SHALL BE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK UNLESS OTHERWISE INDICATED ON THE PLANS.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS RIGHT-OF-WAY PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO S.D.D 3.05.

ALL WORK IN THE RIGHT OF WAY AND PUBLIC EASEMENTS SHALL BE IN ACCORDANCE WITH THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION LATEST EDITION.

ALL PROPOSED IMPROVEMENTS IN THE RIGHT-OF-WAY ARE BASED ON SURVEY DATA PROVIDED BY THE DEVELOPER OR ITS CONSULTANT. IN THE CASE THAT THE PROPOSED IMPROVEMENTS CAN NOT BE INSTALLED PER THESE PLANS OR THE CITY OF MADISON STANDARD SPECIFICATION FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION, THE DEVELOPER SHALL BE RESPONSIBLE FOR THE COST OF EXTRA WORK DUE TO INACCURACIES OF THE SURVEY.

ALL UTILITY VERIFICATIONS AND ACCURACY OF THE DRAWINGS ARE THE RESPONSIBILITY OF THE DEVELOPER. ANY CONFLICTS THAT ARISE FROM MISSING OR ERRONEOUS INFORMATION WILL BE AT THE EXPENSE OF THE DEVELOPER. NO PRECAST STRUCTURES WILL BE APPROVED FOR STORM OR SANITARY SEWER UNTIL ALL POTENTIAL UTILITY CONFLICTS ARE RESOLVED.

IN LOCATIONS WHERE PAVEMENT RESTORATION IS NOT IDENTIFIED ADJACENT TO CURB AND REPLACEMENT, IT IS ASSUMED THAT CURB AND GUTTER WILL BE REPLACED WITHOUT DAMAGING ADJACENT PAVEMENT. IF DAMAGED, THE MILL AND OVERLAY LIMITS SHALL BE EXTENDED BY THE CITY CONSTRUCTION ENGINEER AS NECESSARY TO MEET THE STANDARD PATCHING CRITERIA.

14925

MADISON, WI

9376

CONTRACT NO:

STANDARD NOTES

2503 SEIFERTH ROAD

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14925

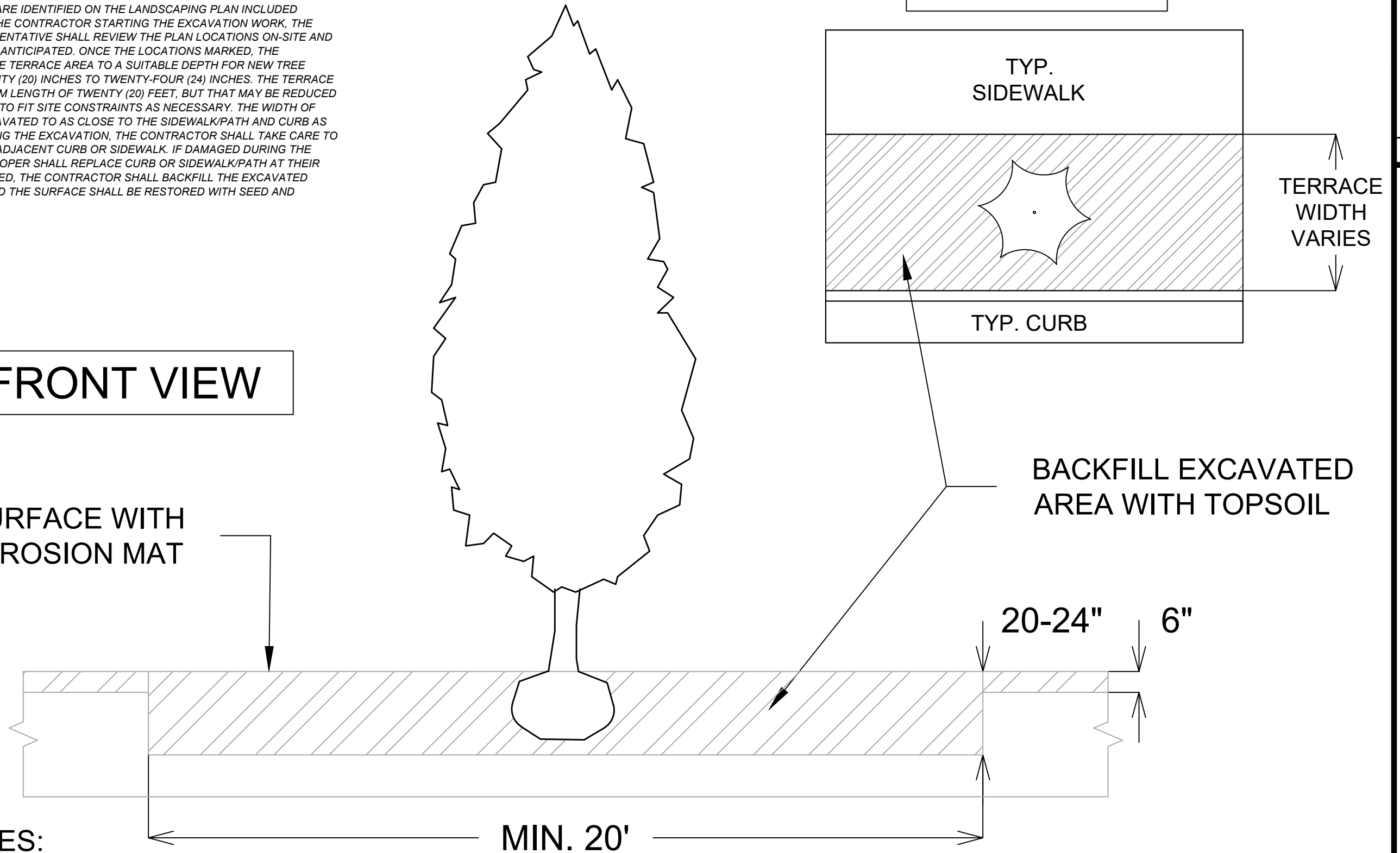
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THIS INCLUDES ALL WORK NECESSARY TO EXCAVATE AREAS WITHIN GRASS TERRACES TO AN APPROPRIATE DEPTH SUITABLE FOR NEW TREE PLANTINGS. ALL WORK SHALL BE IN ACCORDANCE WITH ARTICLE 201 OF THE STANDARD SPECIFICATIONS. THE PROPOSED LOCATIONS FOR TREE PLANTINGS ARE IDENTIFIED ON THE LANDSCAPING PLAN INCLUDED WITHIN THIS PLAN SET. PRIOR TO THE CONTRACTOR STARTING THE EXCAVATION WORK, THE ENGINEER AND FORESTRY REPRESENTATIVE SHALL REVIEW THE PLAN LOCATIONS ON-SITE AND CONFIRM THAT NO CONFLICTS ARE ANTICIPATED. ONCE THE LOCATIONS MARKED, THE CONTRACTOR SHALL EXCAVATE THE TERRACE AREA TO A SUITABLE DEPTH FOR NEW TREE PLANTINGS, APPROXIMATELY TWENTY (20) INCHES TO TWENTY-FOUR (24) INCHES. THE TERRACE SHALL BE EXCAVATED TO A MINIMUM LENGTH OF TWENTY (20) FEET, BUT THAT MAY BE REDUCED BY THE CONSTRUCTION ENGINEER TO FIT SITE CONSTRAINTS AS NECESSARY. THE WIDTH OF THE TERRACE SHALL ALSO BE EXCAVATED TO AS CLOSE TO THE SIDEWALK/PATH AND CURB AS IS REASONABLE. WHILE PERFORMING THE EXCAVATION, THE CONTRACTOR SHALL TAKE CARE TO NOT UNDERMINE OR DAMAGE THE ADJACENT CURB OR SIDEWALK. IF DAMAGED DURING THE EXCAVATION PROCESS, THE DEVELOPER SHALL REPLACE CURB OR SIDEWALK/PATH AT THEIR COST. ONCE THE AREA IS EXCAVATED, THE CONTRACTOR SHALL BACKFILL THE EXCAVATED AREA WITH REGULAR TOPSOIL, AND THE SURFACE SHALL BE RESTORED WITH SEED AND EROSION MAT.

## RESTORE SURFACE WITH SEED AND EROSION MAT

REFER TO S.D.D. 2.01 FOR  
STANDARD PLANTING TECHNIQUE  
FOR TREES IN TURF AREAS.



## TOP VIEW

TYP.  
SIDEWALK

TYP. CURB

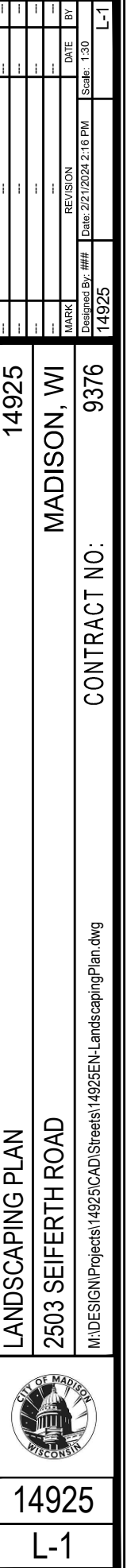
# TERRACE WIDTH VARIES

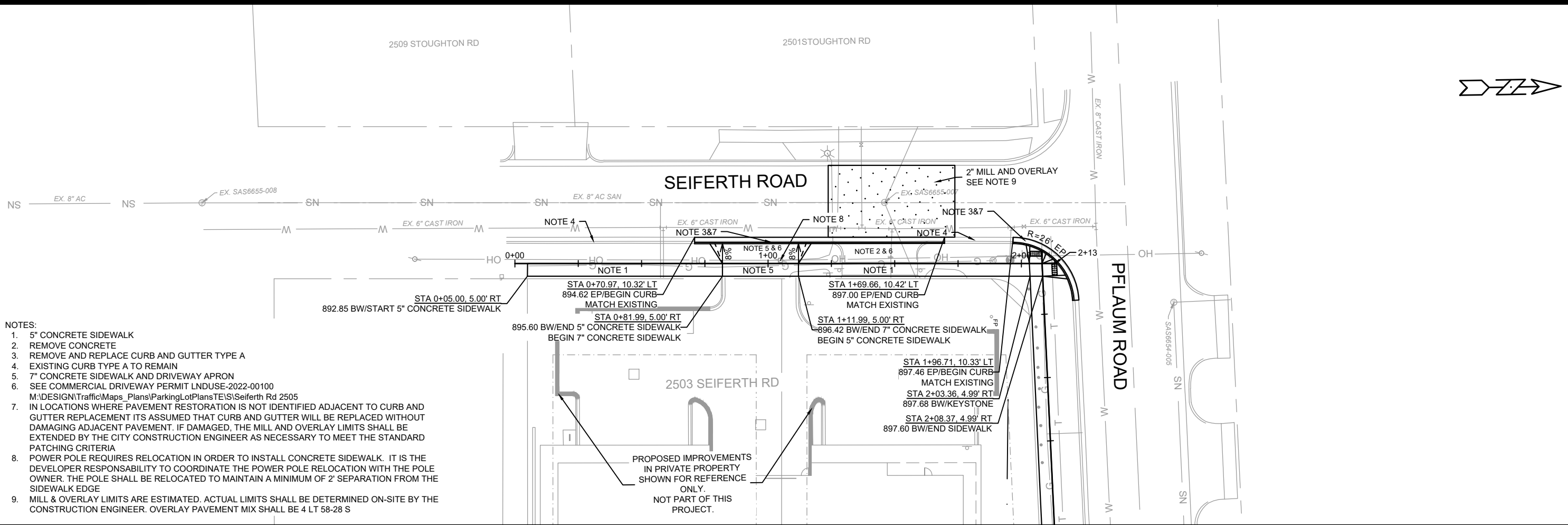
BACKFILL EXCAVATED  
AREA WITH TOPSOIL

20-24"

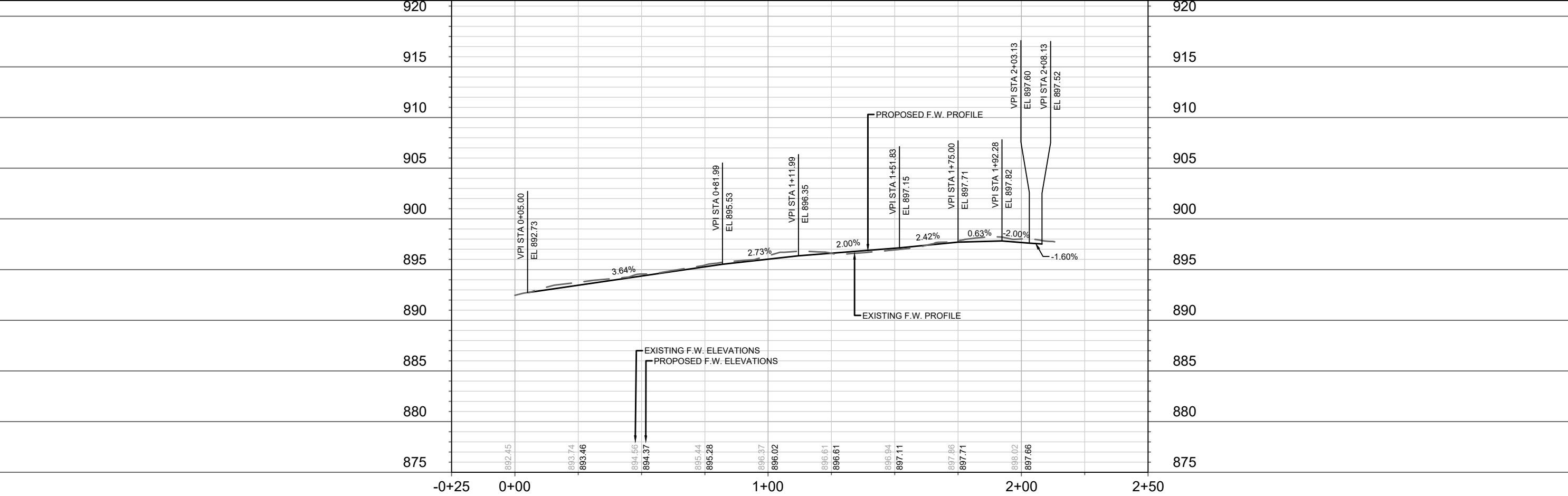
6"

**MIN. 20'**

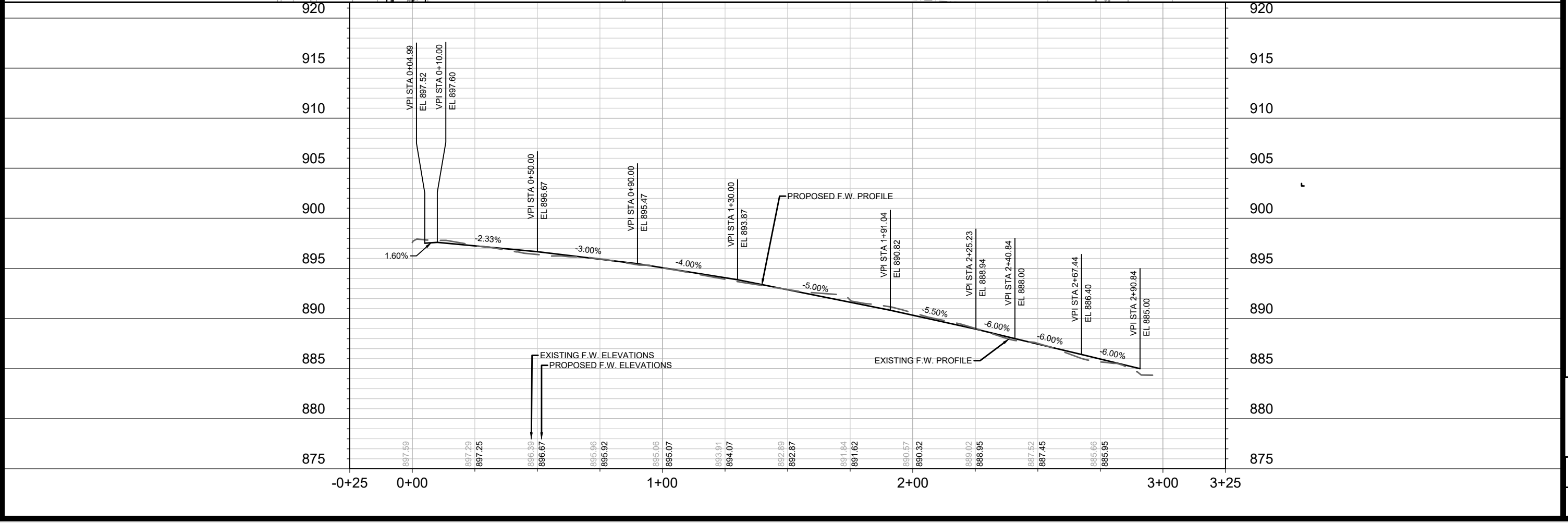





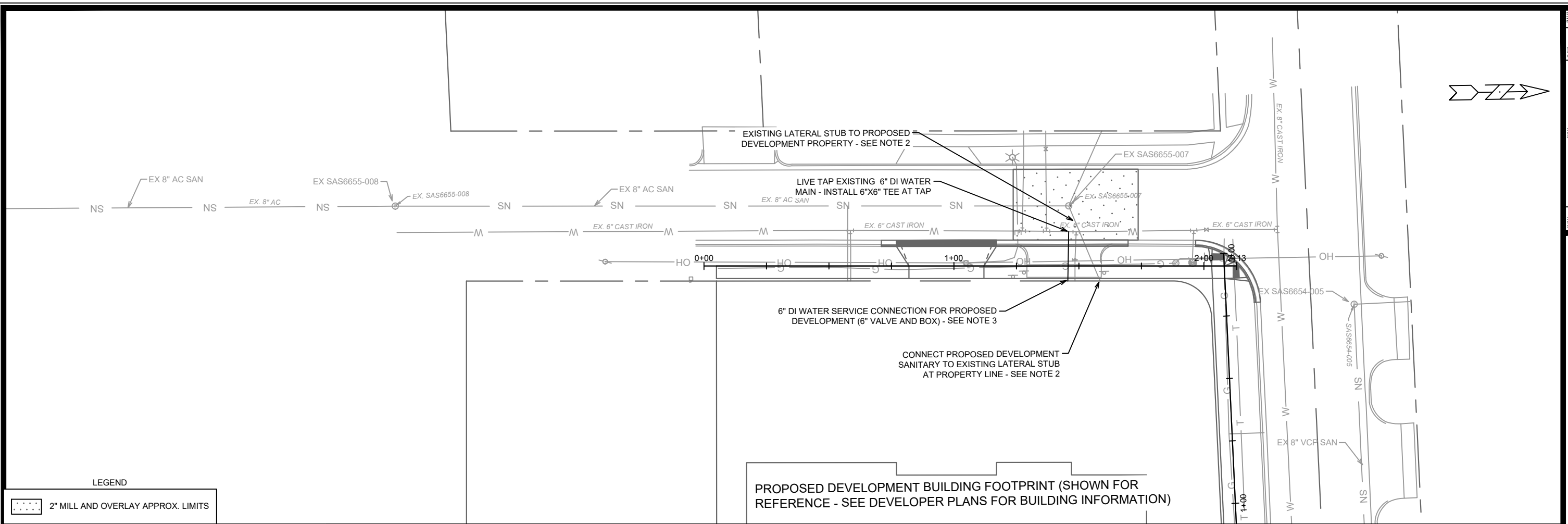
- NOTES:
1. 5" CONCRETE SIDEWALK
  2. REMOVE CONCRETE
  3. REMOVE AND REPLACE CURB AND GUTTER TYPE A
  4. EXISTING CURB TYPE A TO REMAIN
  5. 7" CONCRETE SIDEWALK AND DRIVEWAY APRON
  6. SEE COMMERCIAL DRIVEWAY PERMIT LNDUSE-2022-00100
  7. IN LOCATIONS WHERE PAVEMENT RESTORATION IS NOT IDENTIFIED ADJACENT TO CURB AND GUTTER REPLACEMENT ITS ASSUMED THAT CURB AND GUTTER WILL BE REPLACED WITHOUT DAMAGING ADJACENT PAVEMENT. IF DAMAGED, THE MILL AND OVERLAY LIMITS SHALL BE EXTENDED BY THE CITY CONSTRUCTION ENGINEER AS NECESSARY TO MEET THE STANDARD PATCHING CRITERIA
  8. POWER POLE REQUIRES RELOCATION IN ORDER TO INSTALL CONCRETE SIDEWALK. IT IS THE DEVELOPER RESPONSABILITY TO COORDINATE THE POWER POLE RELOCATION WITH THE POLE OWNER. THE POLE SHALL BE RELOCATED TO MAINTAIN A MINIMUM OF 2' SEPARATION FROM THE SIDEWALK EDGE
  9. MILL & OVERLAY LIMITS ARE ESTIMATED. ACTUAL LIMITS SHALL BE DETERMINED ON-SITE BY THE CONSTRUCTION ENGINEER. OVERLAY PAVEMENT MIX SHALL BE 4 LT 58-28 S



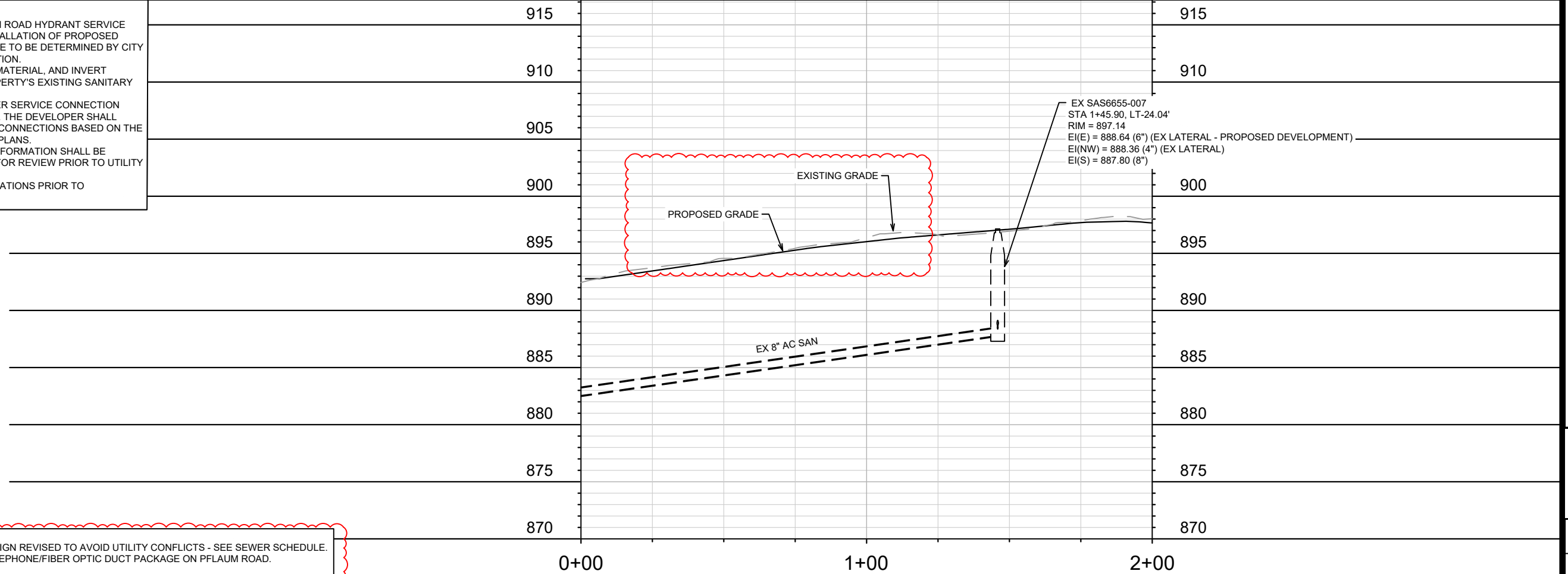
14925		MADISON, WI		CONTRACT NO: 9376	
PLAN & PROFILE - SEIFERTH ROAD		2503 SEIFERTH ROAD		CITY OF MADISON WISCONSIN	
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14925		P-1		P-1	




	14925	PLAN & PROFILE - PFLAUM ROAD		14925
	P-2	2503 SEIFERTH ROAD		
		MADISON, WI		
		CONTRACT NO: 9376		
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		14925		P-2



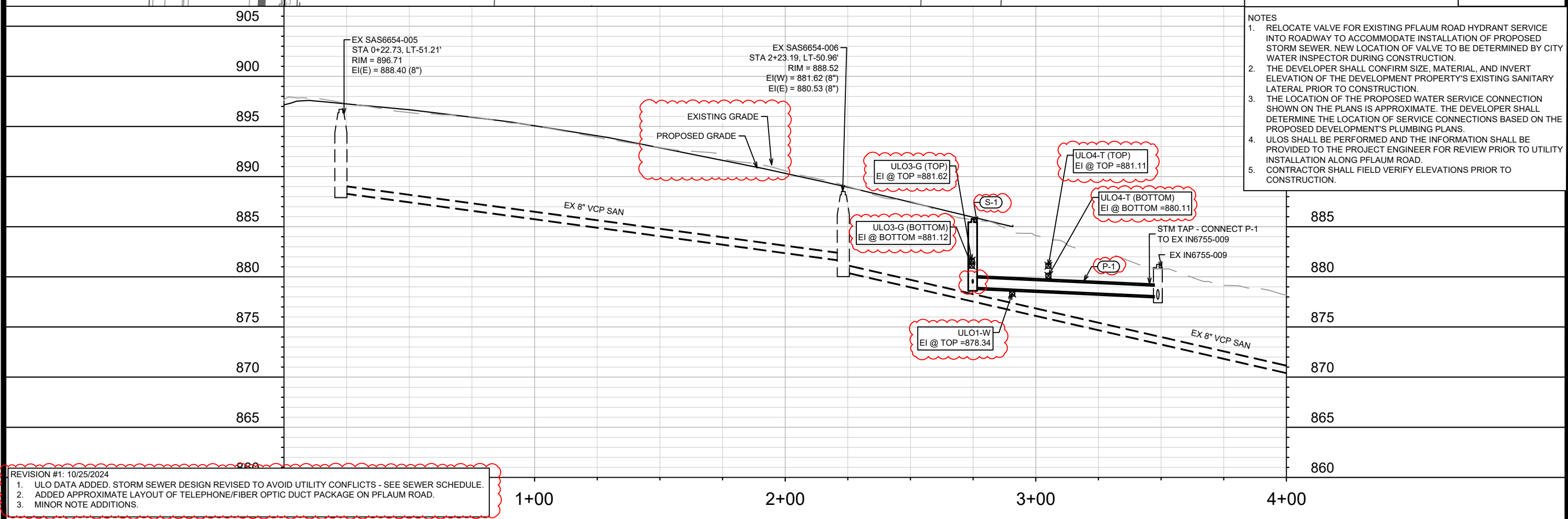
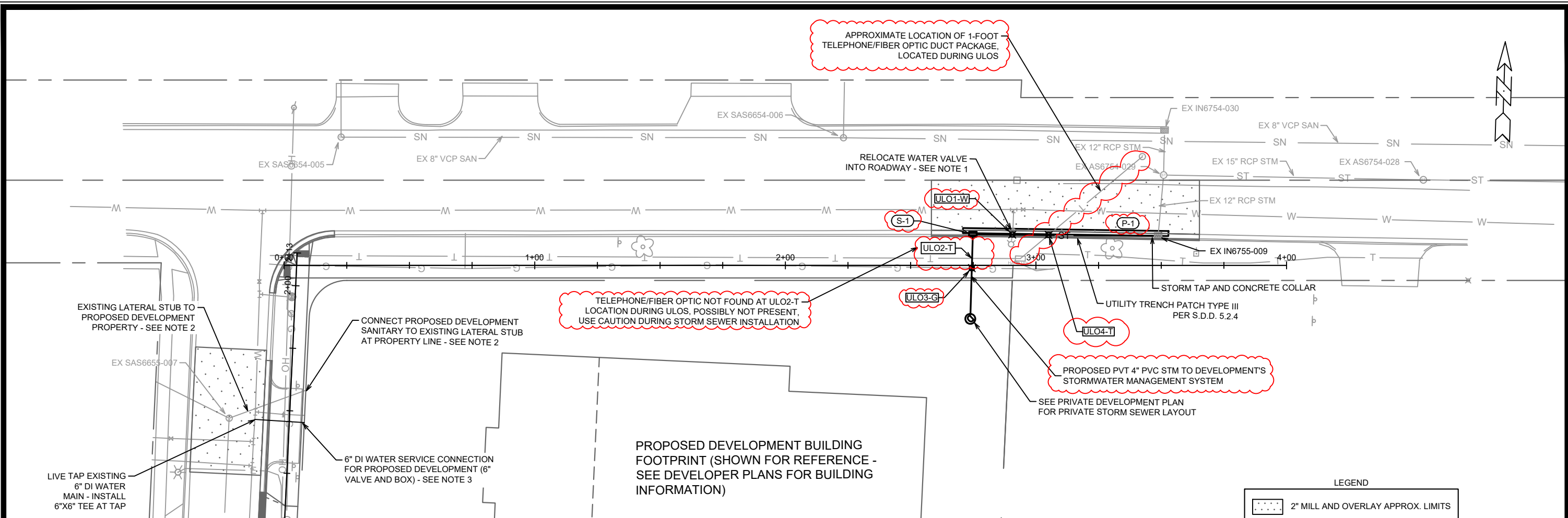
- NOTES**
1. RELOCATE VALVE FOR EXISTING PFLAUM ROAD HYDRANT SERVICE INTO ROADWAY TO ACCOMMODATE INSTALLATION OF PROPOSED STORM SEWER. NEW LOCATION OF VALVE TO BE DETERMINED BY CITY WATER INSPECTOR DURING CONSTRUCTION.
  2. THE DEVELOPER SHALL CONFIRM SIZE, MATERIAL, AND INVERT ELEVATION OF THE DEVELOPMENT PROPERTY'S EXISTING SANITARY LATERAL PRIOR TO CONSTRUCTION.
  3. THE LOCATION OF THE PROPOSED WATER SERVICE CONNECTION SHOWN ON THE PLANS IS APPROXIMATE. THE DEVELOPER SHALL DETERMINE THE LOCATION OF SERVICE CONNECTIONS BASED ON THE PROPOSED DEVELOPMENT'S PLUMBING PLANS.
  4. ULOS SHALL BE PERFORMED AND THE INFORMATION SHALL BE PROVIDED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO UTILITY INSTALLATION ALONG PFLAUM ROAD.
  5. CONTRACTOR SHALL FIELD VERIFY ELEVATIONS PRIOR TO CONSTRUCTION.



- REVISION #1: 10/25/2024
1. ULO DATA ADDED. STORM SEWER DESIGN REVISED TO AVOID UTILITY CONFLICTS - SEE SEWER SCHEDULE.
  2. ADDED APPROXIMATE LAYOUT OF TELEPHONE/FIBER OPTIC DUCT PACKAGE ON PFLAUM ROAD.
  3. MINOR NOTE ADDITIONS.

	U-1	
	14925	
<div style="display: flex; justify-content: space-between;"> <span><b>SEWER PLAN AND PROFILE - SEIFERTH ROAD</b></span> <span><b>14925</b></span> </div> <hr/> <div style="display: flex; justify-content: space-between;"> <span><b>2503 SEIFERTH ROAD</b></span> <span><b>MADISON, WI</b></span> </div> <hr/> <div style="display: flex; justify-content: space-between;"> <span>M:\DESIGN\Projects\14925\CAD\Sewer\14925SWR-PipeNetwork.dwg</span> <span><b>CONTRACT NO:</b></span> </div> <div style="display: flex; justify-content: space-between;"> <span></span> <span><b>9376</b></span> </div>		





REVISION #1: 10/25/2024  
1. ULO DATA ADDED. STORM SEWER DESIGN REVISED TO AVOID UTILITY CONFLICTS - SEE SEWER SCHEDULE.  
2. ADDED APPROXIMATE LAYOUT OF TELEPHONE/FIBER OPTIC DUCT PACKAGE ON PFLAUM ROAD.  
3. MINOR NOTE ADDITIONS.

- LEGEND
- 2" MILL AND OVERLAY APPROX. LIMITS
- NOTES
- RELOCATE VALVE FOR EXISTING PFLAUM ROAD HYDRANT SERVICE INTO ROADWAY TO ACCOMMODATE INSTALLATION OF PROPOSED STORM SEWER. NEW LOCATION OF VALVE TO BE DETERMINED BY CITY WATER INSPECTOR DURING CONSTRUCTION.
  - THE DEVELOPER SHALL CONFIRM SIZE, MATERIAL, AND INVERT ELEVATION OF THE DEVELOPMENT PROPERTY'S EXISTING SANITARY LATERAL PRIOR TO CONSTRUCTION.
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  - ULOS SHALL BE PERFORMED AND THE INFORMATION SHALL BE PROVIDED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO UTILITY INSTALLATION ALONG PFLAUM ROAD.
  - CONTRACTOR SHALL FIELD VERIFY ELEVATIONS PRIOR TO CONSTRUCTION.

14925	MADISON, WI	9376	U-2
14925	2503 SEIFERTH ROAD	U-2	
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STORM SEWER SCHEDULE

REVISION 1 - 10/25/2025 EAC

2503 SEIFERTH ROAD  
PROJECT NO. 14925

SHEET NO.  
U-3

STORM SEWER SCHEDULE

CITY OF MADISON

PROPOSED STORM STRUCTURES

STRUC. NO.	STATION	LOCATION (OFFSET)	TYPE	TOP OF CASTING	E.I.	DEPTH	NOTES
<div><div></div><div>PFLAUM ROAD</div><div>S-1</div></div>	2+74.82	LT-12.45	H-INLET	885.79	878.94	6.85	[1]; W/R-3067-7004-V

PROPOSED STORM PIPES

PIPE NO.	FROM (DNSTM)	TO (UPSTM)	DISCH. E.I.	INLET E.I.	PLAN (PAY) LGTH (FT)	PIPE LGTH (FT)	SLOPE (%)	PIPE SIZE	TYPE	NOTES
<div><div></div><div>PFLAUM ROAD</div><div>P-1</div></div>	EX IN6755-009	S-1	878.10	878.94	74	73	1.15%	12"	RCP	[2] NCM (UPSTREAM)

STORM STRUCTURE ADJUSTMENTS/MODIFICATIONS

STRUC. NO.	STATION	LOCATION (OFFSET)	TOP OF CASTING	PROP TOC	ADJ (FT)	NOTES
<div><div>PFLAUM ROAD</div><div>EX IN6755-009</div></div>	3+48.72	LT-11.38	883.19	-	-	[2]

STORM SEWER ULOs

ULO NO.	STATION	LOCATION (OFFSET)	TYPE	NOTES
<div><div><div></div><div>PFLAUM ROAD</div><div>ULO1-W</div></div><div><div></div><div>ULO2-T</div></div><div><div></div><div>ULO3-G</div></div><div><div></div><div>ULO4-T</div></div></div>	2+90.57 2+74.55 2+74.40 [4]	LT-12.31 LT-2.80 LT-1.11 [4]	WATER TELEPHONE GAS TELEPHONE & FIBER OPTIC	[3] <div><div>6" CAST IRON HYDRANT LEAD. TOP @ 878.34' - NO CONFLICT BUT LESS THAN 6" SEPARATION WITH REVISED DESIGN. USE CAUTION</div><div>NO FIBER OPTIC LOCATED AT ULO2-T LOCATION DURING ULOS, POSSIBLY NOT PRESENT, USE CAUTION DURING STORM SEWER INSTALLATION</div><div>6" GAS, TOP @ 881.62' - NO CONFLICT WITH PRIVATE STORM CONNECTION</div><div>FIBER OPTIC/TELEPHONE CONCRETE DUCT PACK, TOP @ 881.11', BOTTOM @ 880.11' - CONFLICT, STORM DESIGN REVISED TO AVOID BUT LESS THAN 6" SEPARATION, USE CAUTION</div></div>

SPECIFIC NOTES

- [1] CONNECT 4" PVC PRIVATE STORM FROM DEVELOPMENT STORMWATER MANAGEMENT SYSTEM, EI(S) = 879.40
- [2] STORM TAP EX IN6755-009 FOR P-1, EI(W) = 878.10
- [3] APPROXIMATE LOCATION OF HYDRANT SERVICE VALVE - RELOCATE VALVE IF NEEDED TO INSTALL P-1
- [4] ULO LOCATION OUTSIDE BOUNDS OF EXISTING PFLAUM ROAD ALIGNMENT, APPROXIMATE NORTHING = 478335.5633, APPROXIMATE EASTING = 841980.0056

**NOTE:** PLAN LENGTH (PAY LENGTH) IS FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE. PIPE LENGTH IS ACTUAL LENGTH OF PIPE FROM STRUCTURE WALL TO STRUCTURE WALL. SLOPE CALCULATED USING PIPE LENGTH.

STANDARD NOTES:

- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD POURED STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES; UD = UNDERDRAIN

- TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SASs.

- TOP OF CONCRETE ROOF (TR) IS 1.25 BELOW TOP OF CASTING UNLESS OTHERWISE NOTED.

- ALL REINFORCED CONCRETE PIPES TO BE CLASS III UNLESS OTHERWISE NOTED.

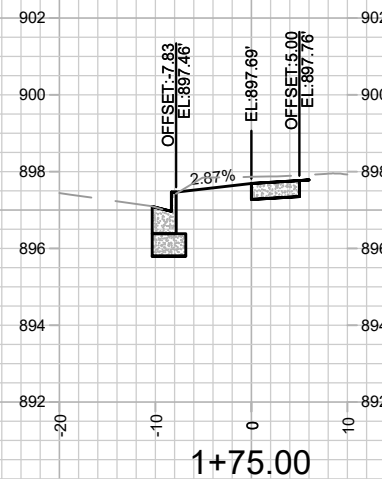
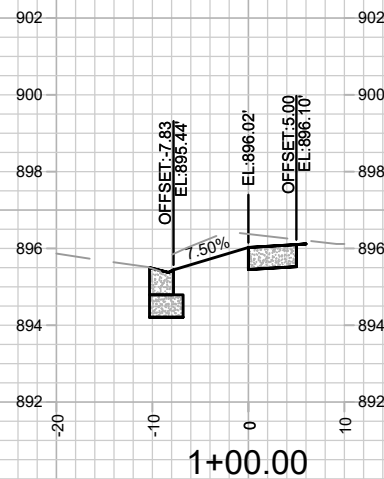
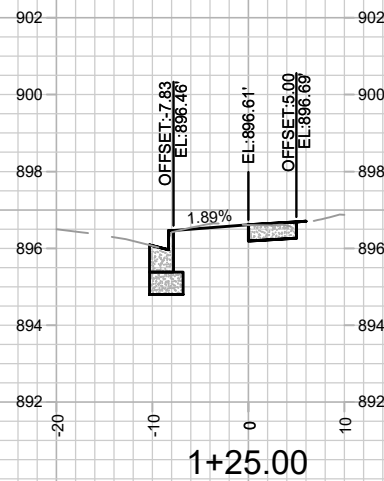
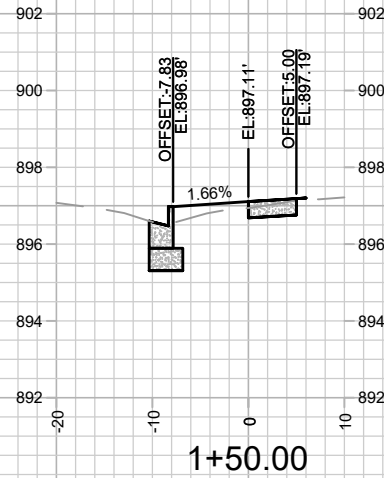
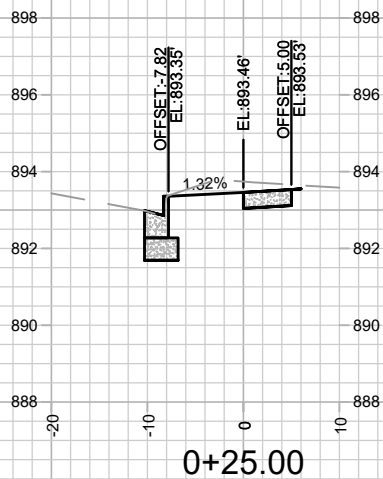
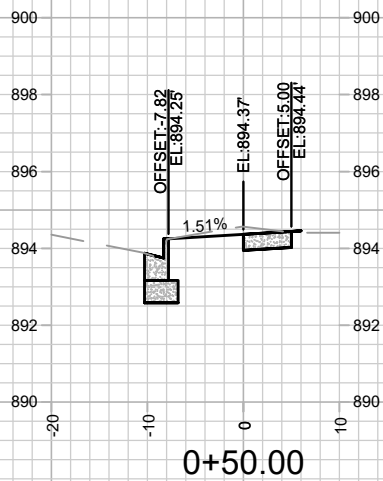
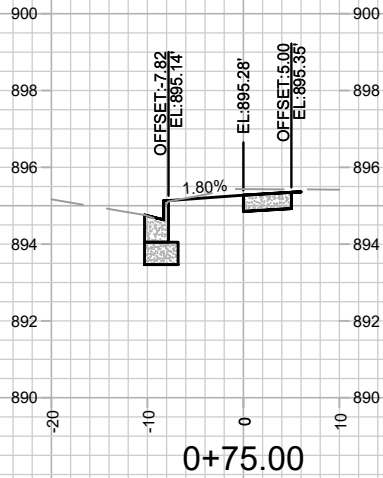
- SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.

-ALL REBAR FOR FIELD POUR STRUCTURES SHALL BE EPOXY COATED. ANY EXPOSED STEEL SHALL BE TOUCHED UP OR RECOATED PRIOR TO USE.

-ALL FIELD POURED SAS STORM STRUCTURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD DETAIL DRAWING 5.7.3.

-ALL PRECAST SAS STORM STRUCTURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD DETAIL DRAWING 5.7.5.

- ALL STRUCTURES CALLED OUT AS FIELD POURED SHALL BE FIELD POURED. ALL OTHER STRUCTURES (NOT INDICATED AS FIELD POURED) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL IF PRECAST STRUCTURES ARE PREFERRED. CONTACT ERIC CEFALU OF CITY ENGINEERING AT (608) 243-5894 FOR PRECAST APPROVALS, FAX SHOP DRAWINGS TO (608)264-9275, OR EMAIL SHOP DRAWINGS TO ECEFALU@CITYOFMADISON.COM.



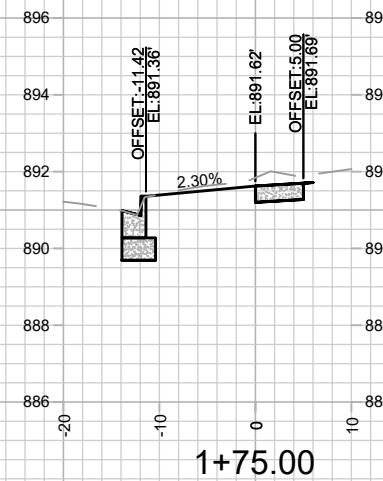
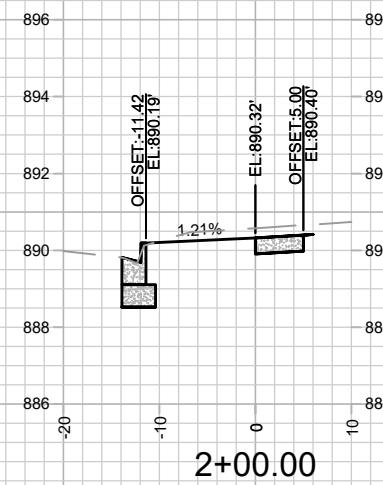
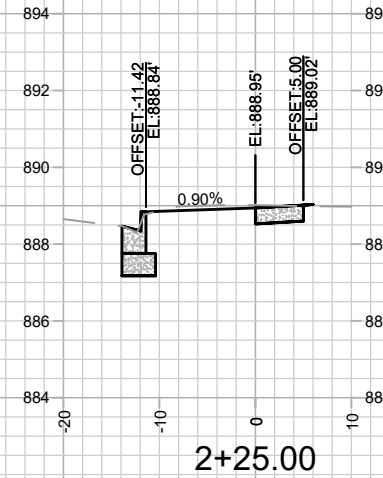
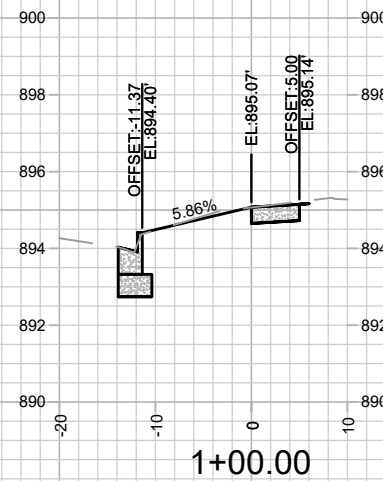
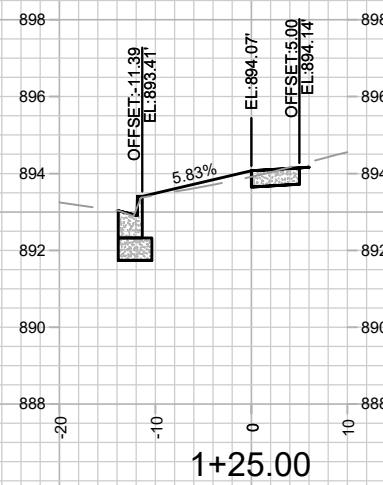
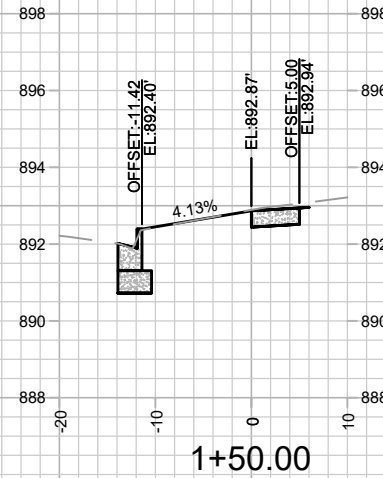
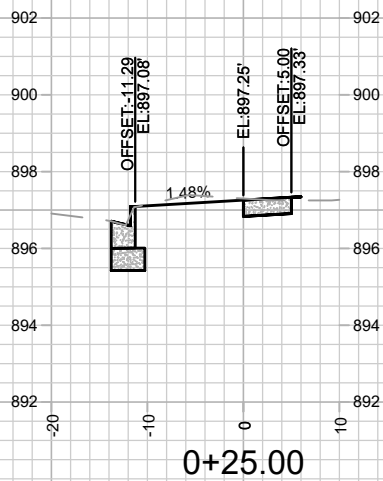
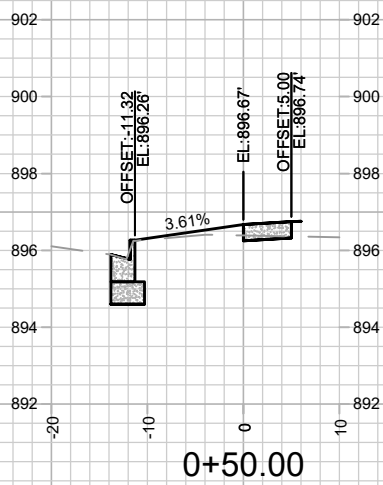
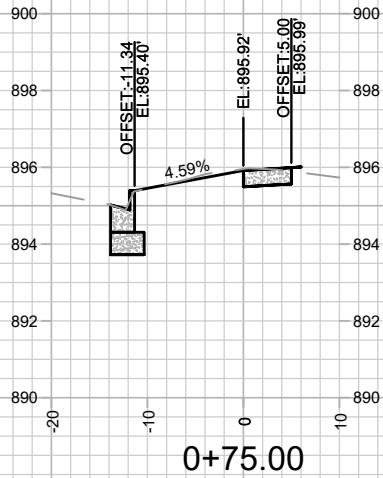
14925  
X-1

CROSS SECTION - SEIFERTH ROAD  
2503 SEIFERTH ROAD

14925  
MADISON, WI  
CONTRACT NO: 9376

MARK	REVISION	DATE	BY
14925	14925	12/11/2023 12:51 PM	X-1

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CROSS SECTION - PFLAUM ROAD  
2503 SEIFERTH ROAD

14925  
MADISON, WI  
CONTRACT NO: 9376

MARK	REVISION	DATE	BY
14925	12/11/2023 12:51 PM	12/11/2023 12:51 PM	X-2

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