

Madison, Wisconsin

CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

INDEX OF SHEETS

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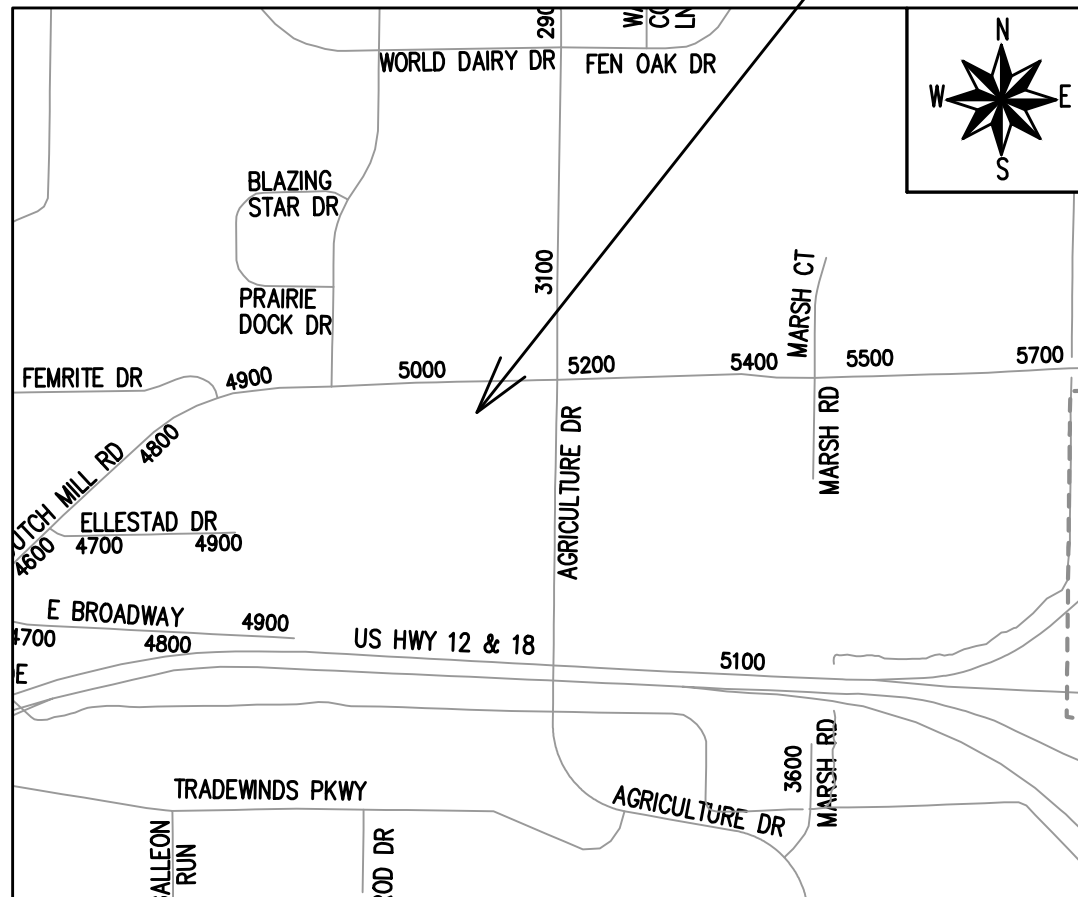
CITY MAINTENANCE PLAN NOT REQUIRED

3322 AGRICULTURE DRIVE CSM

CITY PROJECT NO. 15536

CONTRACT NO. 9620

PROJECT LOCATION



REVISION:
REV #1: **U-1** & **U-2** UPDATED, ULO DATA ADDED

PUBLIC IMPROVEMENT PROJECT
APPROVED

APPROVED DATE
FEBRUARY 11, 2025

BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

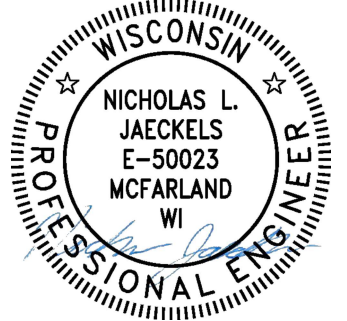
PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

05/20/2025

City Engineer

Date

STREET
DESIGNED BY:



05/20/2025

STORM SHEETS **U-1** AND **U-2**
DESIGNED BY:



05/20/2025

STORM SHEET **U-3**
DESIGNED BY:



Emily Williamson
Expires: 07-31-2026

05/20/2025

PLOT SCALE: 1 IN=1 FT_XREF

PLOT NAME: ----

REV. DATE: 5/13/2025 11:46 AM

ORIGINATOR: CITY_OF_MADISON

ALL PROPOSED STREET TREE REMOVALS WITHIN THE RIGHT OF WAY SHALL BE REVIEWED BY CITY FORESTRY BEFORE THE PLAN COMMISSION MEETING. STREET TREE REMOVALS REQUIRE APPROVAL AND A TREE REMOVAL PERMIT ISSUED BY CITY FORESTRY. ANY STREET TREE REMOVALS REQUESTED AFTER THE DEVELOPMENT PLAN IS APPROVED BY THE PLAN COMMISSION OR THE BOARD OF PUBLIC WORKS AND CITY FORESTRY WILL REQUIRE A MINIMUM OF A 72-HOUR REVIEW PERIOD WHICH SHALL INCLUDE THE NOTIFICATION OF THE ALDERPERSON WITHIN WHO'S DISTRICT IS AFFECTED BY THE STREET TREE REMOVAL(S) PRIOR TO A TREE REMOVAL PERMIT BEING ISSUED.

AS DEFINED BY THE SECTION 107.13 OF CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION: NO EXCAVATION IS PERMITTED WITHIN 5 FEET OF THE TRUNK OF THE STREET TREE OR WHEN CUTTING ROOTS OVER 3 INCHES IN DIAMETER. IF EXCAVATION IS NECESSARY, THE CONTRACTOR SHALL CONTACT MADISON CITY FORESTRY (266-4816) PRIOR TO EXCAVATION. CITY OF MADISON FORESTRY PERSONNEL SHALL ASSESS THE IMPACT TO THE TREE AND TO ITS ROOT SYSTEM PRIOR TO WORK COMMENCING. TREE PROTECTION SPECIFICATIONS CAN BE FOUND ON THE FOLLOWING WEBSITE: [HTTPS://WWW.CITYOFMADISON.COM/BUSINESS/PW/SPECS.CFM](https://www.cityofmadison.com/business/pw/specs.cfm)

DEVELOPER MUST SUBMIT A TRAFFIC CONTROL PLAN TO CITY TRAFFIC ENGINEERING AT LEAST 14 DAYS PRIOR TO THE START OF WORK. WORK SHALL NOT PROCEED UNTIL AN APPROVED TRAFFIC CONTROL PLAN IS IN PLACE.

CONTACT PROJECT ENGINEER AND STREET DESIGNER NICK JAECKELS AT NJAECKELS@CITYOFMADISON.COM FOR CAD AND ALIGNMENT DATA PRIOR TO STAKING.

CONTACT THE CITY CONSTRUCTION ENGINEER, KYLE FRANK, AT KFRANK@CITYOFMADISON.COM FOR PRECONSTRUCTION SCHEDULING, COORDINATION, AND INSPECTION.

CONVENTIONAL SIGNS

FIELD VERIFY ALL UTILITY LOCATIONS

GAS

STORM SEWER

SANITARY SEWER

WATER

BURIED ELECTRIC

OVERHEAD ELECTRIC

POWER POLE

ADA COMPLIANT RAMP W/
DETECTABLE WARNING FIELD

COMBUSTIBLE FLUIDS

— G —

— ST —

— SAN —

— W —

— E —

— OH —

UNDERDRAINS SHALL BE INSTALLED PER STANDARD DETAIL DRAWING 4.05 FOR 75' ON EACH SIDE OF THE LOW POINT, OR TO THE NEAREST CURB HIGH POINT. ALL UNDERDRAIN SHALL BE WRAPPED.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADE OF 0.5% TOWARD STORM SEWER INLETS.

ALL DITCHES SHALL DRAIN WITH A MINIMUM GRADE OF 0.5%

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL TYPICALLY BE 1.5%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.04. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENTS FROM THE CITY PROJECT ENGINEER PRIOR TO STAKING THIS PROJECT. THE CITY PROJECT ENGINEER IS NICK JAECKELS. EMAIL: NJAECKELS@CITYOFMADISON.COM

CURB STATION AND OFFSETS SHALL BE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK UNLESS OTHERWISE INDICATED ON THE PLANS.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS RIGHT-OF-WAY PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO S.D.D 3.05.

ALL WORK IN THE RIGHT OF WAY AND PUBLIC EASEMENTS SHALL BE IN ACCORDANCE WITH THE CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION LATEST EDITION.

ALL PROPOSED IMPROVEMENTS IN THE RIGHT-OF-WAY ARE BASED ON SURVEY DATA PROVIDED BY THE DEVELOPER OR ITS CONSULTANT. IN THE CASE THAT THE PROPOSED IMPROVEMENTS CAN NOT BE INSTALLED PER THESE PLANS OR THE CITY OF MADISON STANDARD SPECIFICATION FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION, THE DEVELOPER SHALL BE RESPONSIBLE FOR THE COST OF EXTRA WORK DUE TO INACCURACIES OF THE SURVEY.

ALL UTILITY VERIFICATIONS AND ACCURACY OF THE DRAWINGS ARE THE RESPONSIBILITY OF THE DEVELOPER. ANY CONFLICTS THAT ARISE FROM MISSING OR ERRONEOUS INFORMATION WILL BE AT THE EXPENSE OF THE DEVELOPER. NO PRECAST STRUCTURES WILL BE APPROVED FOR STORM OR SANITARY SEWER UNTIL ALL POTENTIAL UTILITY CONFLICTS ARE RESOLVED.

IN LOCATIONS WHERE PAVEMENT RESTORATION IS NOT IDENTIFIED ADJACENT TO CURB AND GUTTER REPLACEMENT, IT IS ASSUMED THAT CURB AND GUTTER WILL BE REPLACED WITHOUT DAMAGING ADJACENT PAVEMENT. IF DAMAGED, THE MILL AND OVERLAY LIMITS SHALL BE EXTENDED BY THE CITY CONSTRUCTION ENGINEER AS NECESSARY TO MEET THE STANDARD PATCHING CRITERIA.

15536

MADISON, WI

9620

CONTRACT NO:

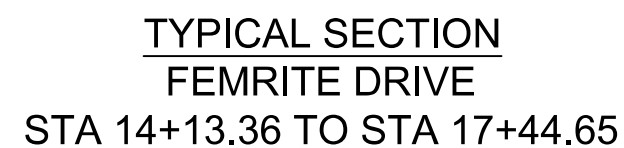
STANDARD NOTES

3322 AGRICULTURE DRIVE CSM

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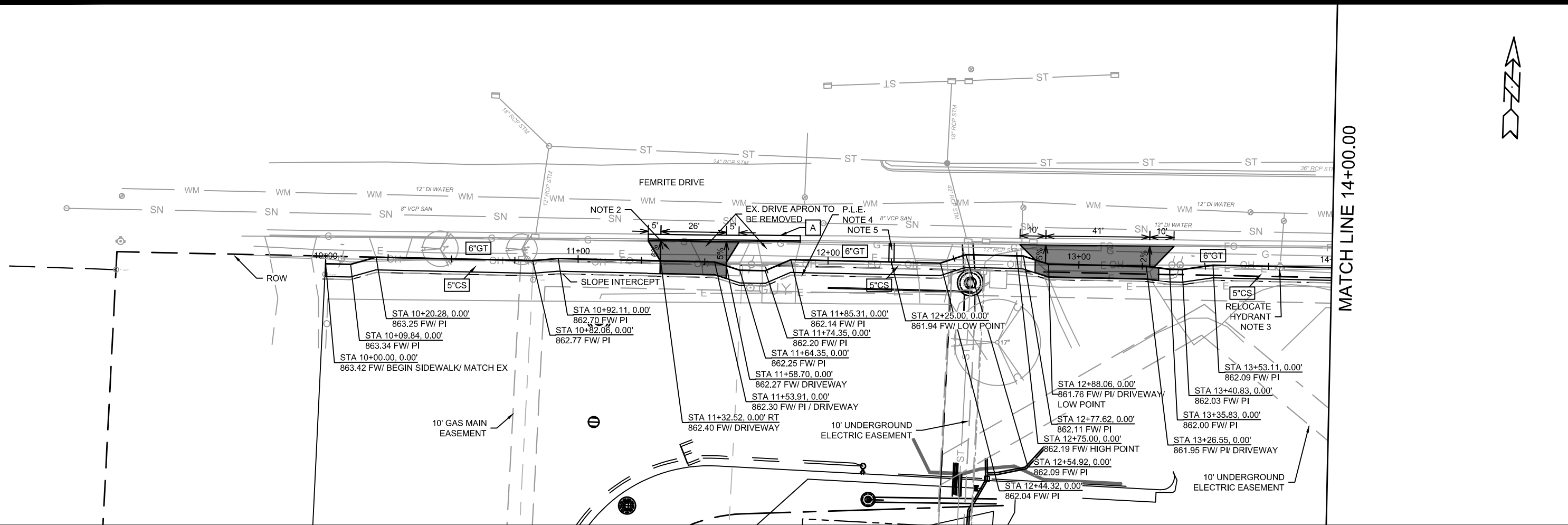
D1



- 1 POINT REFERRED TO ON PROFILE
2 5" CONCRETE SIDEWALK
3 6" TOPSOIL, SEED AND MATTING
4 EX. FILL
5 EX. CURB TYPE 'A' TO REMAIN
6 EX. PAVEMENT TO REMAIN
7 EX. ROAD BASE COURSE TO REMAIN
8 6" TOPSOIL, SEED, AND EROSION MAT CLASS II

*SEE X-SHEETS FOR CROSS SLOPES
 **SEE P-SHEETS FOR R/W LOCATION





MATCH LINE 14+00.00



LEGEND

5"CS

5-INCH CONCRETE SIDEWALK

6"GT

6-INCH TOPSOIL, SEED & MATTING

A

SPOT REPLACE TYPE 'A' CONCRETE CURB & GUTTER, NOTE 1

7-INCH CONCRETE SIDEWALK, DRIVE APRON OR RAMP

- NOTES
1.

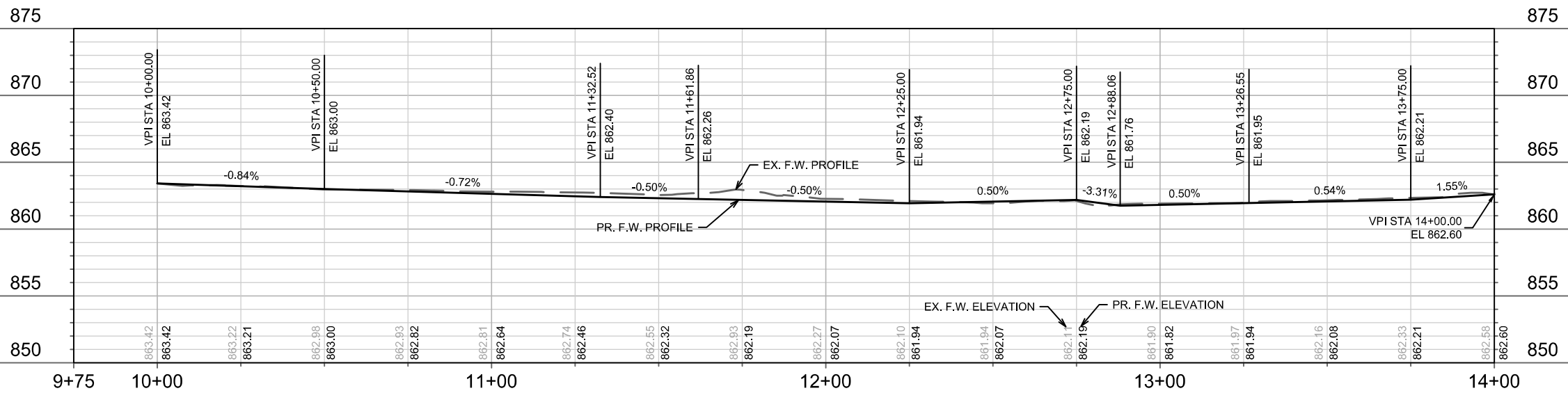
IN LOCATIONS WHERE PAVEMENT RESTORATION IS NOT IDENTIFIED ADJACENT TO CURB AND GUTTER REPLACEMENT, IT IS ASSUMED THAT CURB AND GUTTER WILL BE REPLACED WITHOUT DAMAGING ADJACENT PAVEMENT. IF PAVEMENT IS DAMAGED, THE MILL AND OVERLAY LIMITS SHALL BE EXTENDED BY THE CITY CONSTRUCTION ENGINEER AS NECESSARY TO MEET THE STANDARD PATCHING CRITERIA.
2.

SEE COMMERCIAL DRIVEWAY PERMIT LNDSPR-2024-00144
\\Gisserver\data\DESIGN\Projects\15536\Project Documents\Permits\Commercial Driveway Permit
3.

CONTACT JEFF BELSHAW (608-261-9835) OF THE WATER UTILITY ONE WEEK PRIOR TO HYDRANT RELOCATION.
4.

SIDEWALK IS NOT TO BE CONSTRUCTED UNTIL P.L.E. HAS BEEN RECORDED.
5.

2' WIDE, 5" CONCRETE WATERWAY.



15536

MADISON, WI

CONTRACT NO: 9620

PLAN AND PROFILE - FEMRITE DRIVE

3322 AGRICULTURE DRIVE CSM

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15536

P1

REVISION

DATE

BY

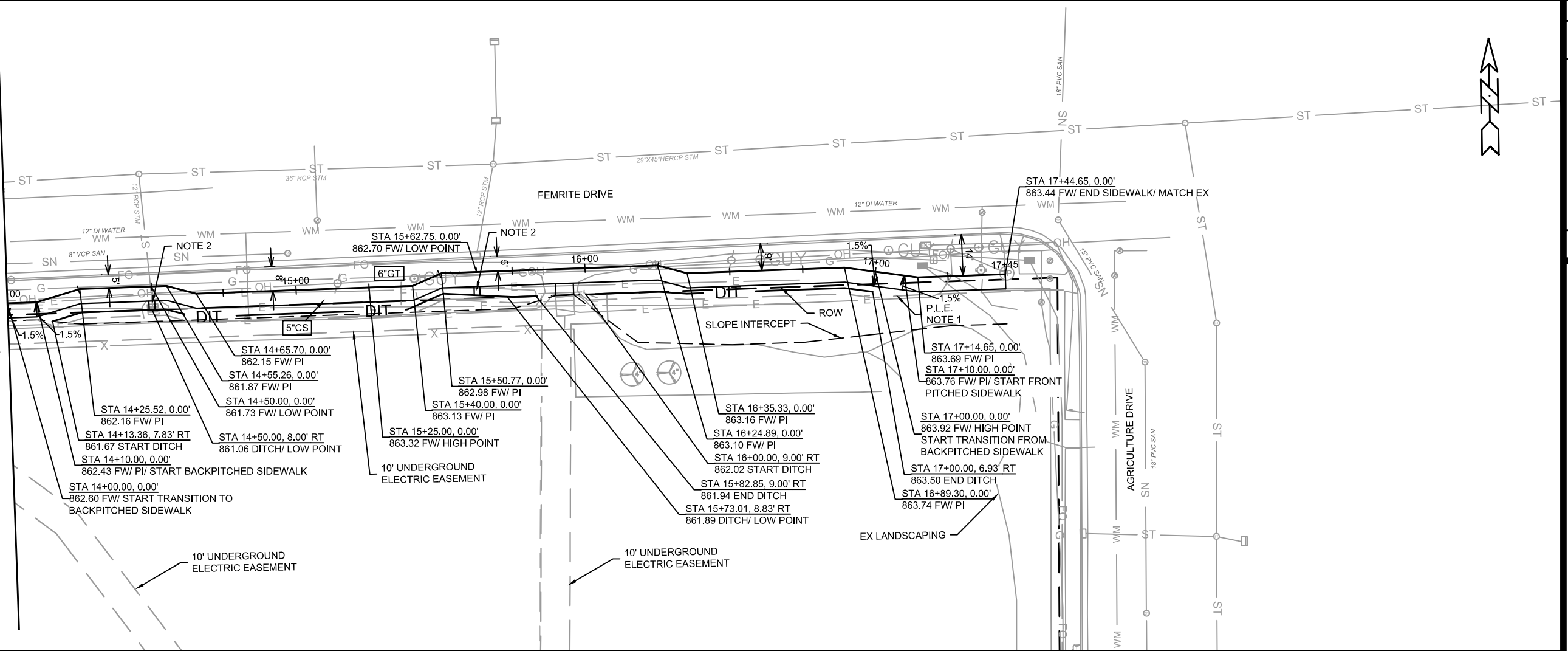
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5/15/2025 3:55 PM

MLJ

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MATCH LINE 14+00.00

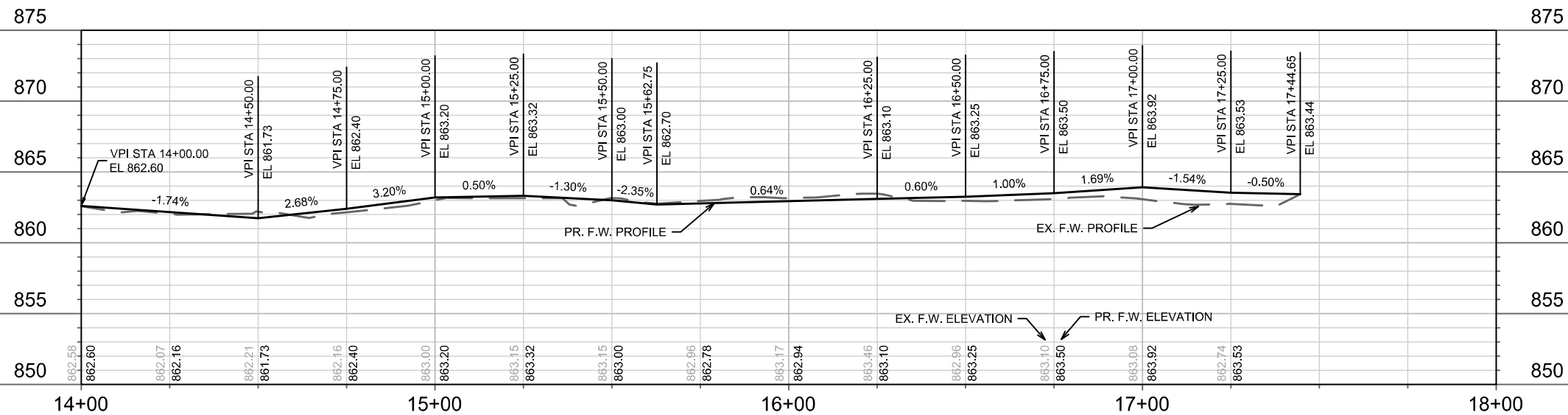


LEGEND

- 5"CS 5-INCH CONCRETE SIDEWALK
- 6"GT 6-INCH TOPSOIL, SEED & MATTING

NOTES

- SIDEWALK IS NOT TO BE CONSTRUCTED UNTIL P.L.E. HAS BEEN RECORDED.
- 2' WIDE, 5" CONCRETE WATERWAY.



15536		MADISON, WI		CONTRACT NO: 9620	
PLAN AND PROFILE - FEMRITE DRIVE		3322 AGRICULTURE DRIVE CSM		M:\DESIGN\Projects\15536\CAD\Streets\15536EN-PnP.dwg	
15536		P2		P2	
REVISION		DATE		BY	
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Designed By: N.J.		Scale: 1" = 40'		P2	

STORM SEWER SCHEDULE

PROPOSED STORM STRUCTURES

STRUC. NO.	STATION	LOCATION (OFFSET)	TYPE	TOP OF CASTING	E.I.	DEPTH	NOTES
FEMRITE DRIVE (DITCH CENTERLINE)							
S-1	20+34.57	CL	3'X3' STORM SAS	861.06	857.55	3.51	[1]; LP; R-1878-B7G
S-2	21+57.84	CL	3'X3' STORM SAS	861.89	859.36	2.53	LP; FP; R-1878-B7G
S-3	21+67.84	CL	[2]; PIPE END	-	861.94	-	[3]; [4]; [5]
S-4	21+84.85	CL	[2]; PIPE END	-	862.02	-	[3]; [4]; [5]

STORM STRUCTURE REMOVALS & ABANDONMENTS

STRUC. NO.	ID NO.	STATION	LOCATION (OFFSET)	TYPE	DEPTH (FT)	NOTES
FEMRITE DRIVE (DITCH CENTERLINE)						
RS-1	IN6959-017	20+34.90	LT-0.18	3'X3' STORM SAS	3.10	
RS-2	UNMARKED	21+68.11	LT-0.80	12" CMP END	-	[6]
RS-3	UNMARKED	21+84.03	LT-0.86	12" CMP END	-	[6]

STORM STRUCTURE ADJUSTMENTS

STRUC. NO.	ID NO.	STATION	LOCATION (OFFSET)	EX TOC	PROP TOC	ADJ (FT)	NOTES
FEMRITE DRIVE (DITCH CENTERLINE)							
ADJ-1	IN6959-020	21+46.44	LT-13.44	863.29	-	-	[7]

SPECIFIC NOTES:

- [1] RECONNECT EX 12" RCP STM TO S-1
- [2] 8" AWWA-C900 PVC PIPE (BLUE BRUTE OR EQUIVLAENT)
- [3] MATCH PIPE EI TO DRAINAGE DITCH ELEVATION AT STRUCTURE LOCATION
- [4] CUT PIPE ENDS TO MATCH GRADE AT STRUCTURE LOCATION
- [5] ****USE CAUTION DURING INSTALLATION - NEAR ELECTRICAL TRANSFORMER****
- [6] 12" CMP PIPE RUNNING BENEATH RAISED SECTION FOR ELECTRICAL TRANSFORMER ACCESS TO MAINTAIN DITCH FLOW, NOT MARKED IN CITY RECORDS
- [7] STM TAP & CONCRETE COLLAR AT ADJ-1 FOR P-2 INSTALL, CONTRACTOR TO VERIFY TAP WILL WORK IN FIELD
- [8] **USE CAUTION DURING S-1 INSTALL, CONDUIT ALIGNMENT APPEARS TO BE SOUTH OF STRUCTURE LOCATION HOWEVER CONDUIT ALSO APPEARS TO BE LESS THAN 0.5' FROM STRUCTURE BOTTOM**
- [9] **USE CAUTION DURING P-2 INSTALL, LESS THAN 0.5' FROM TOP OF P-2 PIPE (12" PVC) TO BOTTOM OF CONDUIT (2" PVC), COORDINATE WITH PRIVATE UTILITY IF RELOCATION IS REQUIRED FOR P-2 INSTALLATION**
- [10] **USE CAUTION DURING S-2 INSTALL, CONDUIT ALIGNMENT APPEARS TO BE SOUTH OF STRUCTURE LOCATION HOWEVER CONDUIT ALSO APPEARS TO BE LESS THAN 0.5' FROM STRUCTURE BOTTOM,**
- FIELD POUR STRUCTURE TO AVOID CONFLICTS AS NEEDED AND/OR COORDINATE WITH PRIVATE UTILITY IF RELOCATION IS REQUIRED FOR S-2 INSTALLATION**

STANDARD NOTES:

- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD POURED STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES; UD = UNDERDRAIN
- PLAN LENGTH (PAY LENGTH) IS FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE. PIPE LENGTH IS ACTUAL LENGTH OF PIPE FROM STRUCTURE WALL TO STRUCTURE WALL. SLOPE CALCULATED USING PIPE LENGTH.
- TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SAS UNLESS NOTED OTHERWISE.
- APPROXIMATE DISCHARGE E.I. GIVEN, ADJUST E.I. AND PIPE SLOPE IN THE FIELD.
- ALL REINFORCED CONCRETE PIPES TO BE CLASS 3 UNLESS OTHERWISE NOTED.

● REVISION 1 - 7/7/2025 EAC

PROPOSED STORM PIPES

PIPE NO.	FROM (DNSTM)	TO (UPSTM)	DISCH. E.I.	INLET E.I.	PLAN (PAY) LGTH (FT)	PIPE LGTH (FT)	SLOPE (%)	PIPE SIZE	TYPE	NOTES
FEMRITE DRIVE (DITCH CENTERLINE)										
P-2	ADJ-1	S-2	859.27	859.36	17	16	0.56%	12"	TYPE II	NCM
P-3	S-3	S-4	861.94	862.02	17	17	0.47%	8"	[2]	[3]; [4]; [5]

STORM PIPE REMOVALS & ABANDONMENTS

PIPE REMOVAL NO.	REMOVE FROM	REMOVE TO	LGTH (FT)	PAID (Y/N)	REM LGTH PAID (FT)	ABN LGTH (FT)	SLURRY (CY)	PIPE SIZE	PIPE TYPE	NOTES
FEMRITE DRIVE (DITCH CENTERLINE)										
RP-1	RS-2	RS-3	16	N	-	-	-	12"	CMP	[6]

STORM ULO SCHEDULE

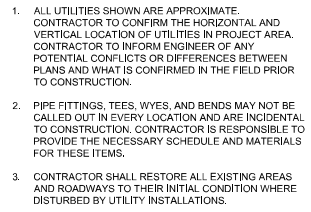
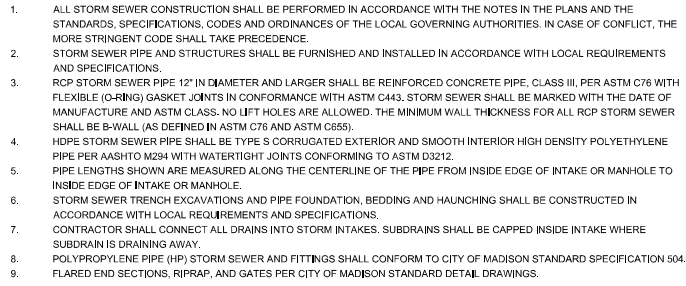
ID NO.	STATION	LOCATION	TYPE	NOTES
(OFFSET)				
FEMRITE DRIVE (DITCH CENTERLINE)				
● ULO1A-E	20+34.27	RT-2.46	[8]; 2" PVC ELECTRIC, TOP @ 857.14', NO CONFLICT EXPECTED	
● ULO1B-E	20+34.26	RT-2.33	[8]; 2" PVC ELECTRIC, TOP @ 857.15', NO CONFLICT EXPECTED	
● ULO1C-E	20+34.29	RT-2.18	[8]; 2" PVC ELECTRIC, TOP @ 857.18', NO CONFLICT EXPECTED	
● ULO2A-E	20+38.06	LT-1.63	2" PVC ELECTRIC, TOP @ 856.39', NO CONFLICT EXPECTED	
● ULO2B-E	20+38.12	LT-1.73	2" PVC ELECTRIC, TOP @ 856.42', NO CONFLICT EXPECTED	
● ULO3A-FO	21+48.70	LT-10.87	[9]; 2" PVC FIBER OPTIC, TOP @ 860.75', NO CONFLICT EXPECTED	
● ULO3B-FO	21+48.71	LT-10.73	[9]; 2" PVC FIBER OPTIC, TOP @ 860.74', NO CONFLICT EXPECTED	
● ULO4A-E	21+55.21	LT-4.28	5" PVC ELECTRIC, TOP @ 858.73', NO CONFLICT EXPECTED	
● ULO4B-E	21+55.17	LT-4.60	5" PVC ELECTRIC, TOP @ 858.76', NO CONFLICT EXPECTED	
● ULO5A-E	21+57.79	RT-3.85	[10]; 5" PVC ELECTRIC, TOP @ 859.34', NO CONFLICT EXPECTED	
● ULO5B-E	21+57.72	RT-4.27	[10]; 2" PVC ELECTRIC, TOP @ 859.15', NO CONFLICT EXPECTED	
● ULO6A-E	21+67.65	RT-3.53	2" PVC ELECTRIC, TOP @ 858.68', NO CONFLICT EXPECTED	
● ULO6B-E	21+67.90	RT-3.32	5" PVC ELECTRIC, TOP @ 859.26', NO CONFLICT EXPECTED	
● ULO7A-E	21+85.99	LT-3.43	5" PVC ELECTRIC, TOP @ 858.91', NO CONFLICT EXPECTED	
● ULO7B-E	21+86.34	LT-3.88	5" PVC ELECTRIC, TOP @ 859.08', NO CONFLICT EXPECTED	
● ULO8A-E	21+55.32	LT-0.50	2" PVC ELECTRIC, TOP @ 857.78', NO CONFLICT EXPECTED	
● ULO8B-E	21+55.28	LT-0.41	2" PVC ELECTRIC, TOP @ 857.87', NO CONFLICT EXPECTED	

-ALL FIELD POURED SAS STORM STRUCTURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD DETAIL DRAWING 5.7.3. ALL PRECAST SAS STORM STRUCTURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD DETAIL DRAWING 5.7.5.

- ALL STRUCTURES CALLED OUT AS FIELD POURED SHALL BE FIELD POURED. ALL OTHER STRUCTURES STORM AND SANITARY (NOT INDICATED AS FIELD POURED) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL ECEFALU@CITYOFMADISON.COM ((608) 243-5894).

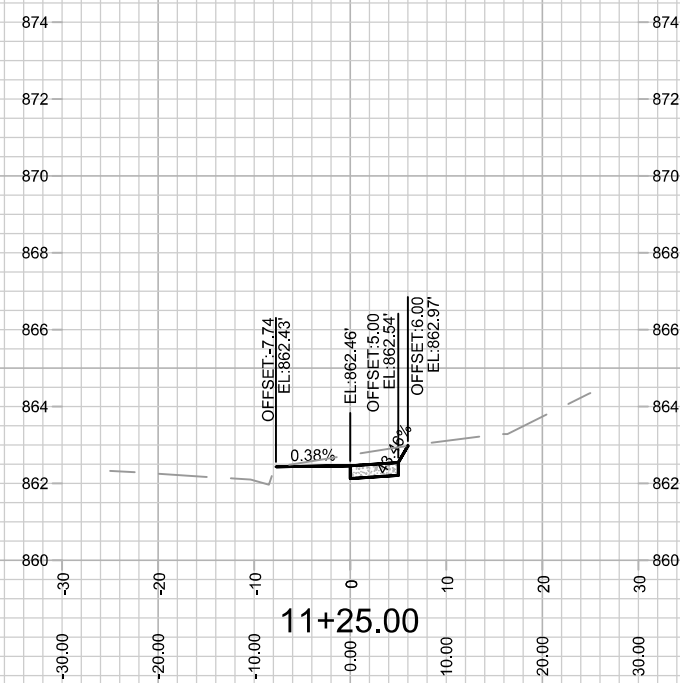
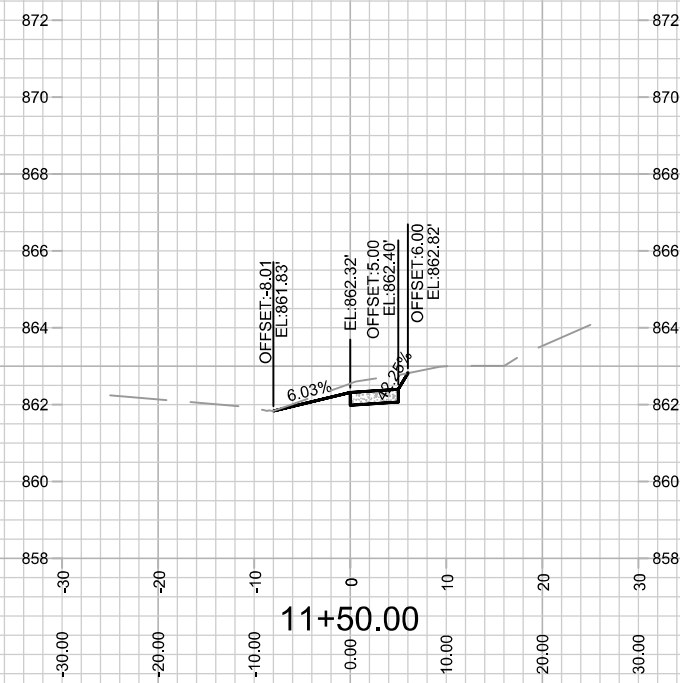
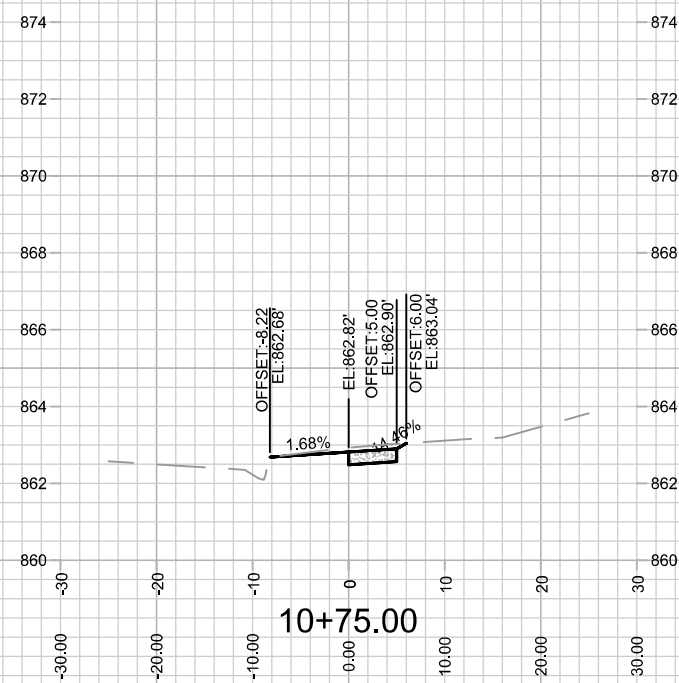
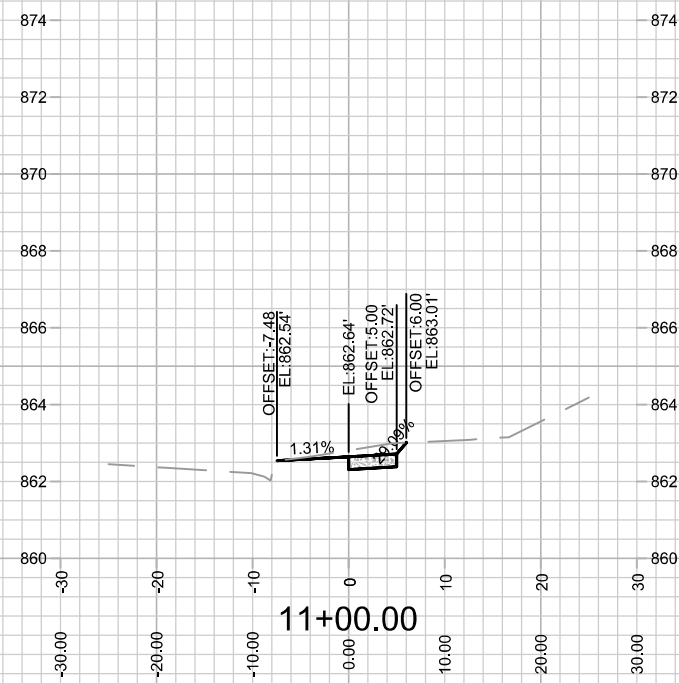
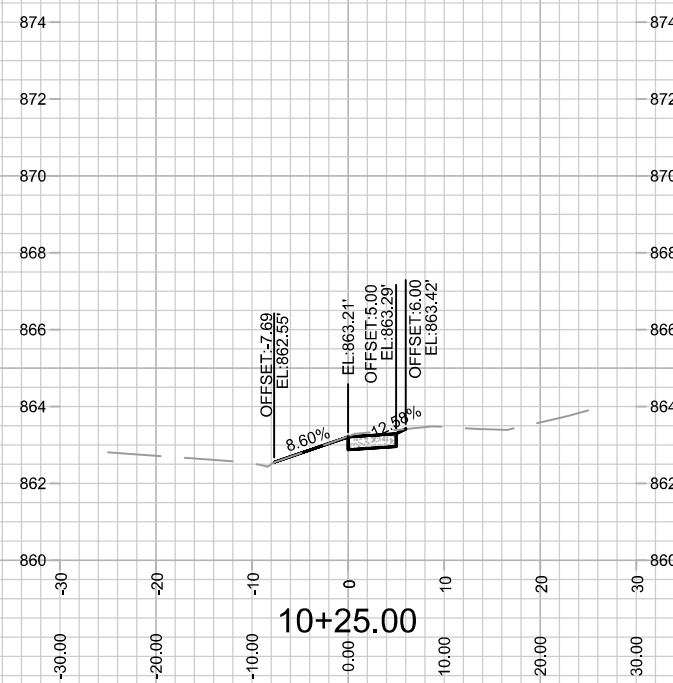
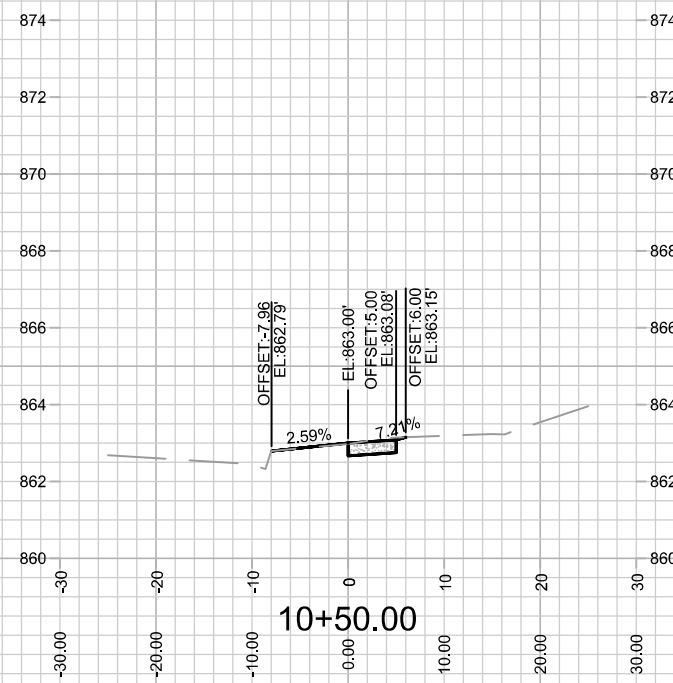
-ALL REBAR FOR FIELD POURED STRUCTURES SHALL BE EPOXY COATED. ANY EXPOSED STEEL SHALL BE TOUCHED UP OR RECOATED PRIOR TO USE.

- SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.



PROJECT # 2240005920





15536

X1

CROSS SECTIONS - FEMRITE DRIVE

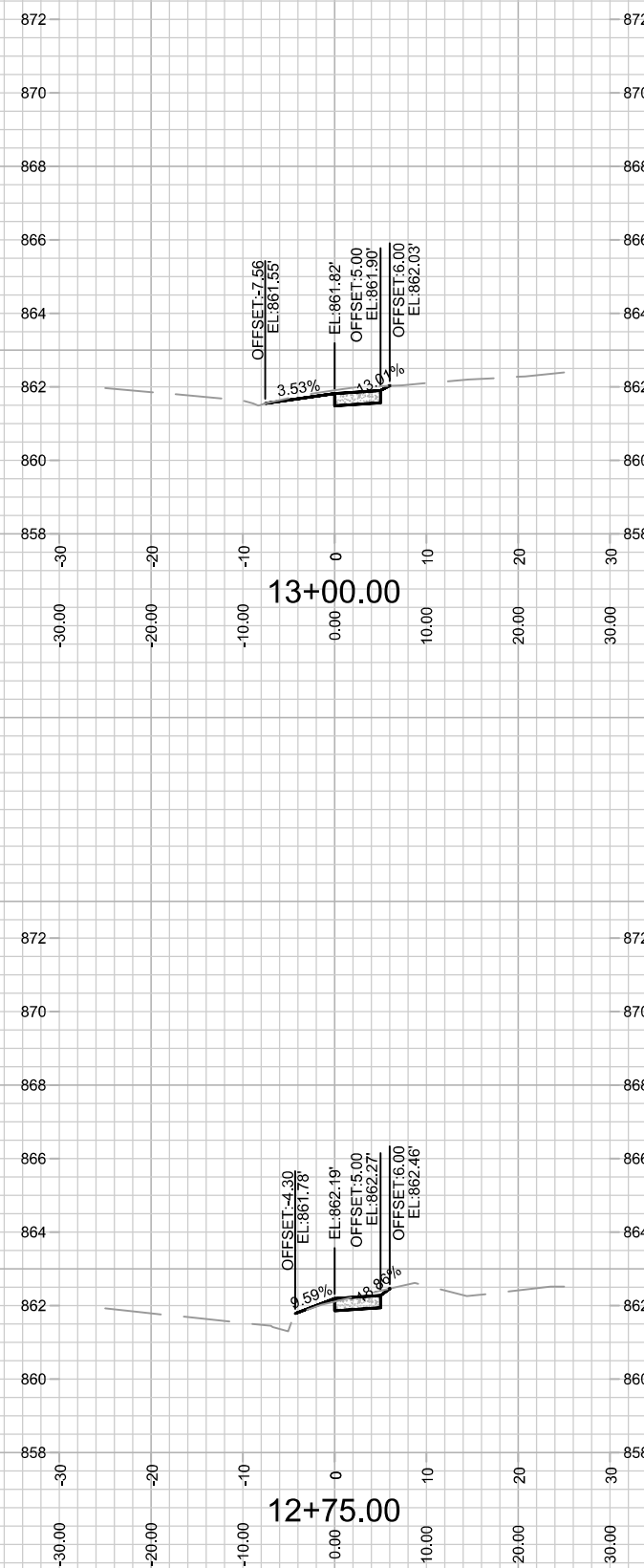
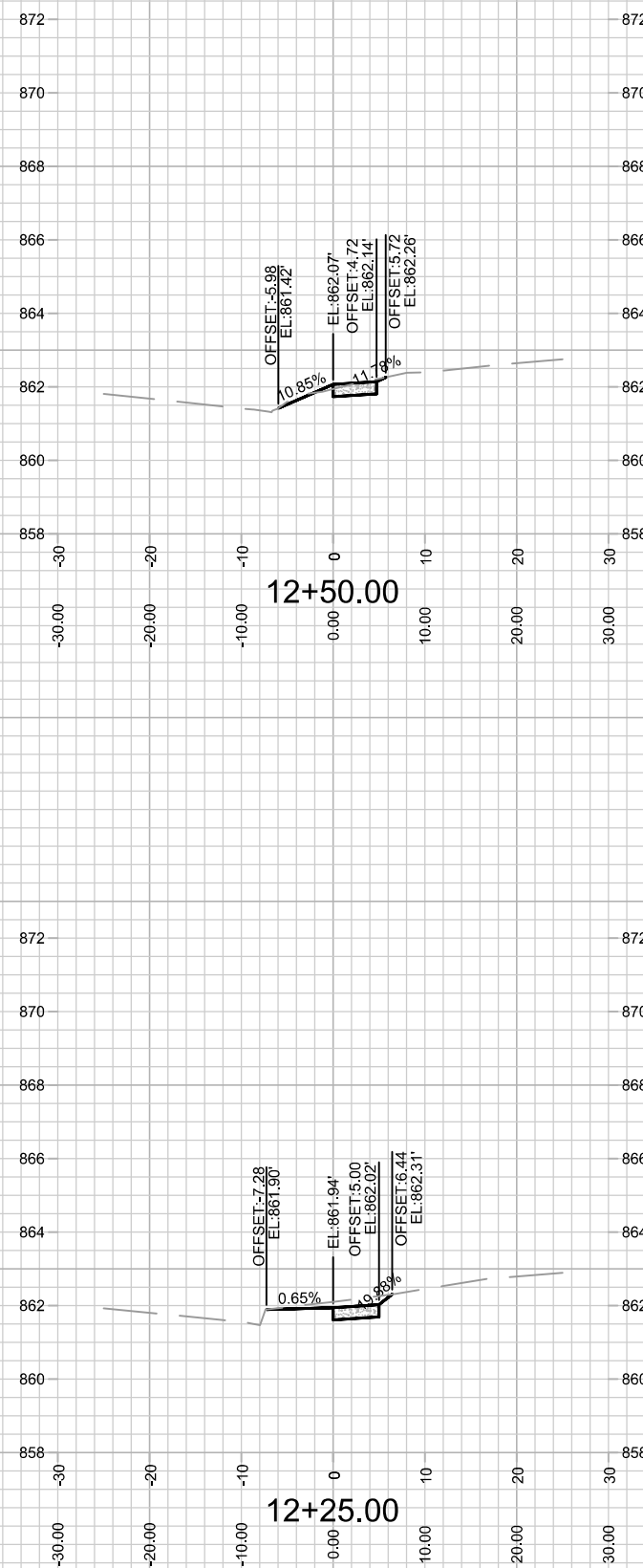
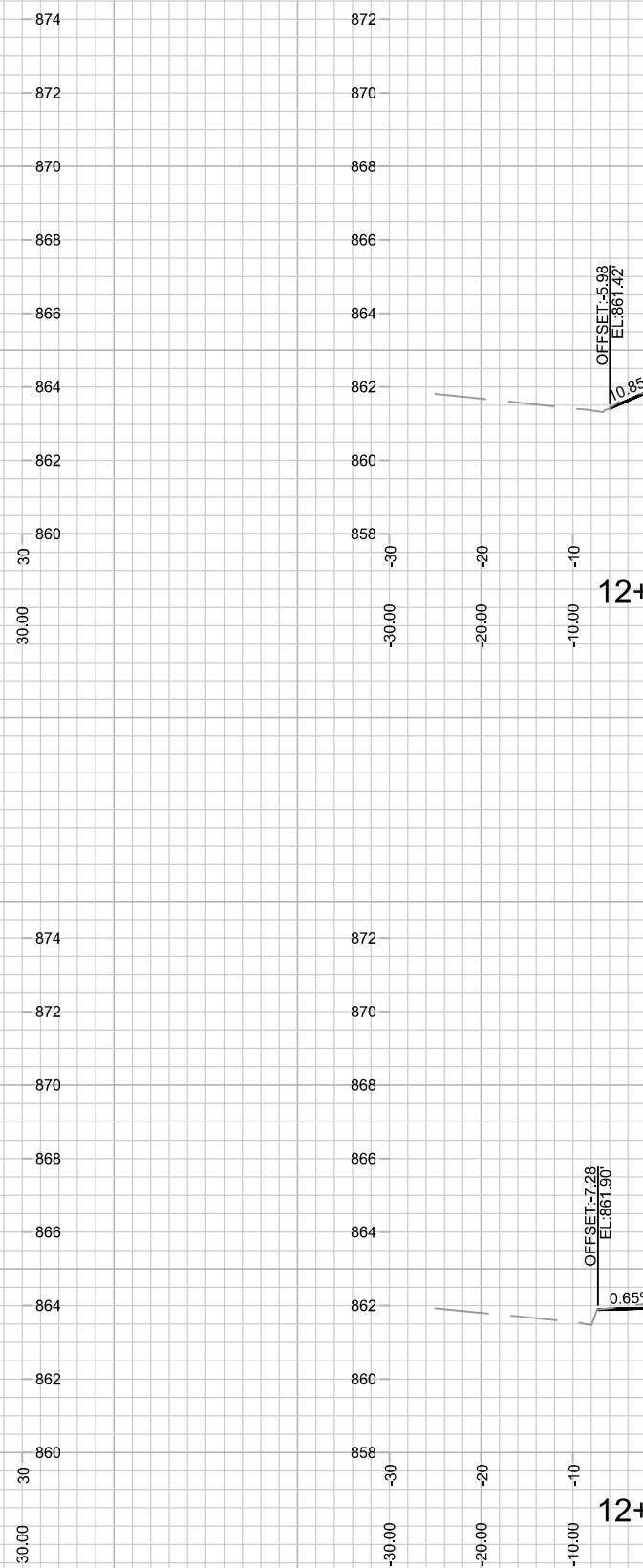
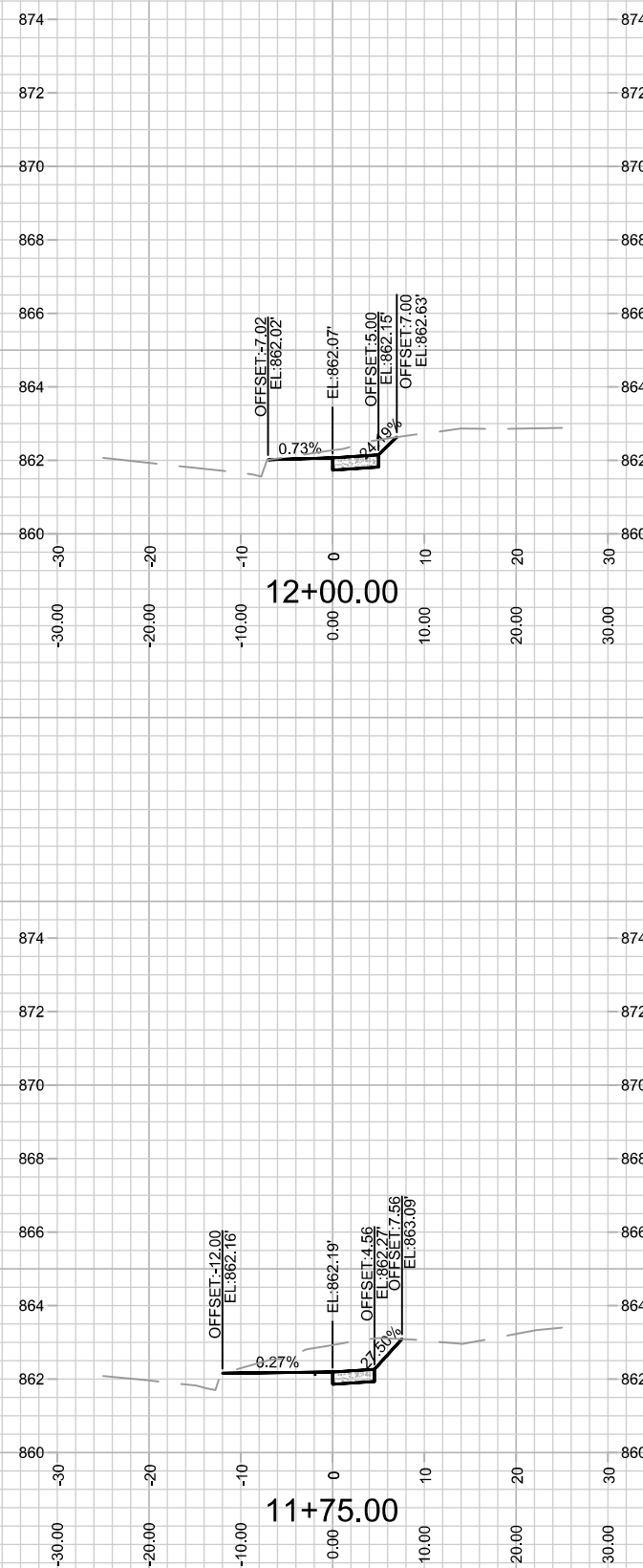
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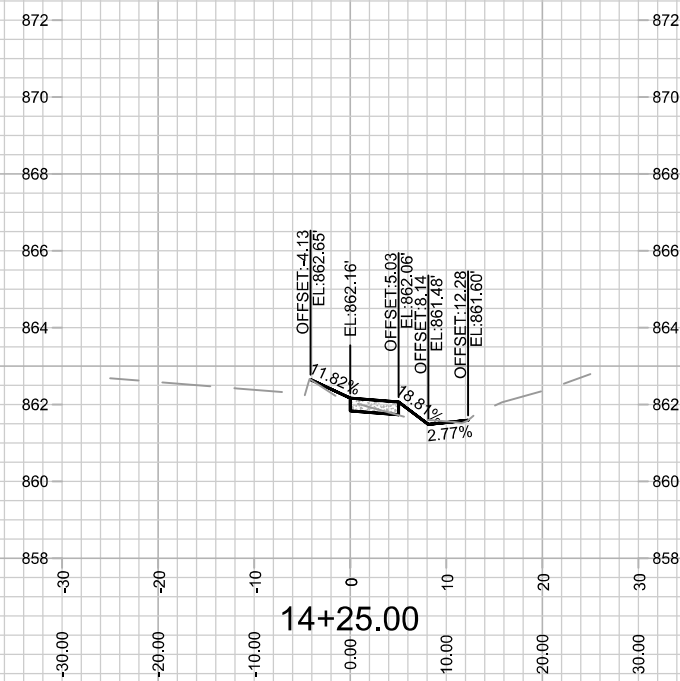
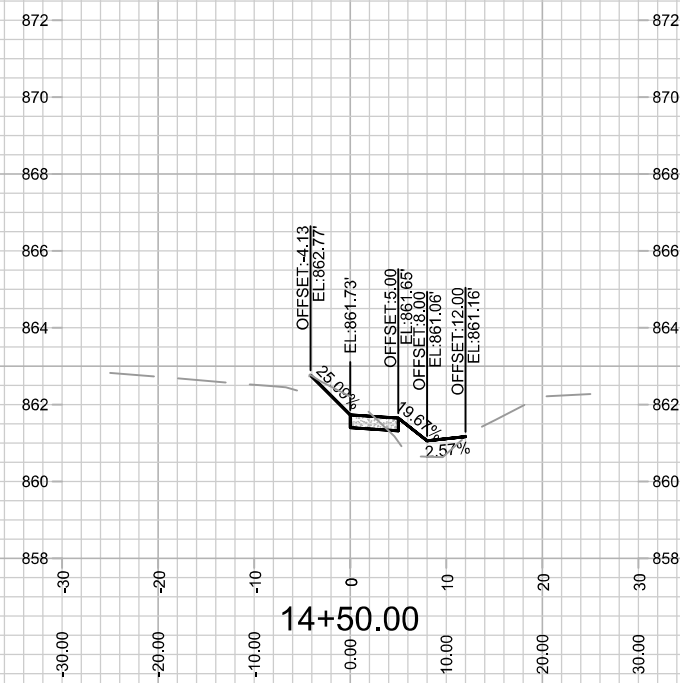
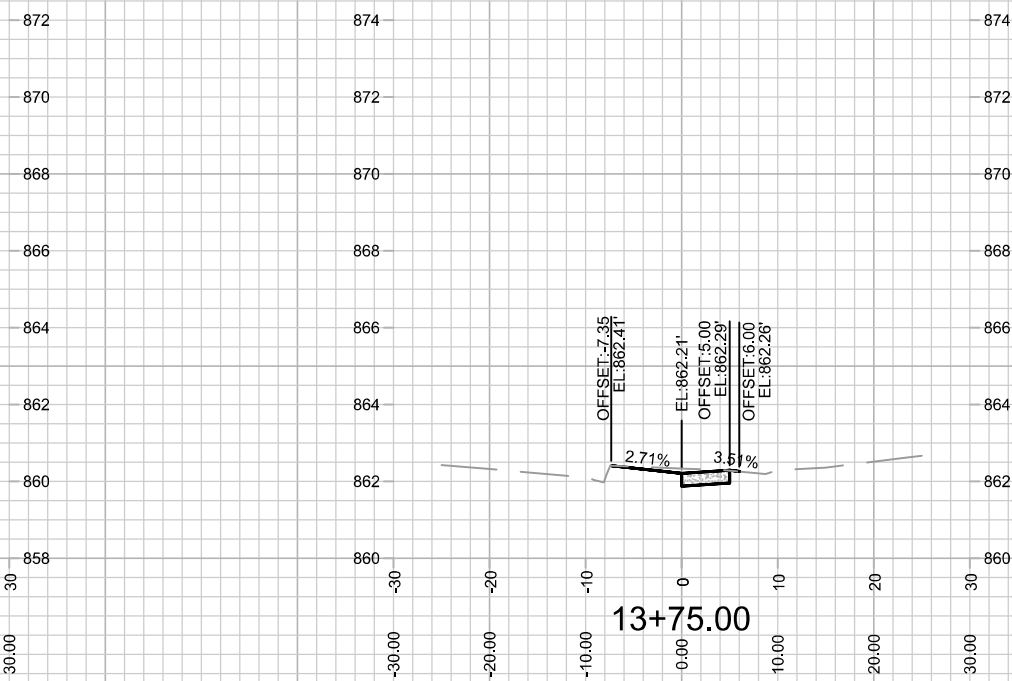
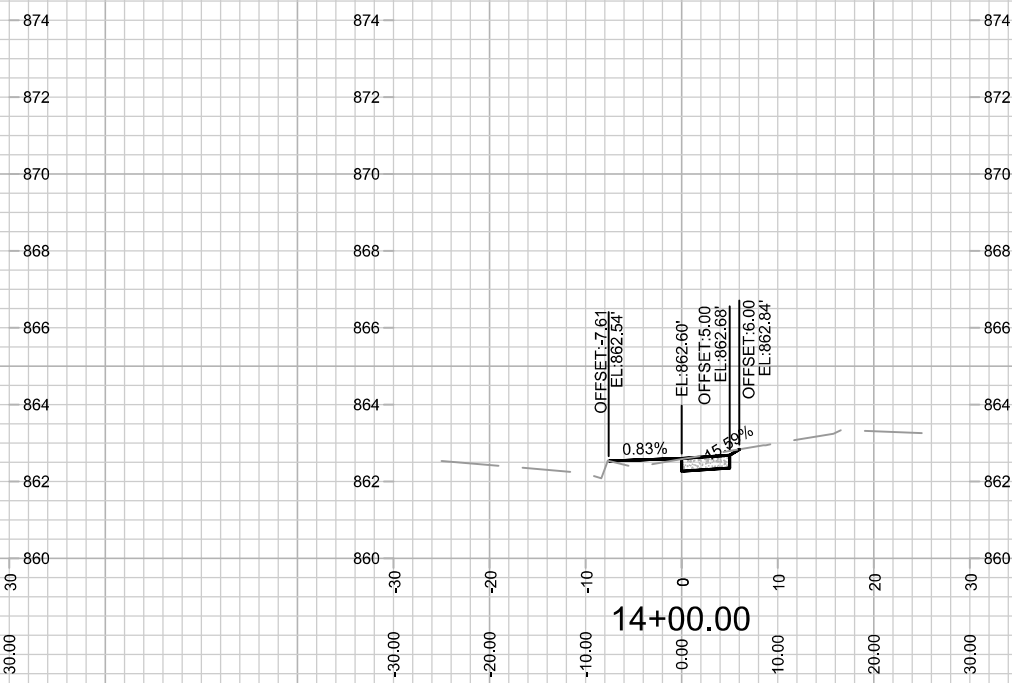
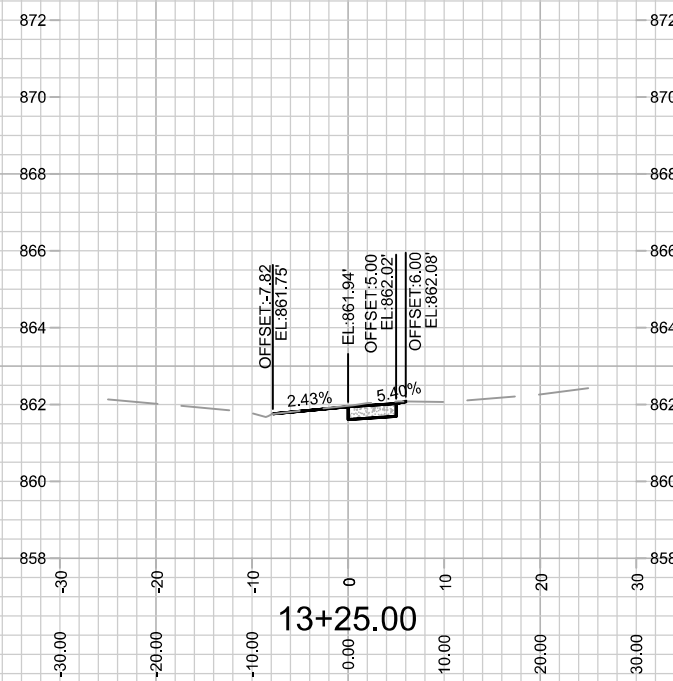
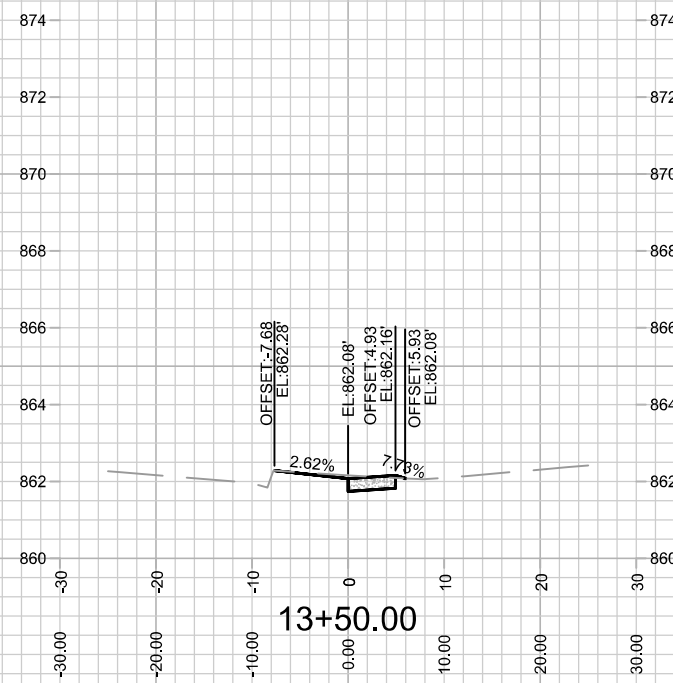
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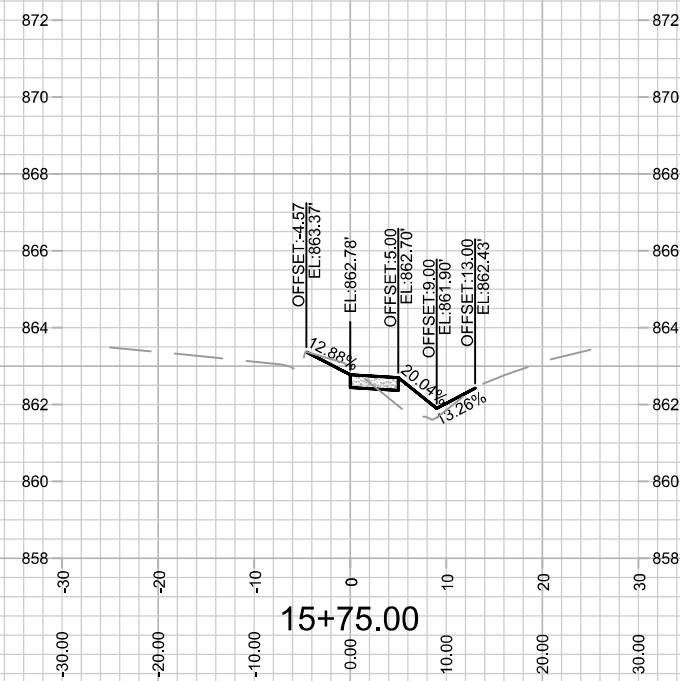
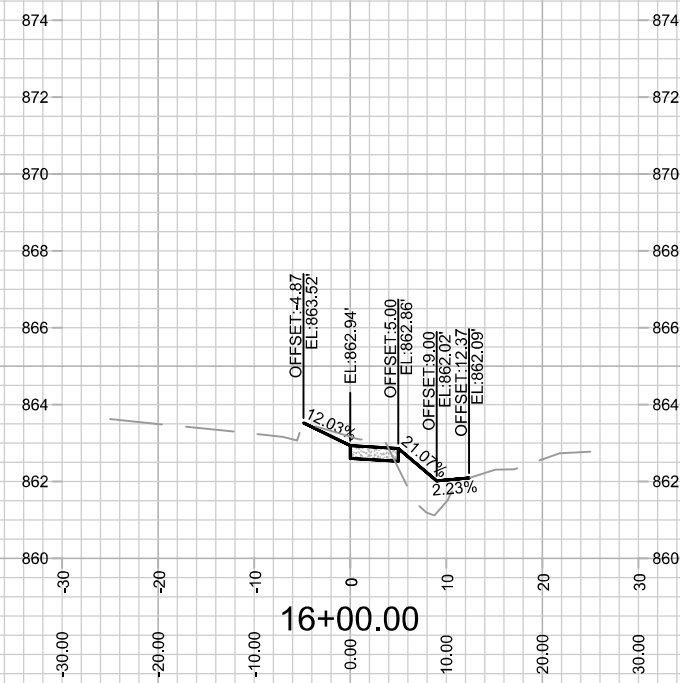
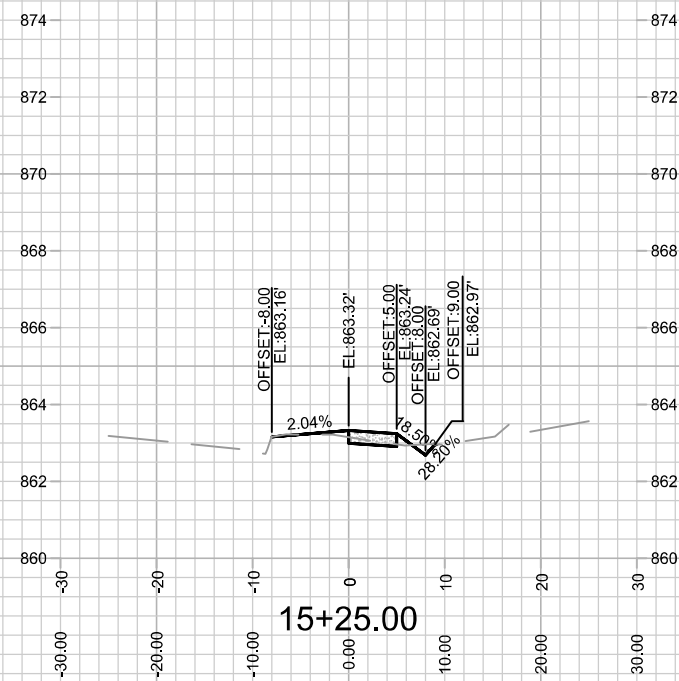
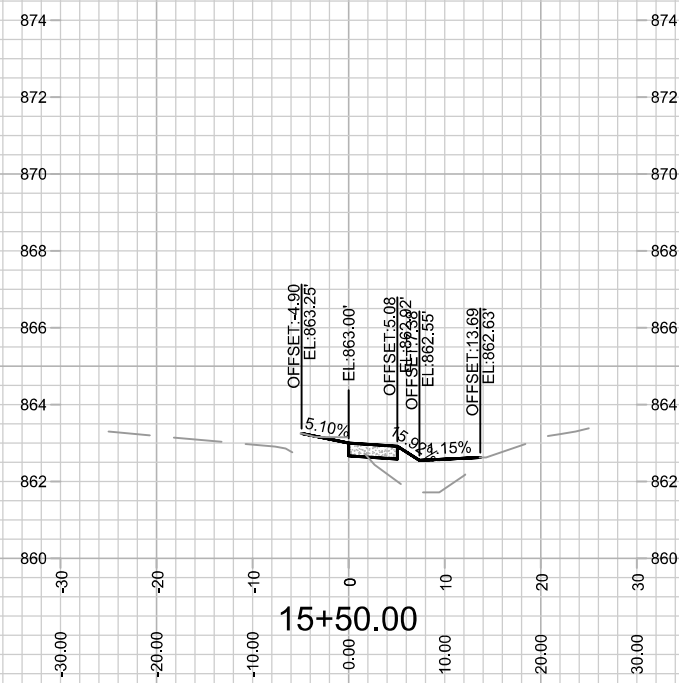
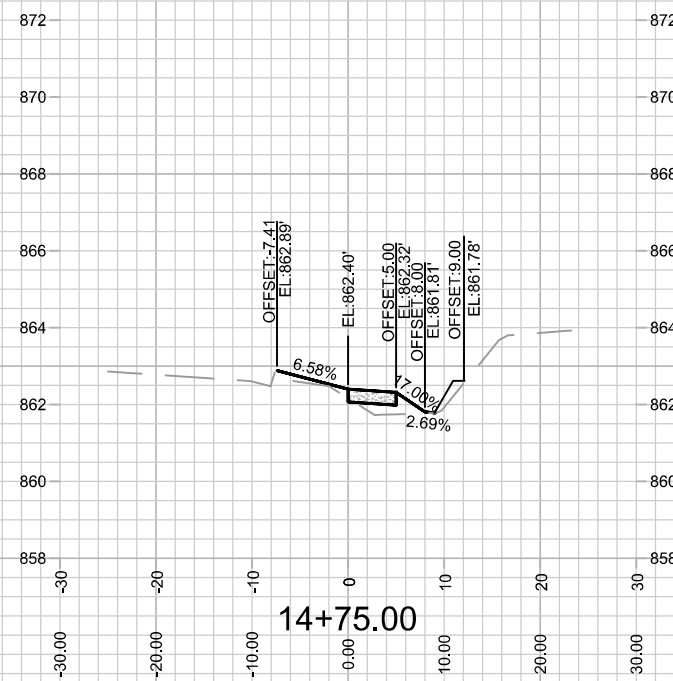
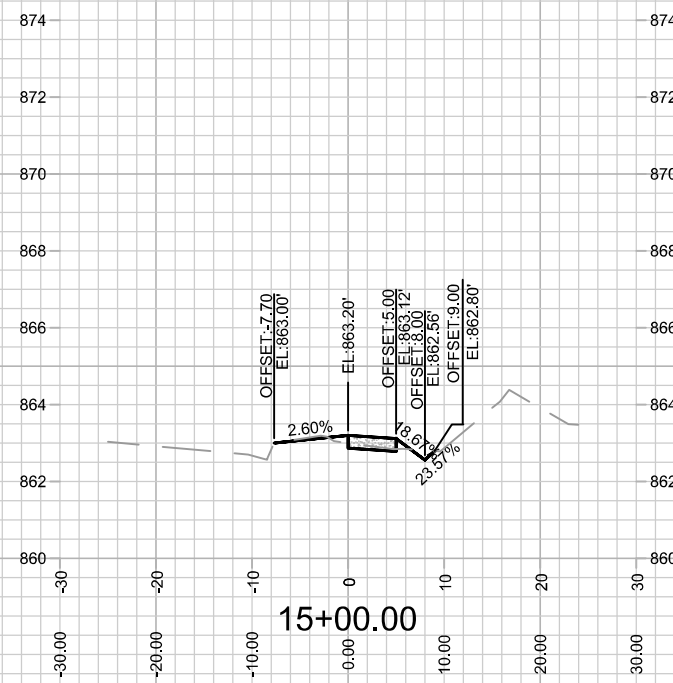
CROSS SECTIONS - FEMRITE DRIVE
3322 AGRICULTURE DRIVE CSM

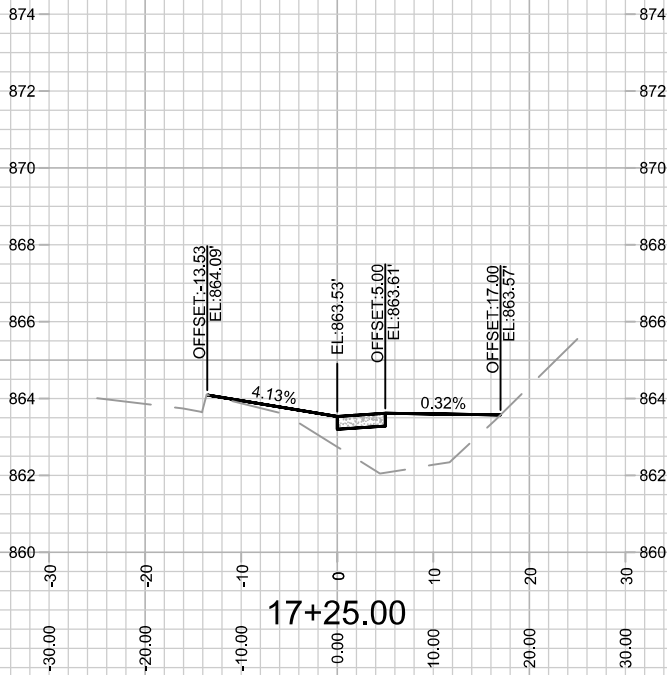
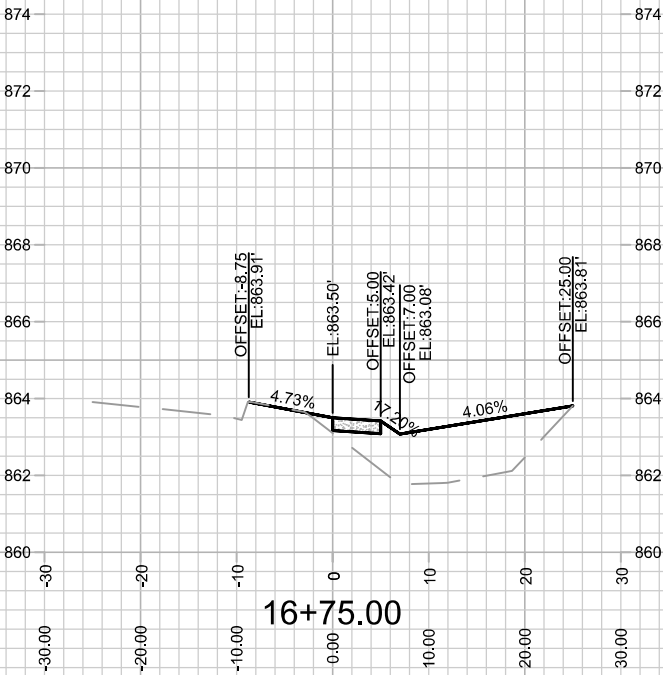
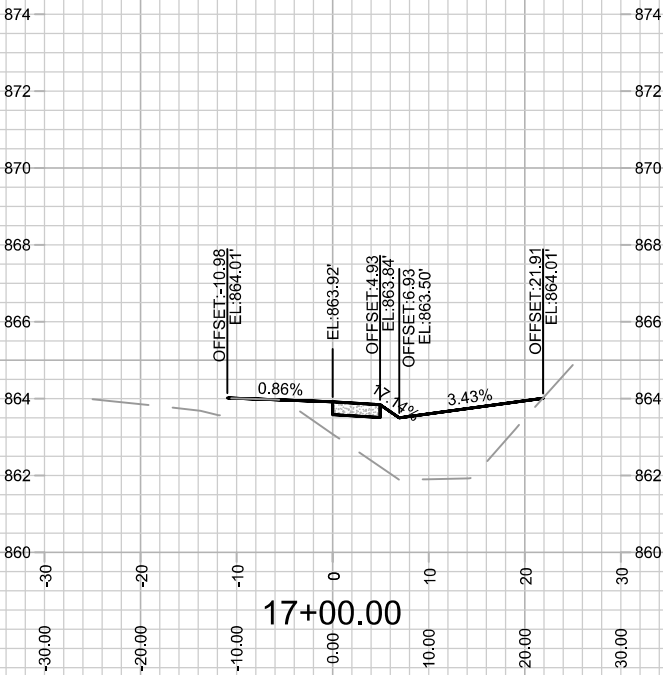
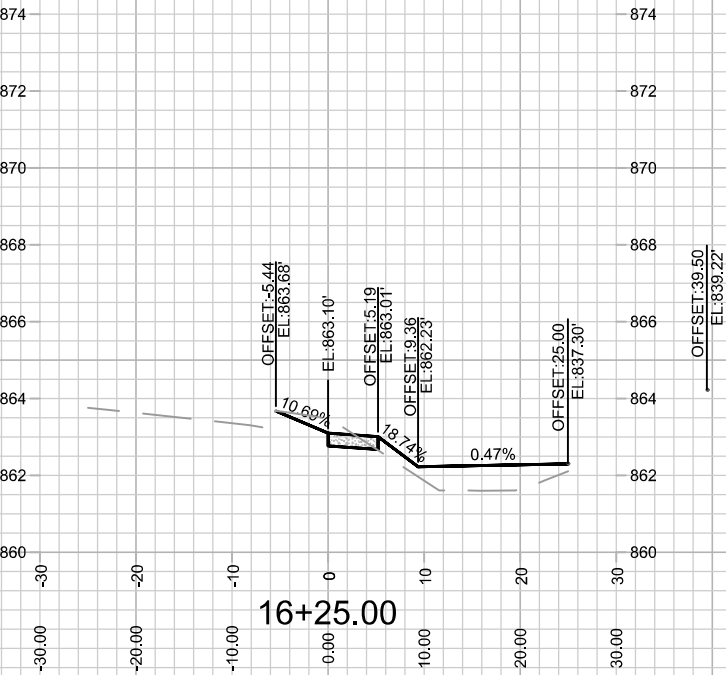
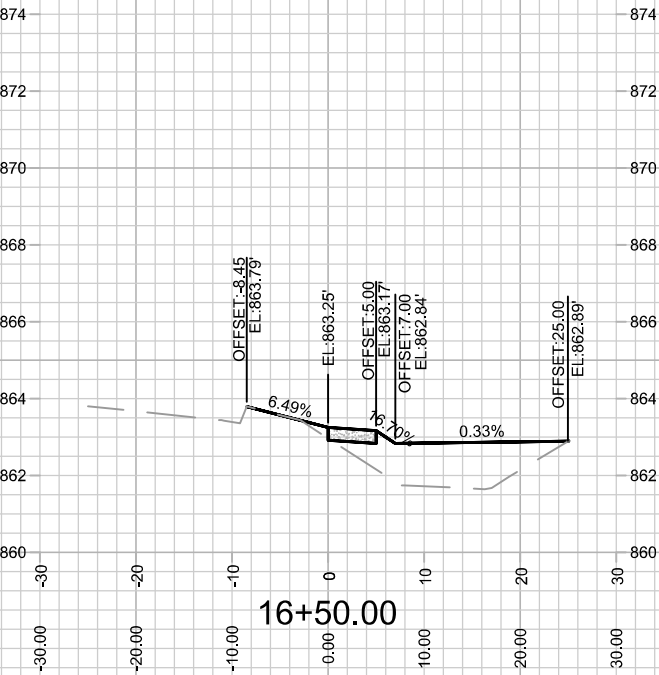
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 Designed By: NLJ
 15536 X3





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CROSS SECTIONS - FEMRITE DRIVE
3322 AGRICULTURE DRIVE CSM
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MADISON, WI
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