

Madison, Wisconsin

INDEX OF SHEETS

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CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

JPS 2015 12-07 Rev.

HAWK'S CROSSING AND THE FIRST ADDITION HAWK'S CROSSING PHASE 1

CITY PROJECT NO. 53B2373

CITY CONTRACT NO. 2373

PUBLIC IMPROVEMENT PROJECT APPROVED

AUGUST 5, 2014

BY THE COMMON COUNCIL OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN APPROVED BY:

[Signature] 9/10/14
 City Engineer Date

STREET DESIGNED BY:

SANITARY SEWER DESIGNED BY:

WATER DESIGNED BY:

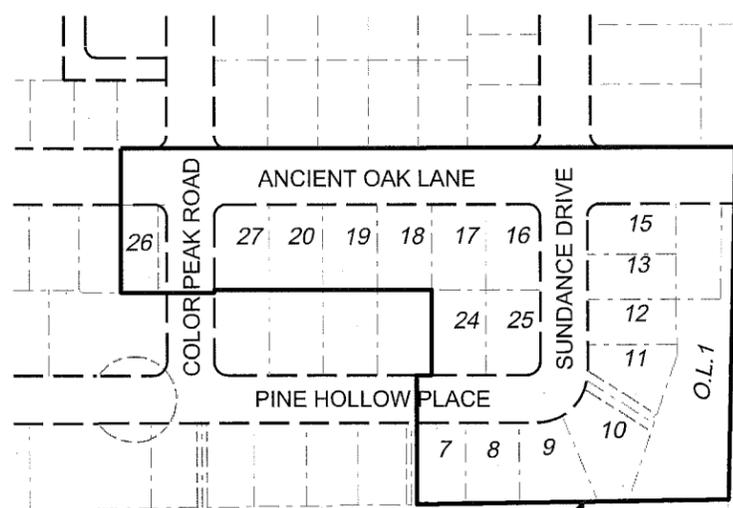
STORM SEWER DESIGNED BY:

PLOT SCALE:

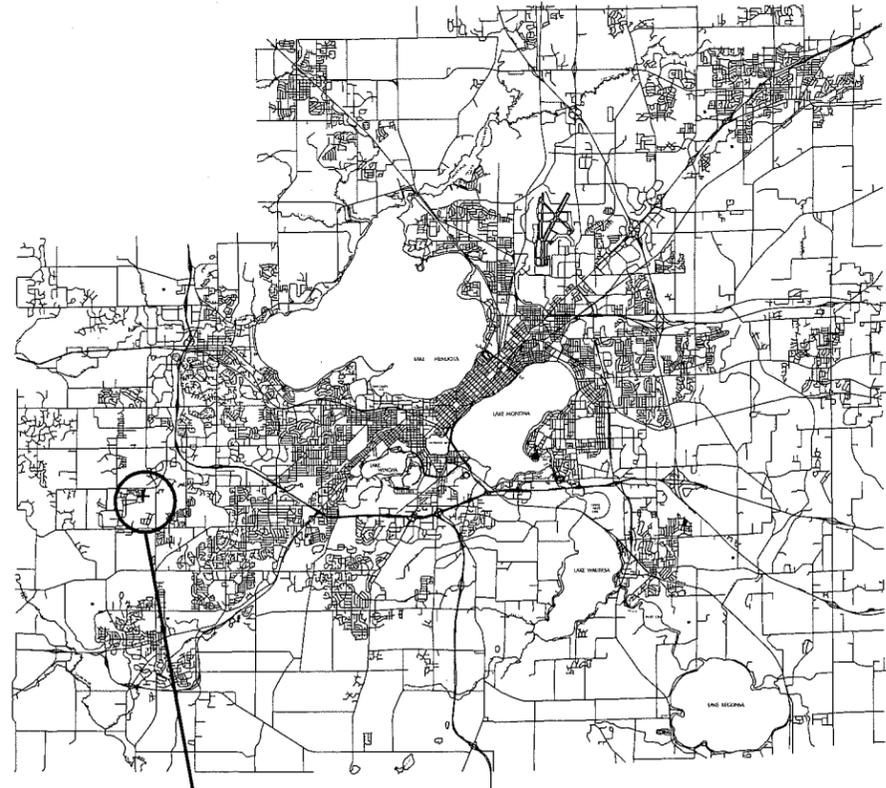
PLOT NAME:

REV. DATE:

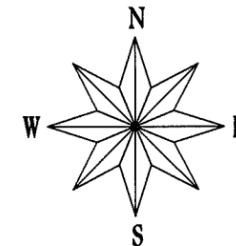
ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PHASE 1



PROJECT LOCATION



THE LOCATION AND INFORMATION FOR PROPOSED NEW TREES, IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS ARE APPROXIMATE AND ARE SHOWN FOR REFERENCE ONLY. THE LOCATIONS, SPECIFICATIONS AND PLANTING METHODS OF ALL PROPOSED NEW OR REPLACEMENT TREES IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE APPROVED BY THE CITY FORESTER PRIOR TO INSTALLATION.

NO TREES IN THE RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE TRIMMED, PRUNED, REMOVED OR ADVERSELY AFFECTED IN ANY WAY UNTIL THE DEVELOPER HAS RECEIVED WRITTEN PERMISSION FROM THE CITY ENGINEER OR CITY FORESTER. SAID WRITTEN PERMISSION SHALL INCLUDE LANGUAGE INDICATING THAT SECTION 10.101 OF THE MADISON GENERAL ORDINANCES AND ADMINISTRATIVE PROCEDURE MEMORANDUM NO. 6-2, REFERING TO NOTIFICATION OF PROPERTY OCCUPANTS AND/OR OWNERS, HAS BEEN COMPLIED WITH.

ALL PAVEMENT WITHIN THE COLOR PEAK ROAD, PINE HOLLOW PLACE AND SUNDANCE DRIVE RIGHT-OF-WAYS IS TO BE TYPE A PAVEMENT.

ALL PAVEMENT WITHIN THE ANCIENT OAK LANE RIGHT-OF-WAY IS TO BE TYPE B PAVEMENT.

UNDERDRAINS SHALL BE INSTALLED, PER STANDARD DETAIL DRAWING 4.05 FOR 75' ON EACH SIDE OF THE LOW POINT, OR TO THE NEAREST CURB HIGH POINT. ALL UNDERDRAIN SHALL BE WRAPPED.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADES OF 0.5% TOWARD STORM SEWER INLETS.

PAVEMENT CROSS SLOPES SHALL BE 2% AND TERRACES SHALL SLOPE AT A 4% GRADE TOWARD THE GUTTER.

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL BE 2%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.03. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.4% AND SHALL DRAIN TOWARD STORM SEWER INLETS. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4:00:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENT FROM THE CITY ENGINEER PRIOR TO STAKING THIS PROJECT.

CURB STATION AND OFFSETS SHALL BE TO THE FACE OF CURB UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE TOP OF CURB (OR EXTENDED TOP OF CURB FOR DRIVEWAYS OR RAMPS) UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO S.D.D. 3.05

CONVENTIONAL SIGNS

FIELD VERIFY ALL UTILITY LOCATIONS

GAS ——— G ———

STORM SEWER ——— ST ———

SANITARY SEWER ——— SAN ———

WATER ——— W ———

OVERHEAD ELECTRIC ——— OH ———

POWER POLE 

ADA COMPLIANT RAMP W/
DETECTABLE WARNING FIELD 

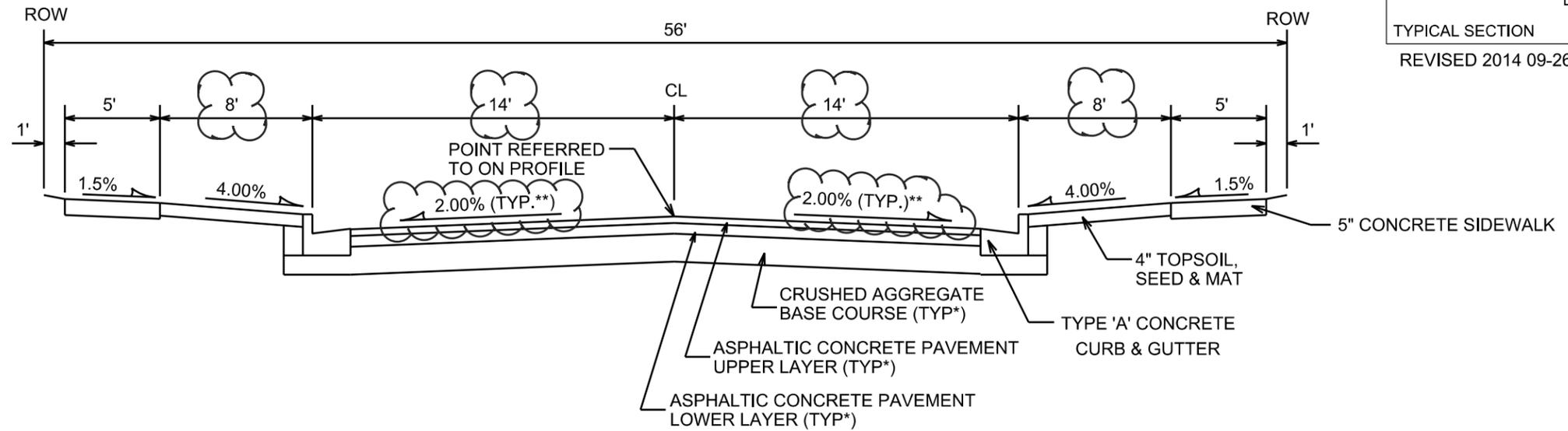
COMBUSTIBLE FLUIDS 

PLOT SCALE: _____

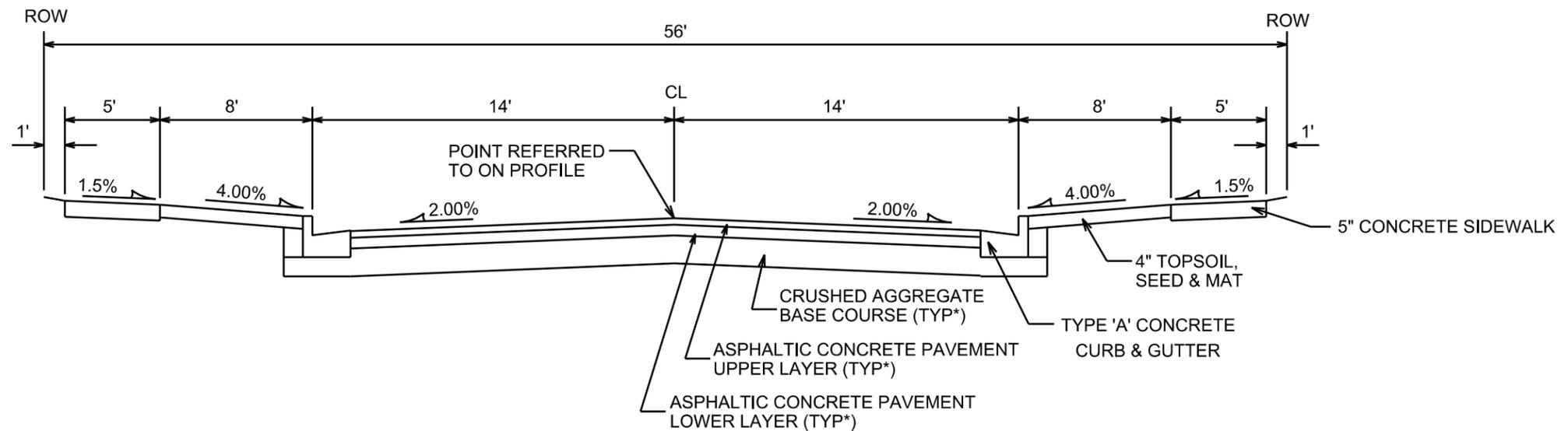
PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



TYPICAL SECTION
 PINE HOLLOW PLACE AND SUNDANCE DRIVE
 NOT TO SCALE



TYPICAL SECTION
 COLOR PEAK ROAD
 NOT TO SCALE

CITY OF MADISON MINIMUM PAVEMENT DESIGN

TYPE	CRUSHED AGG. BASE COURSE		ASPHALTIC CONCRETE PAVEMENT			
	LOWER LAYER GRADATION 1	UPPER LAYER GRADATION 2	LOWER LAYER TYPE	LOWER LAYER THICKNESS	UPPER LAYER TYPE	UPPER LAYER THICKNESS
A	6"	4"	E-0.3	1.75"	E-0.3	1.75"
B	6"	4"	E-1	2.50"	E-1	2"
C	6"	4"	E-3	3.50"	E-3	2"

NOTES:

* PINE HOLLOW PLACE, SUNDANCE DRIVE, AND COLOR PEAK ROAD TO BE CONSTRUCTED AS TYPE 'A' PAVEMENT PER CITY OF MADISON MINIMUM PAVEMENT DESIGN

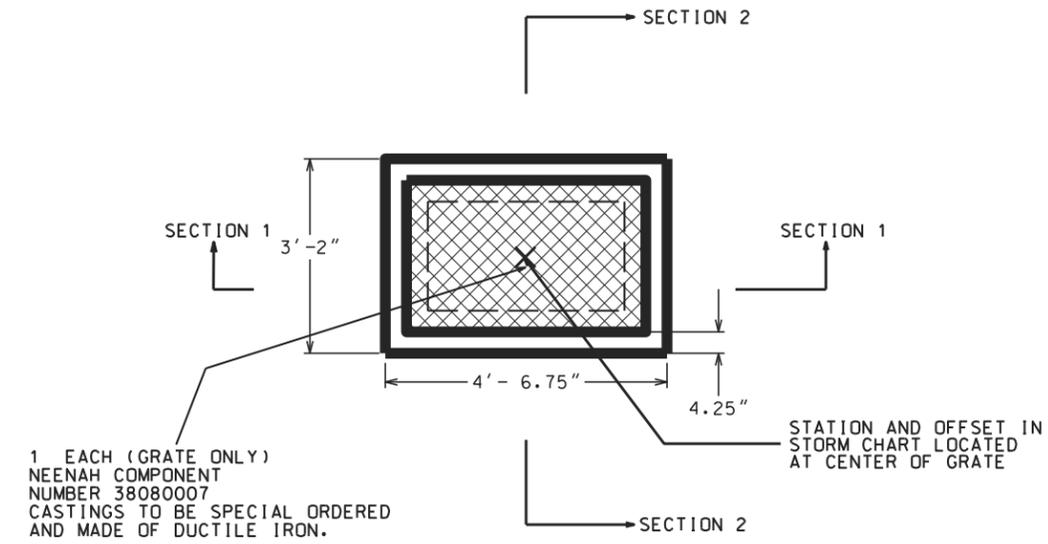
** CROSS SLOPES VARY ON SUNDANCE DRIVE FROM STA 6+25 TO STA 8+25. SEE PLANS FOR CURB GRADES

PLOT SCALE: _____

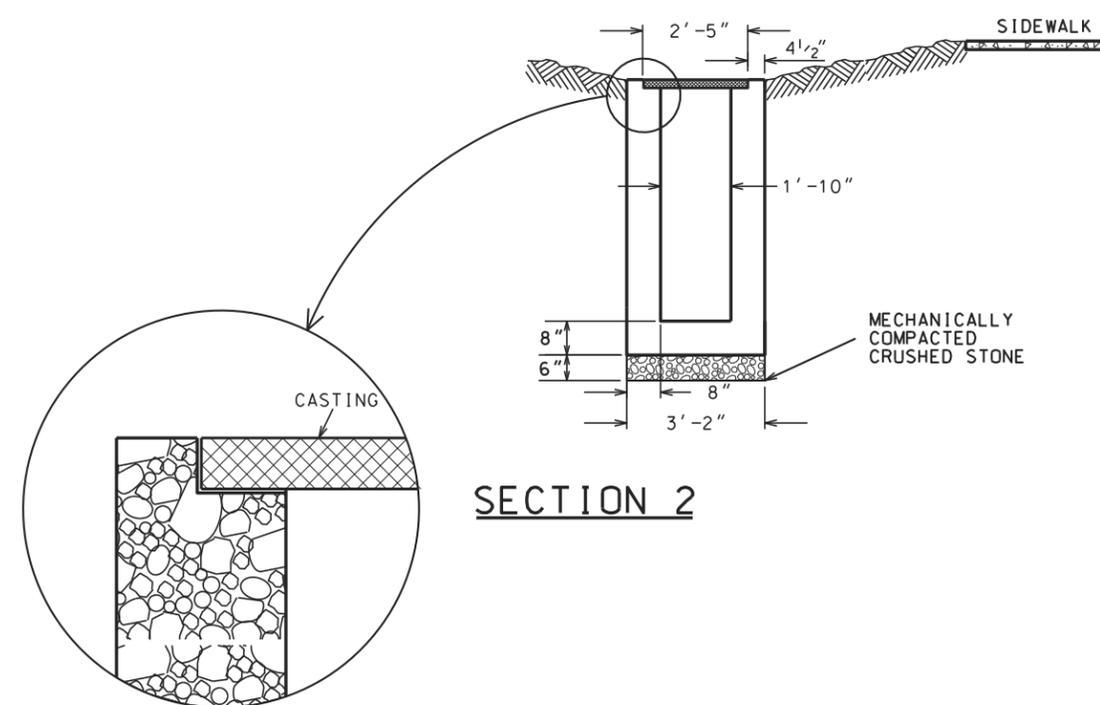
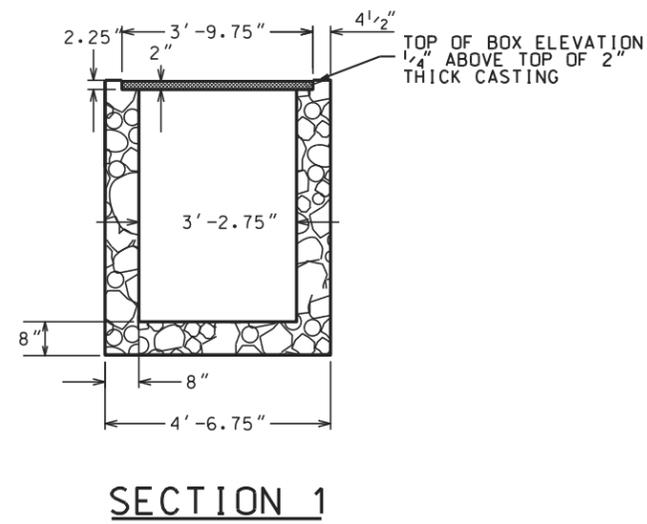
PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PLAN VIEW

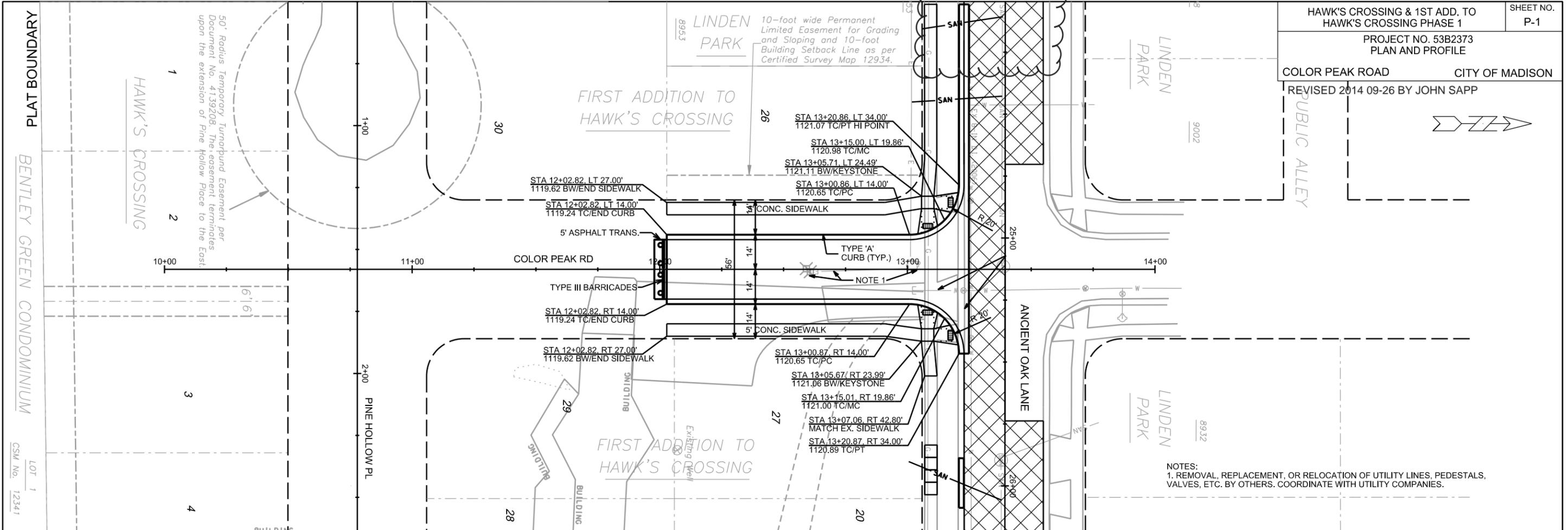


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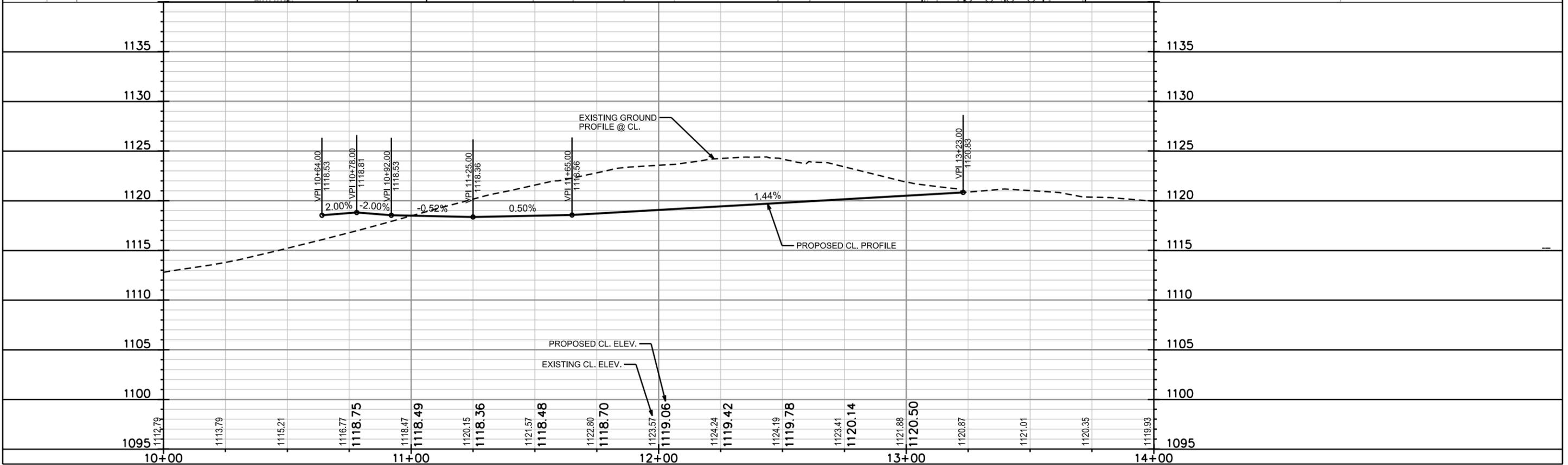
PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



NOTES:
1. REMOVAL, REPLACEMENT, OR RELOCATION OF UTILITY LINES, PEDESTALS, VALVES, ETC. BY OTHERS. COORDINATE WITH UTILITY COMPANIES.

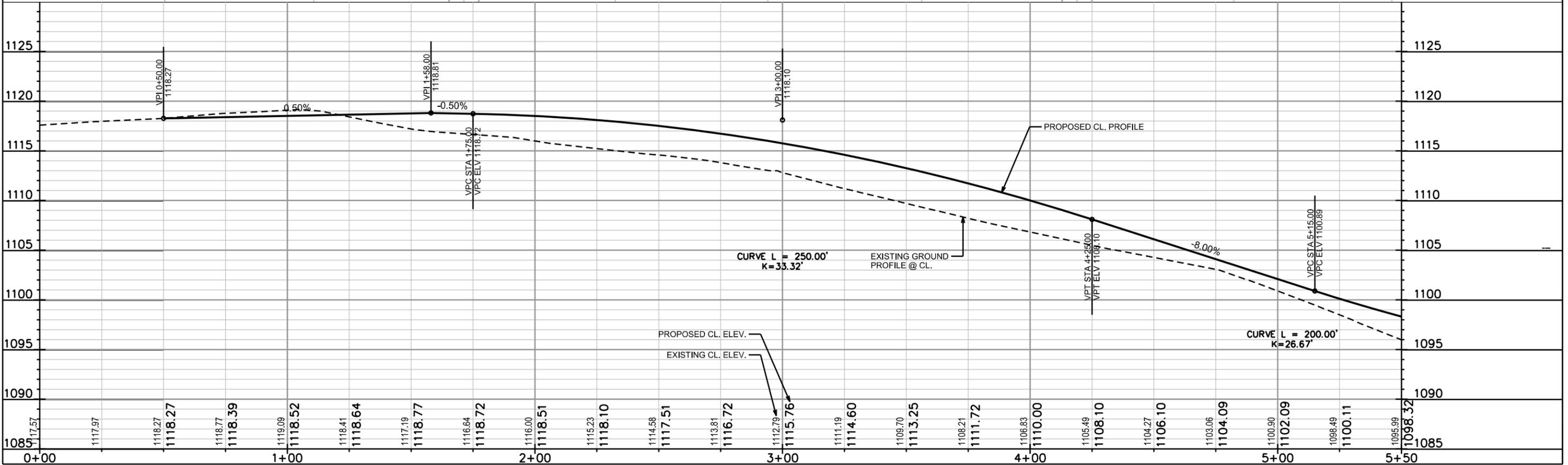
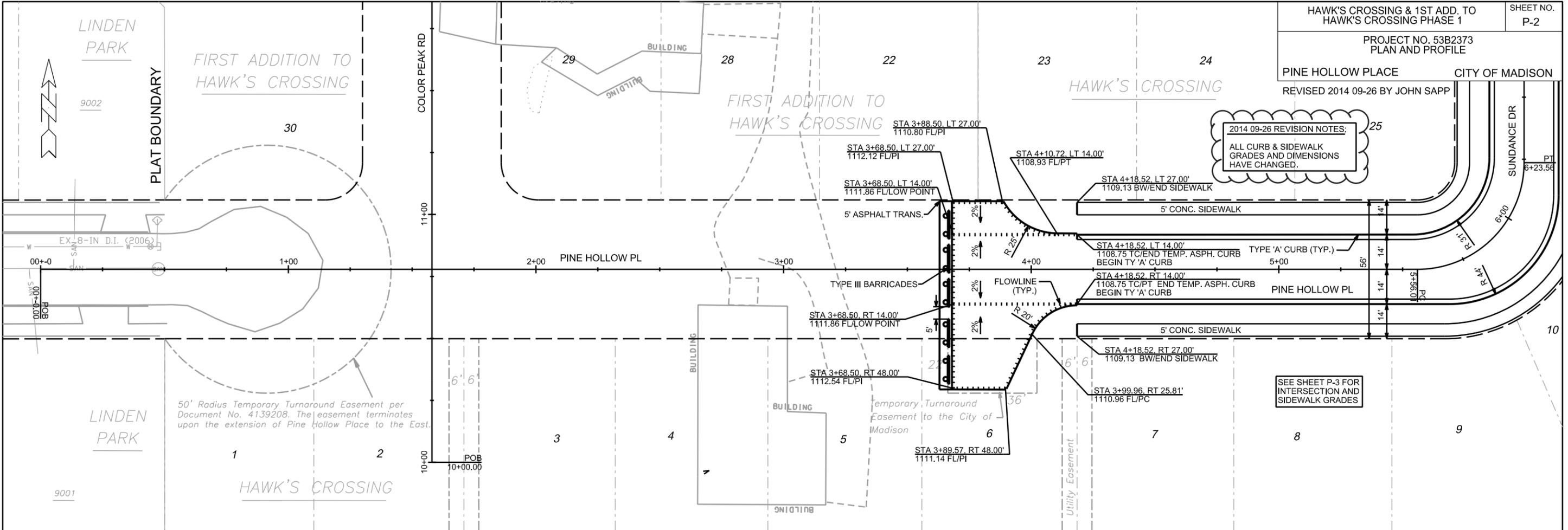


PLOT SCALE:

PLOT NAME:

REV. DATE:

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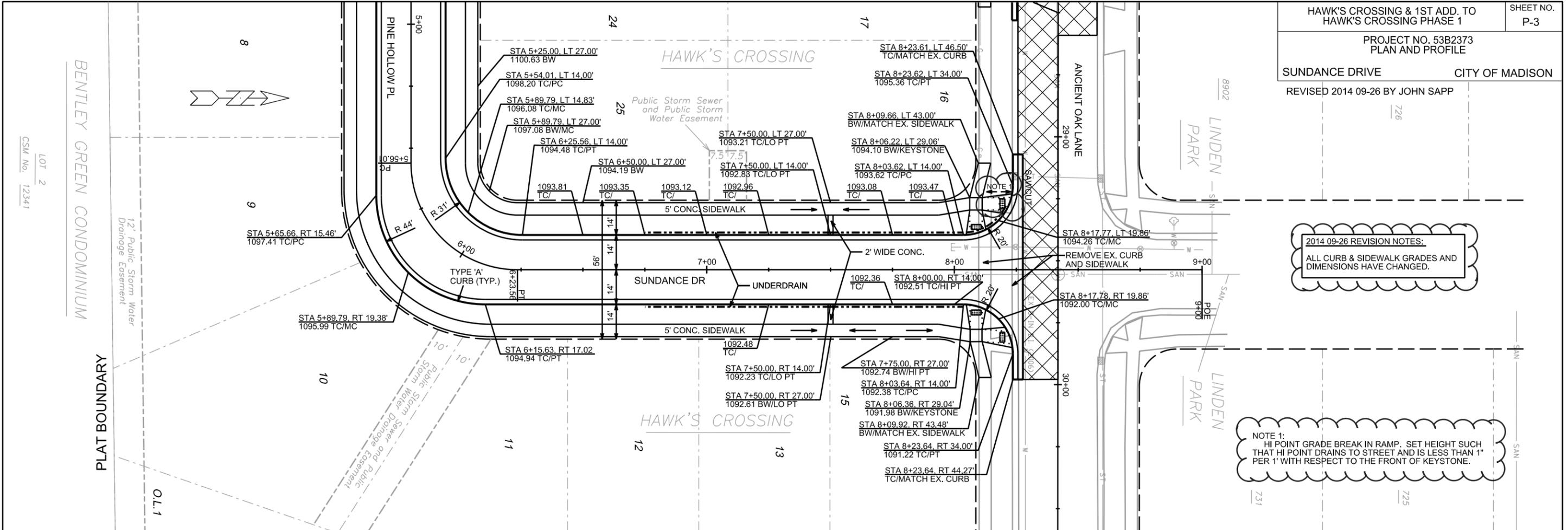


PLOT SCALE: _____

PLOT NAME: _____

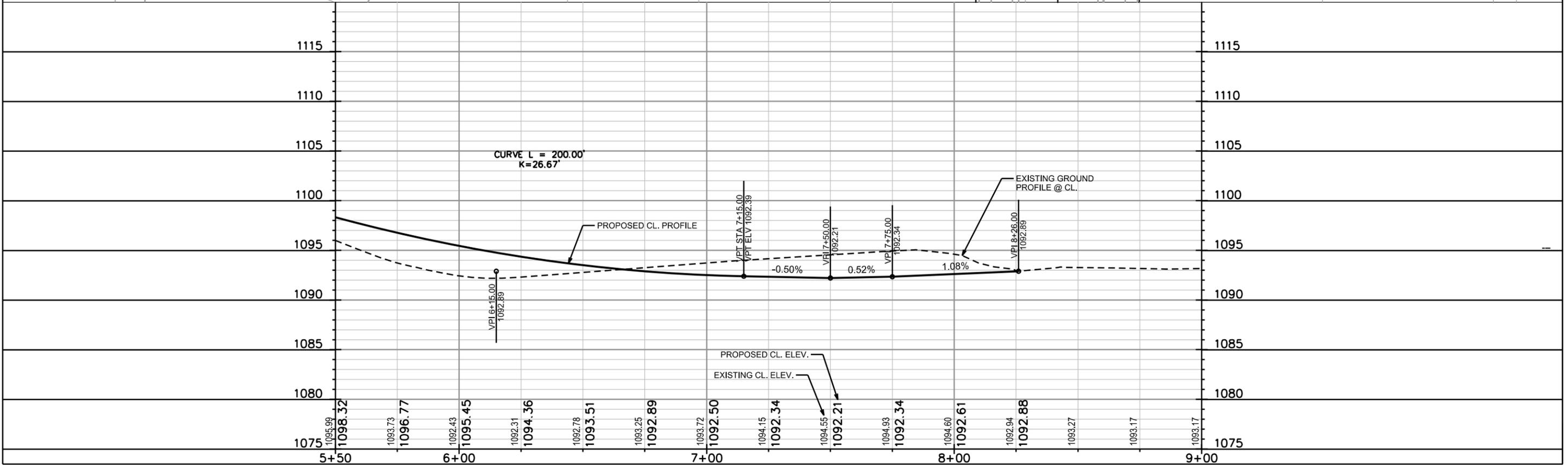
REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

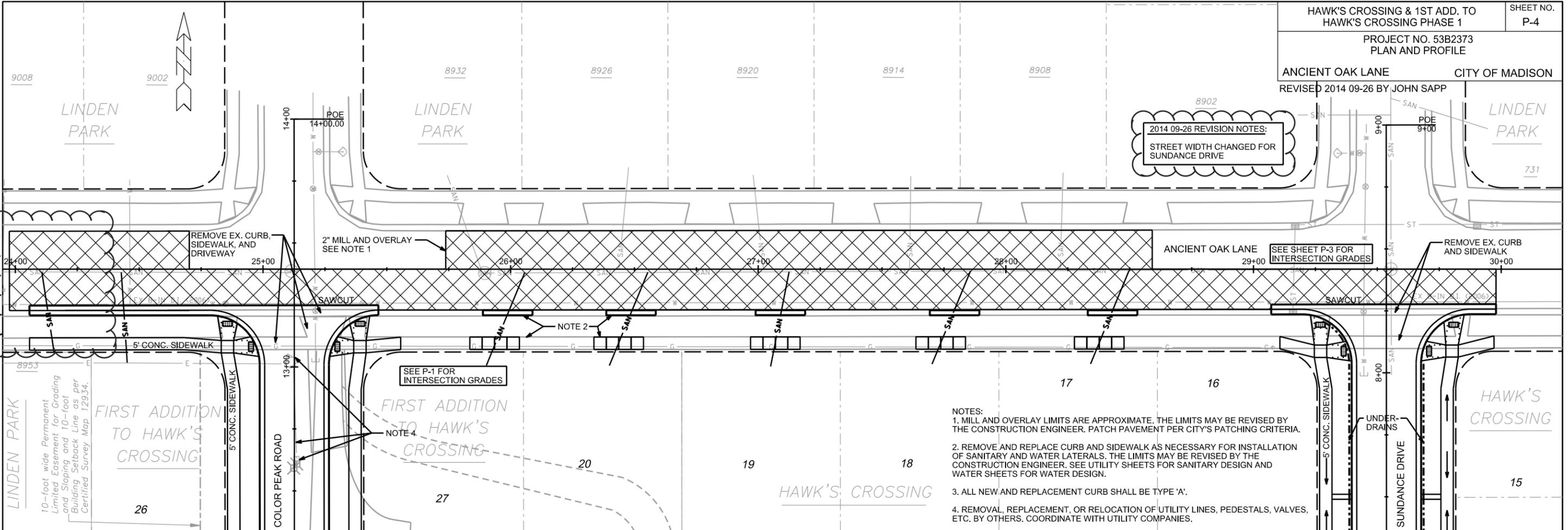


2014 09-26 REVISION NOTES:
 ALL CURB & SIDEWALK GRADES AND DIMENSIONS HAVE CHANGED.

NOTE 1:
 HI POINT GRADE BREAK IN RAMP. SET HEIGHT SUCH THAT HI POINT DRAINS TO STREET AND IS LESS THAN 1" PER 1' WITH RESPECT TO THE FRONT OF KEYSTONE.

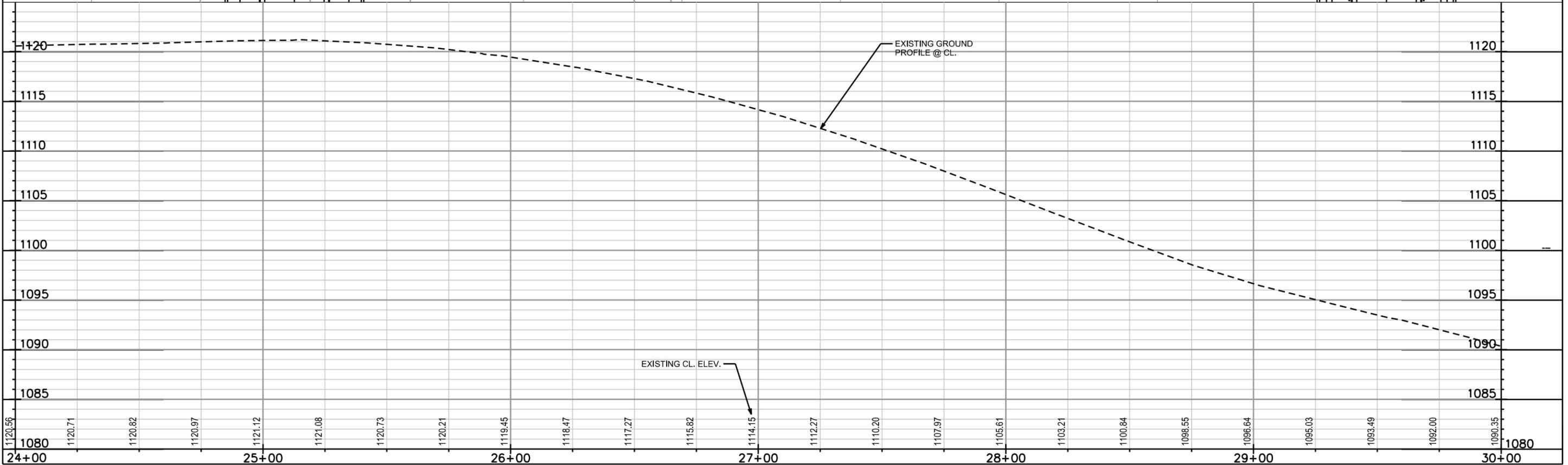


PLOT SCALE: _____ PLOT NAME: _____ REV. DATE: _____ ORIGINATOR: CITY OF MADISON, STREETS DIVISION



2014 09-26 REVISION NOTES:
 STREET WIDTH CHANGED FOR SUNDANCE DRIVE

- NOTES:
- MILL AND OVERLAY LIMITS ARE APPROXIMATE. THE LIMITS MAY BE REVISED BY THE CONSTRUCTION ENGINEER. PATCH PAVEMENT PER CITY'S PATCHING CRITERIA.
 - REMOVE AND REPLACE CURB AND SIDEWALK AS NECESSARY FOR INSTALLATION OF SANITARY AND WATER LATERALS. THE LIMITS MAY BE REVISED BY THE CONSTRUCTION ENGINEER. SEE UTILITY SHEETS FOR SANITARY DESIGN AND WATER SHEETS FOR WATER DESIGN.
 - ALL NEW AND REPLACEMENT CURB SHALL BE TYPE 'A'.
 - REMOVAL, REPLACEMENT, OR RELOCATION OF UTILITY LINES, PEDESTALS, VALVES, ETC. BY OTHERS. COORDINATE WITH UTILITY COMPANIES.



PLOT SCALE:

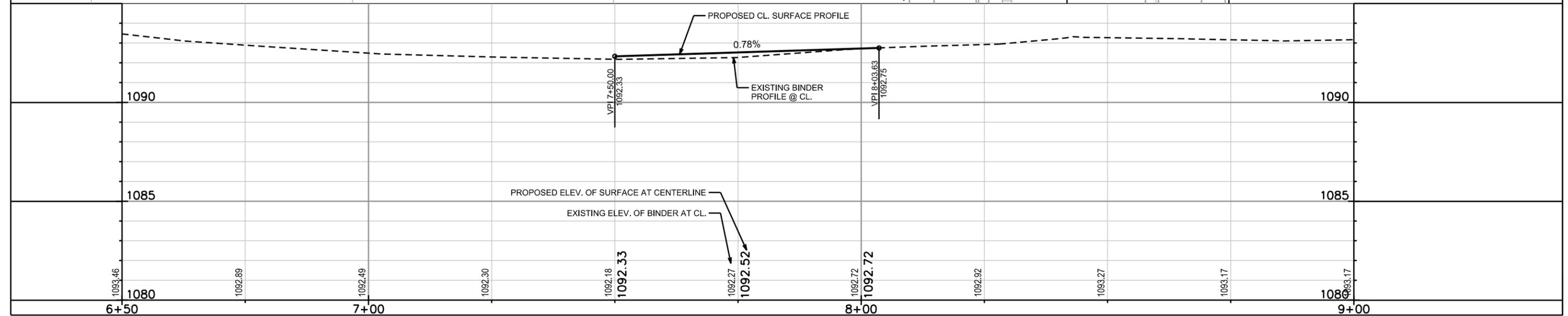
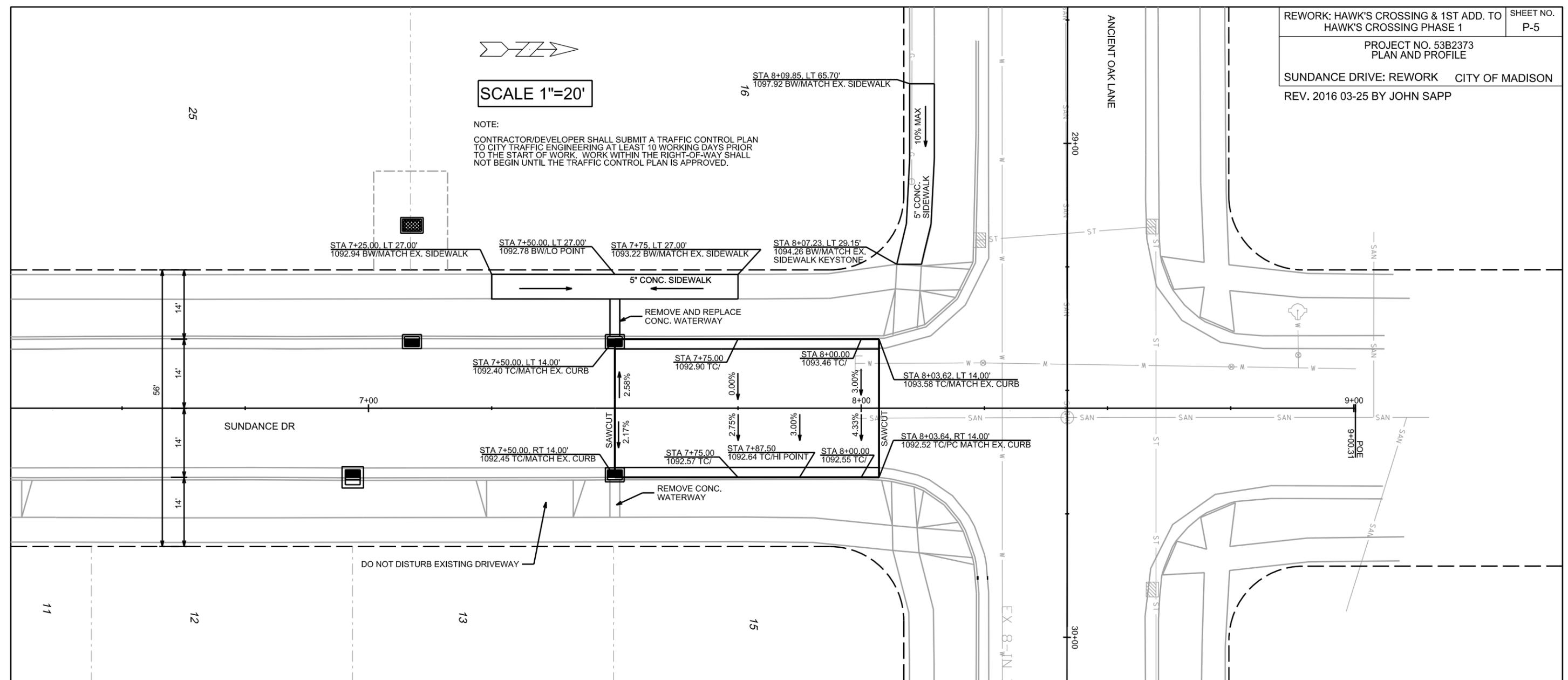
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REV. DATE:

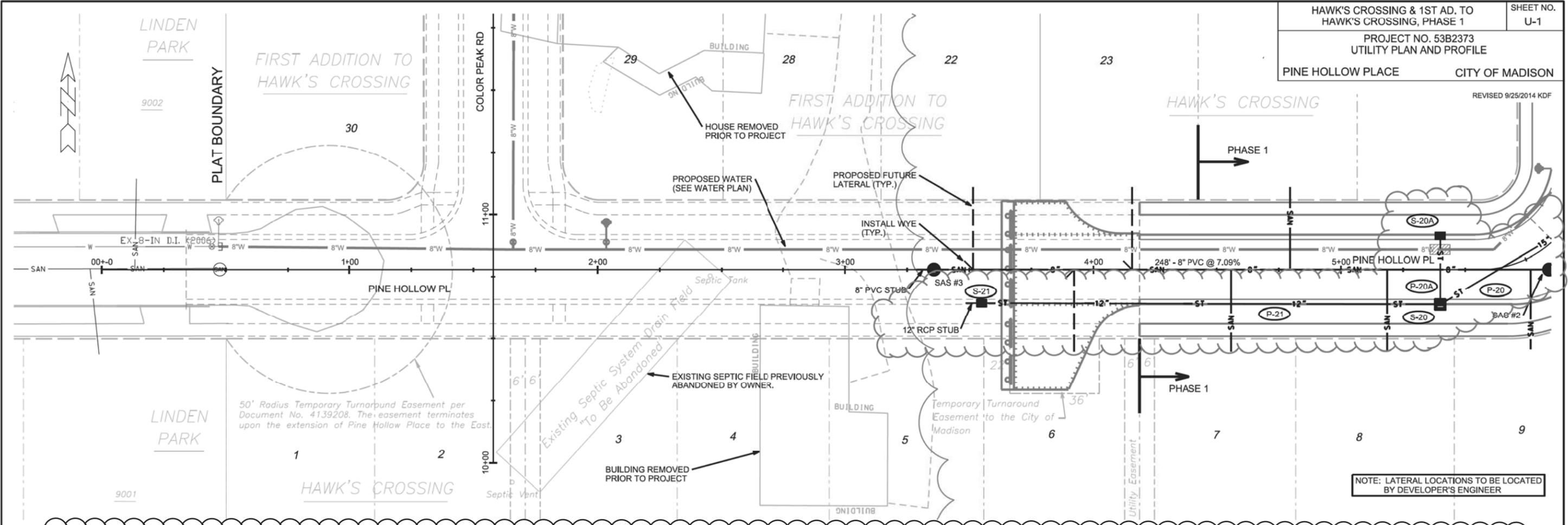
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

SCALE 1"=20'

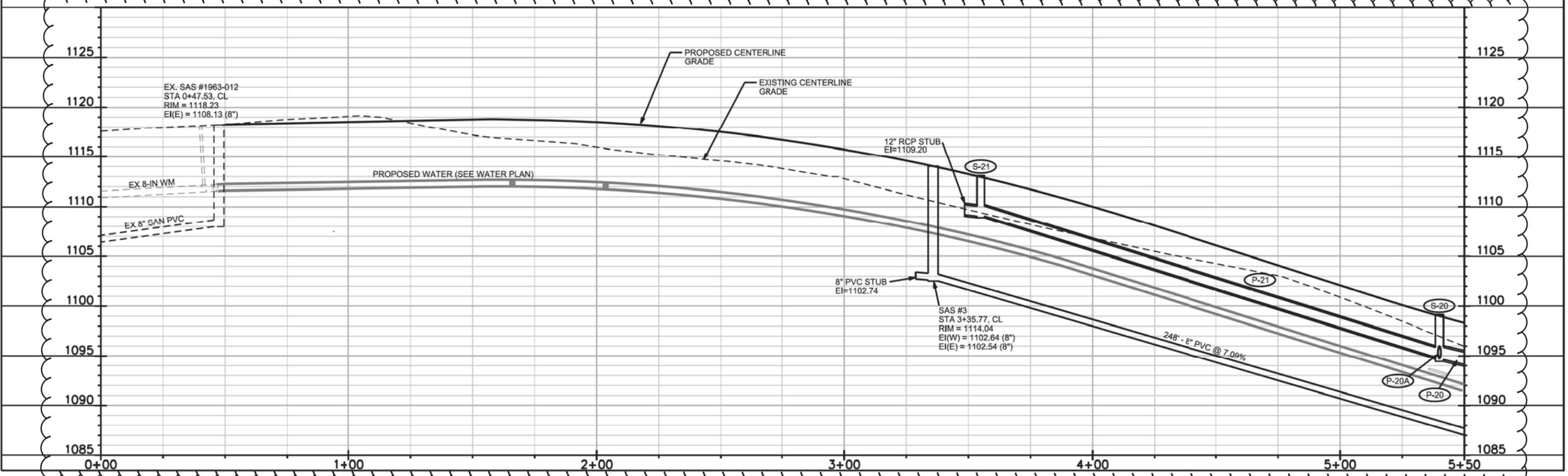
NOTE:
 CONTRACTOR/DEVELOPER SHALL SUBMIT A TRAFFIC CONTROL PLAN TO CITY TRAFFIC ENGINEERING AT LEAST 10 WORKING DAYS PRIOR TO THE START OF WORK. WORK WITHIN THE RIGHT-OF-WAY SHALL NOT BEGIN UNTIL THE TRAFFIC CONTROL PLAN IS APPROVED.



PLOT SCALE: _____
 PLOT NAME: _____
 REV. DATE: _____
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION



NOTE: LATERAL LOCATIONS TO BE LOCATED BY DEVELOPER'S ENGINEER

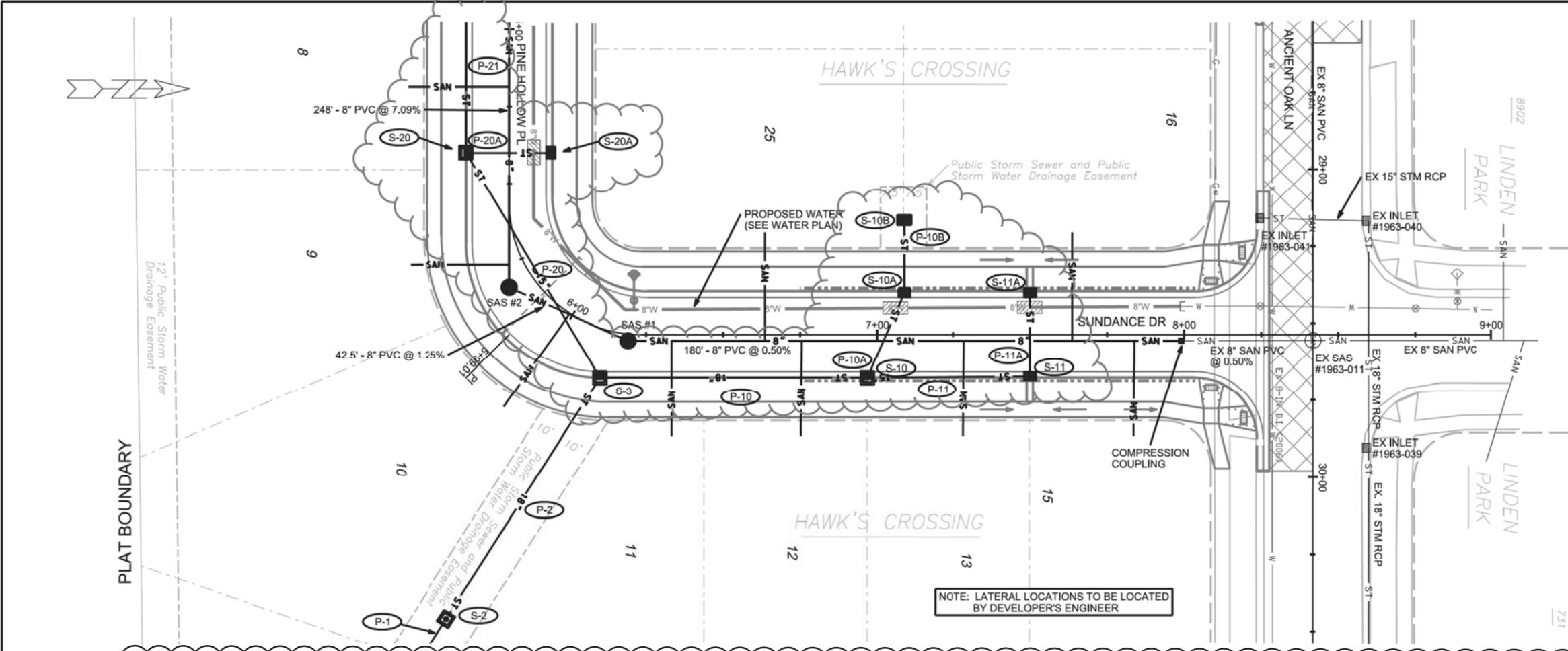


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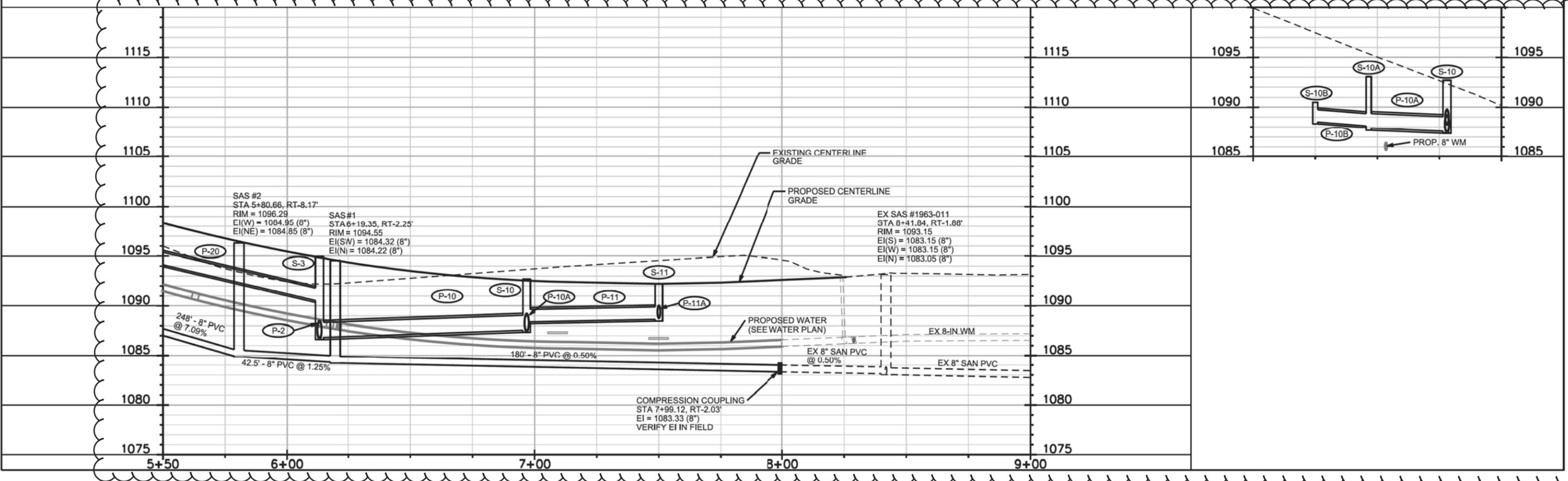
PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



NOTE: LATERAL LOCATIONS TO BE LOCATED BY DEVELOPER'S ENGINEER

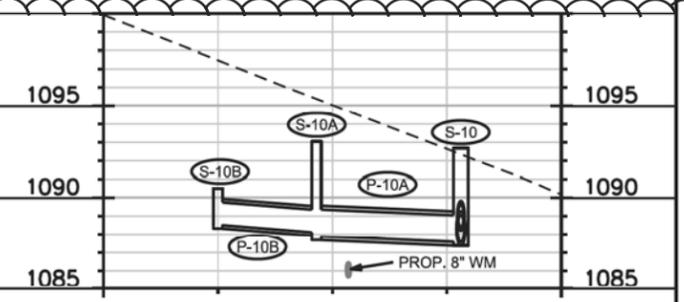


SAS #2
 STA 5+80.66, RT-8.17'
 RIM = 1096.29
 EI(W) = 1084.95 (8")
 EI(N) = 1084.85 (8")

SAS #1
 STA 6+19.35, RT-2.25'
 RIM = 1094.55
 EI(S/W) = 1084.32 (8")
 EI(N) = 1084.22 (8")

EX SAS #1963-011
 STA 8+41.84, RT-1.88'
 RIM = 1093.15
 EI(S) = 1083.15 (8")
 EI(W) = 1083.15 (8")
 EI(N) = 1083.05 (8")

COMPRESSION COUPLING
 STA 7+99.12, RT-2.03'
 EI = 1083.33 (8")
 VERIFY EI IN FIELD



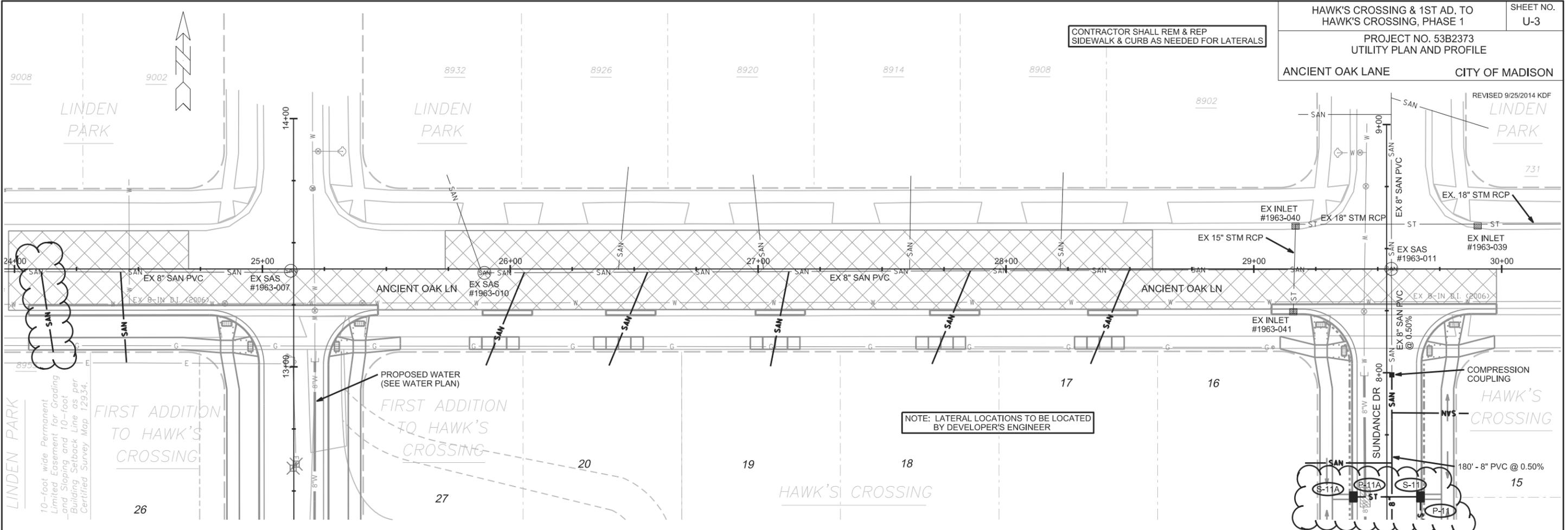
PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CONTRACTOR SHALL REM & REP SIDEWALK & CURB AS NEEDED FOR LATERALS



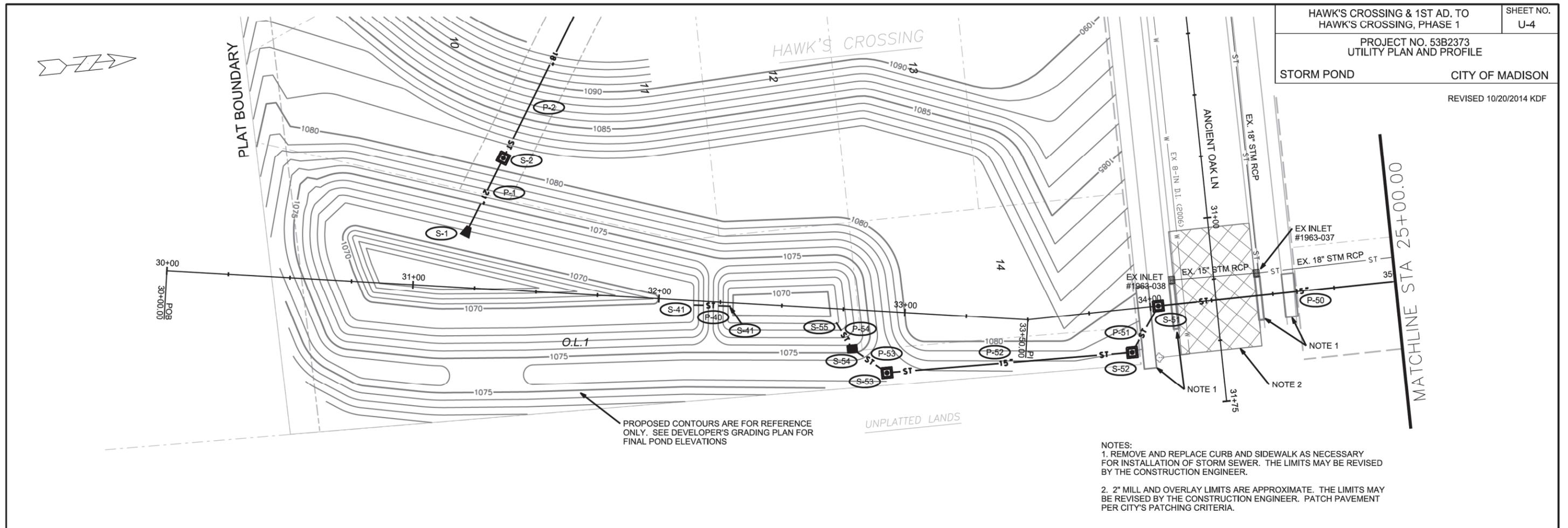
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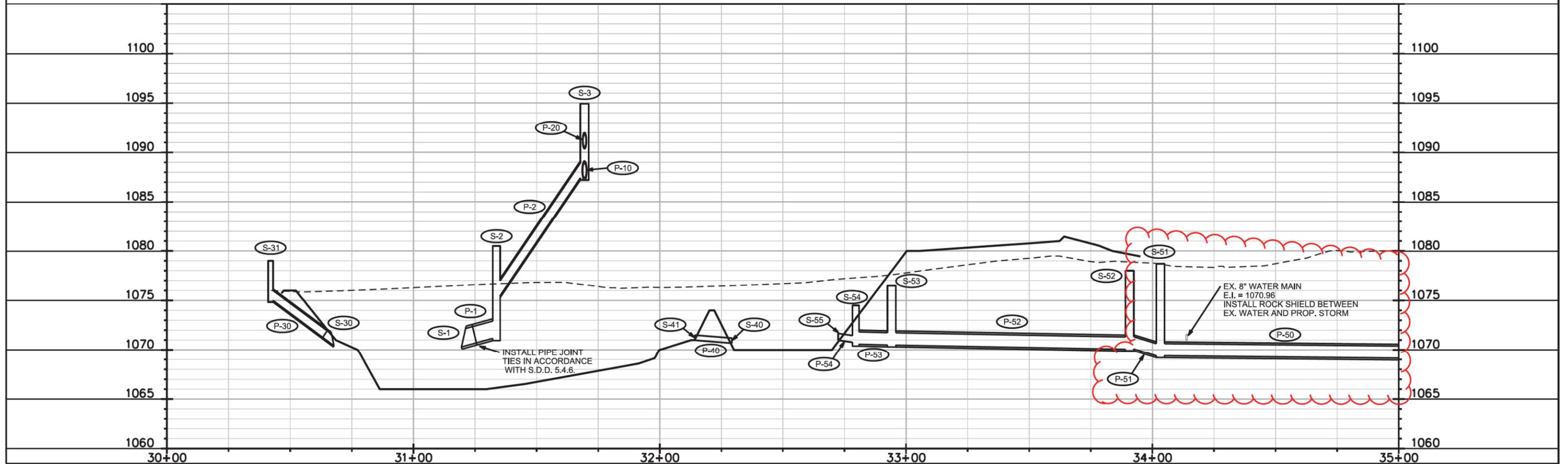
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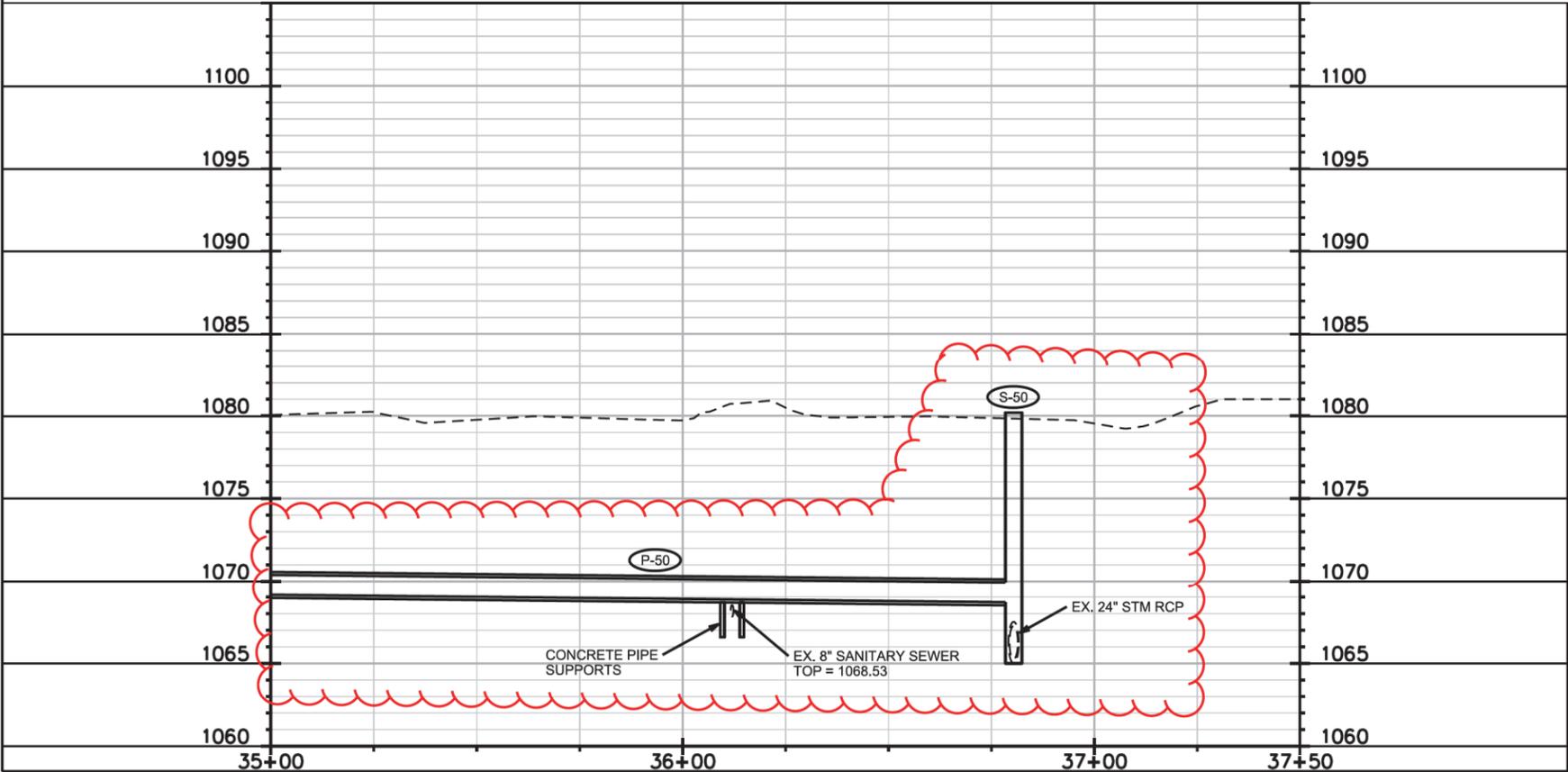
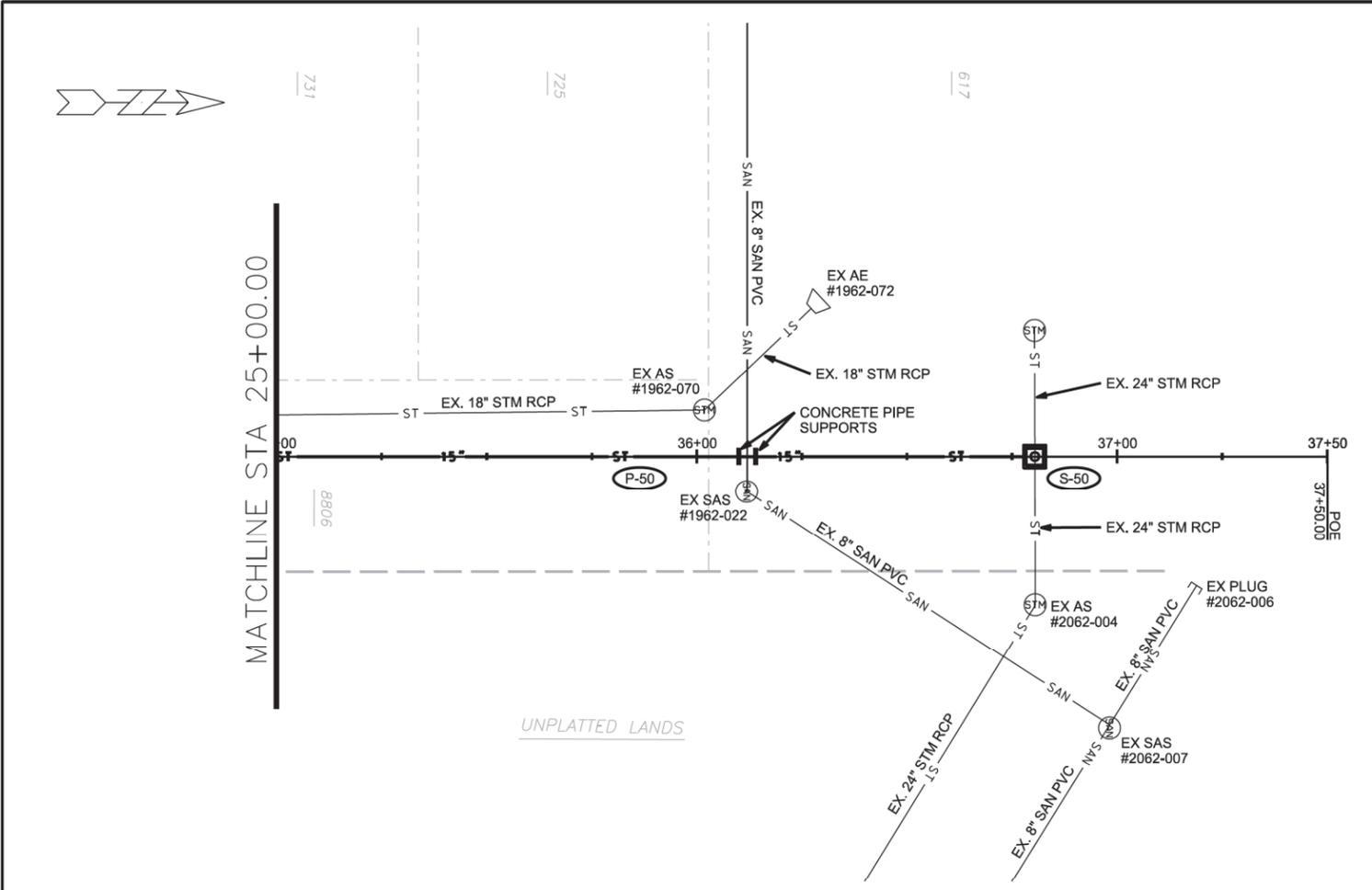


PROPOSED CONTOURS ARE FOR REFERENCE ONLY. SEE DEVELOPER'S GRADING PLAN FOR FINAL POND ELEVATIONS

- NOTES:
 1. REMOVE AND REPLACE CURB AND SIDEWALK AS NECESSARY FOR INSTALLATION OF STORM SEWER. THE LIMITS MAY BE REVISED BY THE CONSTRUCTION ENGINEER.
 2. 2" MILL AND OVERLAY LIMITS ARE APPROXIMATE. THE LIMITS MAY BE REVISED BY THE CONSTRUCTION ENGINEER. PATCH PAVEMENT PER CITY'S PATCHING CRITERIA.



PLOT SCALE: _____
 PLOT NAME: _____
 REV. DATE: _____
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PLOT SCALE: _____
 PLOT NAME: _____
 REV. DATE: _____
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION

SANITARY SEWER SCHEDULE

HAWK'S CROSSING & 1ST AD. TO
HAWK'S CROSSING , PH 1 & 2

SHEET NO.
U-6

PROJECT NO. 53B2373
SANITARY SEWER SCHEDULE
CITY OF MADISON

PROPOSED SANITARY STRUCTURES

SAS NO.	STATION	LOCATION (OFFSET)	TOP OF CASTING	E.I.	DEPTH	NOTES
SUNDANCE DR						
SAS #1	6+19.35	RT-2.25	1094.55	1084.22	10.33	-
SAS #2	5+80.66	RT-8.17	1096.29	1084.85	11.44	-
PINE HOLLOW PL						
SAS #3	3+35.77	CL	1114.04	1102.54	11.50	-

PROPOSED SANITARY PIPES

FROM (DNSTM)	TO (UPSTM)	DWNSTRM E.I.	UPSTRM E.I.	PLAN (PAY) LGTH (FT)	SLOPE (%)	PIPE SIZE	PVC TYPE	NOTES
SUNDANCE DR								
COMP. COUPLING	SAS #1	1083.33	1084.22	180	0.50%	8"	SDR-35	(1)
SAS #1	SAS #2	1084.32	1084.85	42.5	1.25%	8"	SDR-35	-
PINE HOLLOW PL								
SAS #2	SAS #3	1084.95	1102.54	248	7.09%	8"	SDR-35	-

SPECIFIC NOTES

(1) COMPRESSION COUPLING LOCATED @ STA 7+99.12, RT-2.03'. VERIFY E.I. IN FIELD.

STORM SEWER SCHEDULE

*REVISED 9/25/2014 KDF

**REVISED 10/20/2014 KDF

HAWK'S CROSSING & 1ST AD. TO
HAWK'S CROSSING , PH 1

SHEET NO.
U-7

PROJECT NO. 53B2373
STORM SEWER SCHEDULE

CITY OF MADISON

PROPOSED STORM STRUCTURES

STRUC. NO.	STATION	LOCATION (OFFSET)	TYPE	TOP OF CASTING	E.I.	DEPTH	NOTES
SUNDANCE DR							
* S-3	6+13.53	RT-15.58	3X3 SAS	1094.94	1086.83	8.11	W/ R-3067-7004-V
* S-10	6+96.81	RT-14.00	3X3 SAS	1092.68	1087.55	5.13	W/ R-3067-7004-V
* S-10A	7+08.82	LT-13.50	H INLET	1093.04	1087.86	5.18	W/ R-3067-7004-V
S-10B	7+08.82	LT-37.03	MODIFIED INLET	1090.50	1088.50	2.00	(4), FP
* S-11	7+50.00	RT-13.50	H INLET	1092.23	1088.64	3.59	W/ R-3067-7004-VB, LP, UD
* S-11A	7+50.00	LT-13.50	H INLET	1092.83	1089.04	3.79	W/ R-3067-7004-VB, LP, UD
PINE HOLLOW PL							
* S-20	5+39.98	RT-14.00	3X3 SAS	1099.15	1094.61	4.54	W/ R-3067-7004-V
* S-20A	5+39.98	LT-13.50	H INLET	1099.15	1095.01	4.14	W/ R-3067-7004-V
* S-21	3+54.98	RT-13.50	H INLET	1113.10	1109.06	4.04	W/ R-3067-7004-V
STORM POND							
S-1	31+19.73	LT-20.75	21" RCP AE	DNA	1070.33	DNA	(1), W/ GATE
S-2	31+33.70	LT-53.30	3X3 SAS	1080.50	1071.18	9.32	W/ R-1550-0054
S-30	30+67.40	LT-27.65	12" RCP AE	DNA	1070.50	DNA	(1), W/ GATE
S-31	30+42.05	LT-58.57	H INLET	1079.00	1075.00	4.00	W/ R-1878-B7G
S-40	32+29.29	RT-1.41	FREE END	DNA	1070.70	DNA	-
S-41	32+14.21	RT-1.81	FREE END	DNA	1071.00	DNA	-
S-50	36+80.41	CL	4X4 SAS	1080.20	1065.27	14.93	(3), W/ R-1550-0054
** S-51	34+03.25	CL	3X3 SAS	1078.70	1069.39	9.31	W/ R-1550-0054
S-52	33+90.75	RT-17.50	3X3 SAS	1078.00	1070.07	7.93	W/ R-1550-0054
S-53	32+94.13	RT-24.60	3X3 SAS	1076.50	1070.51	5.99	W/ R-1550-0054
S-54	32+79.50	RT-15.78	H INLET	1074.50	1070.59	3.91	W/ R-1878-B7G
S-55	32+72.42	RT-5.58	FREE END	DNA	1071.00	DNA	-

PROPOSED STORM PIPES

PIPE NO.	FROM (DNSTM)	TO (UPSTM)	DISCH. E.I.	INLET E.I.	PLAN (PAY) LGTH (FT)	PIPE LGTH (FT)	SLOPE (%)	PIPE SIZE	TYPE	NOTES
SUNDANCE DR										
* P-2	S-2	S-3	1075.50	1086.83	93	90	12.59%	18"	RCP	(2)
* P-10	S-3	S-10	1086.83	1087.55	87	84	0.86%	18"	RCP	-
* P-10A	S-10	S-10A	1087.55	1087.86	30	27.5	1.13%	18"	RCP	-
* P-10B	S-10A	S-10B	1088.11	1088.50	23.5	21.5	1.81%	15"	RCP	(2)
P-11	S-10	S-11	1088.39	1088.64	53.5	50.5	0.50%	15"	RCP	-
* P-11A	S-11	S-11A	1088.89	1089.04	27	25	0.60%	12"	RCP	-
PINE HOLLOW PL										
* P-20	S-3	S-20	1090.54	1094.61	85	81.5	4.99%	15"	RCP	-
* P-20A	S-20	S-20A	1094.86	1095.01	27.5	25	0.60%	12"	RCP	-
P-21	S-20	S-21	1094.86	1109.06	185	182	7.80%	12"	RCP	-
STORM POND										
P-1	S-1	S-2	1070.48	1071.18	29.5	28	2.50%	21"	RCP	(2)
P-30	S-30	S-31	1071.20	1075.00	34	33	11.52%	12"	RCP	(2)
P-40	S-40	S-41	1070.70	1071.00	15	15	2.00%	3"	PVC	-
** P-50	S-50	S-51	1068.70	1069.39	277.5	274	0.25%	15"	RCP	-
** P-51	S-51	S-52	1070.07	1070.07	21.5	18	3.78%	15"	RCP	-
P-52	S-52	S-53	1070.07	1070.51	100	97	0.45%	15"	RCP	-
P-53	S-53	S-54	1070.51	1070.59	17	13.5	0.59%	15"	RCP	-
P-54	S-54	S-55	1070.79	1071.00	13	12	1.75%	8"	PVC	-

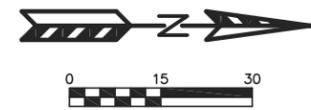
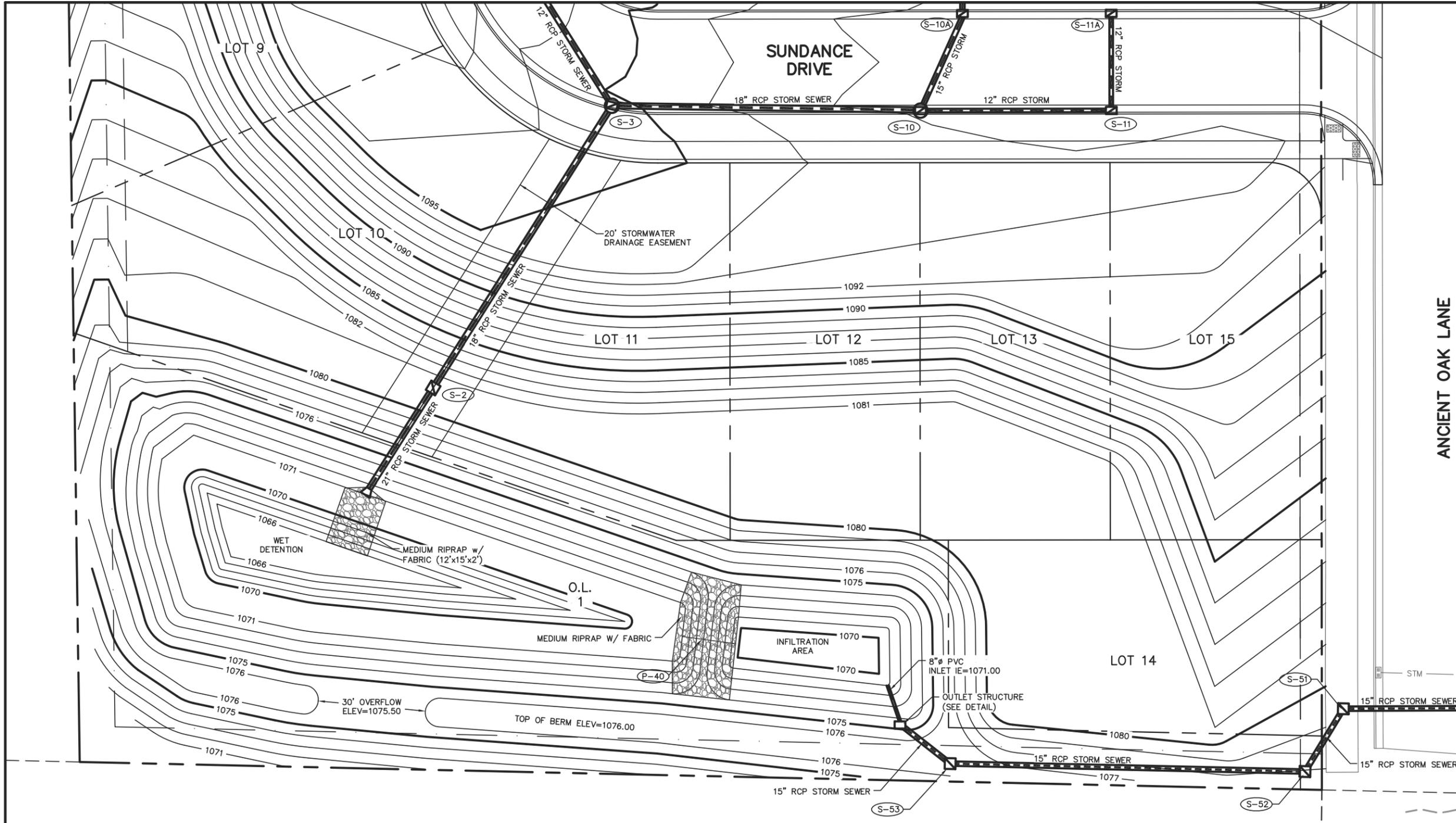
NOTE: PLAN LENGTH (PAY LENGTH) IS FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE. PIPE LENGTH IS ACTUAL LENGTH OF PIPE FROM STRUCTURE WALL TO STRUCTURE WALL. SLOPE CALCULATED USING PIPE LENGTH.

STANDARD NOTES:

- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD POURED STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES; UD = UNDERDRAIN
- APPROXIMATE DISCHARGE E.I. GIVEN, ADJUST E.I. AND PIPE SLOPE IN THE FIELD.
- TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SAS's.
- TOP OF CONCRETE ROOF (TR) IS 1.25' BELOW TOP OF CASTING UNLESS OTHERWISE NOTED.
- ALL REINFORCED CONCRETE PIPES TO BE CLASS III UNLESS OTHERWISE NOTED.
- SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.
- ALL STRUCTURES CALLED OUT AS FIELD POURED SHALL BE FIELD POURED. ALL OTHER STRUCTURES (NOT INDICATED AS FIELD POURED) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL IF PRECAST STRUCTURES ARE PREFERRED. CONTACT KYLE FRANK OF CITY ENGINEERING AT (608) 266-4098 FOR PRECAST APPROVALS, FAX SHOP DRAWINGS TO (608)264-9275, OR EMAIL SHOP DRAWINGS TO KRANK@CITYOFMADISON.COM.

SPECIFIC NOTES

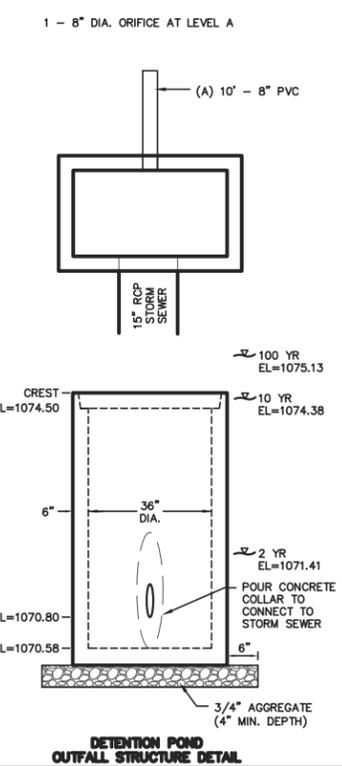
- (1) STATION, OFFSET, AND E.I. GIVEN AT RCP AE END
- (2) PLAN AND PIPE LENGTHS DO NOT INCLUDE LENGTH OF RCP AE. PIPE E.I. IS GIVEN AT AE CONNECTION, FOR AE E.I. SEE STRUCTURE CHART
- (3) E.I.(E) = 1065.27 (24"), E.I.(W) = 1065.78 (24"), VERIFY EXISTING E.I. IN FIELD
- (4) SEE DETAIL SHEET D-2 FOR INLET DIMENSIONS AND CASTING INFORMATION



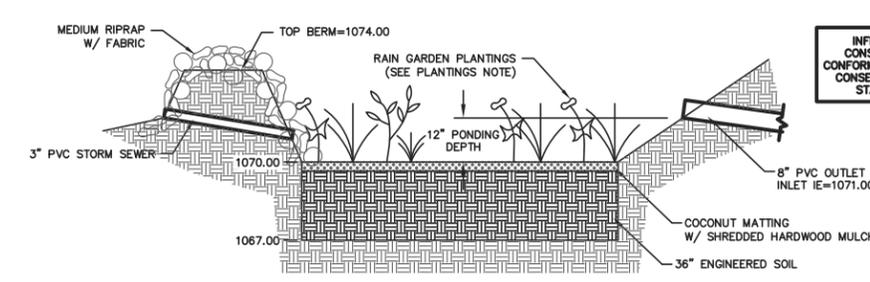
Kevin Parish
9-15-14

ANCIENT OAK LANE

TRASH RACK NOTE:
RISER STRUCTURE SHALL HAVE A PRE-FABRICATED TRASH RACK FROM HAALA INDUSTRIES OR EQUAL.



DETENTION POND OUTFALL STRUCTURE DETAIL



INFILTRATION AREA CONSTRUCTION SHALL CONFORM TO WISCONSIN DNR CONSERVATION PRACTICE STANDARD #1004.

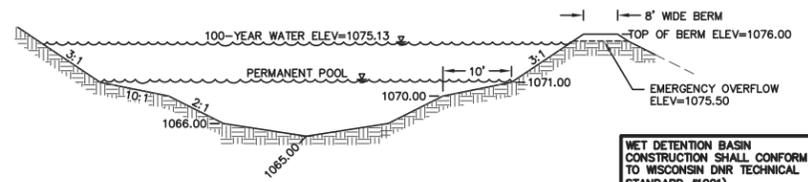
ENGINEERED SOIL SHALL CONFORM WITH WISCONSIN DNR CONSERVATION PRACTICE STANDARD 1004 AND CONSIST OF THE FOLLOWING:
70 - 85% SAND AND 15 - 30% COMPOST
COMPOST SHALL MEET MNOR SPECIFICATION S100.

STORAGE/INTERFACE LAYER SHALL CONSIST OF SAND IN CONFORMANCE WITH WISCONSIN DNR CONSERVATION PRACTICE STANDARD 1004.

INFILTRATION AREA PLANTINGS TO BE PLANTED AT ONE PLUG PER SQUARE FOOT. PLUGS TO BE PLANT STOCK NAMED IN THE WET PRAIRIE SHORT STATURE MIX FROM AGRECOL CORPORATION OR APPROVED EQUIVALENT. A MINIMUM OF 10 DIFFERENT PLANT STOCK NAMES TO BE PLANTED.

TO PREVENT COMPACTION OF ENGINEERED SOIL AND SUBSOILS, CONTRACTOR SHALL PROTECT AGAINST MACHINERY ENTERING OR COMPACTING THE INFILTRATION AREA.

INFILTRATION AREA DETAIL



WET DETENTION BASIN CONSTRUCTION SHALL CONFORM TO WISCONSIN DNR TECHNICAL STANDARD #1001

WET DETENTION POND CROSS SECTION

CALL DIGGERS HOTLINE
1-800-242-8511
TOLL FREE

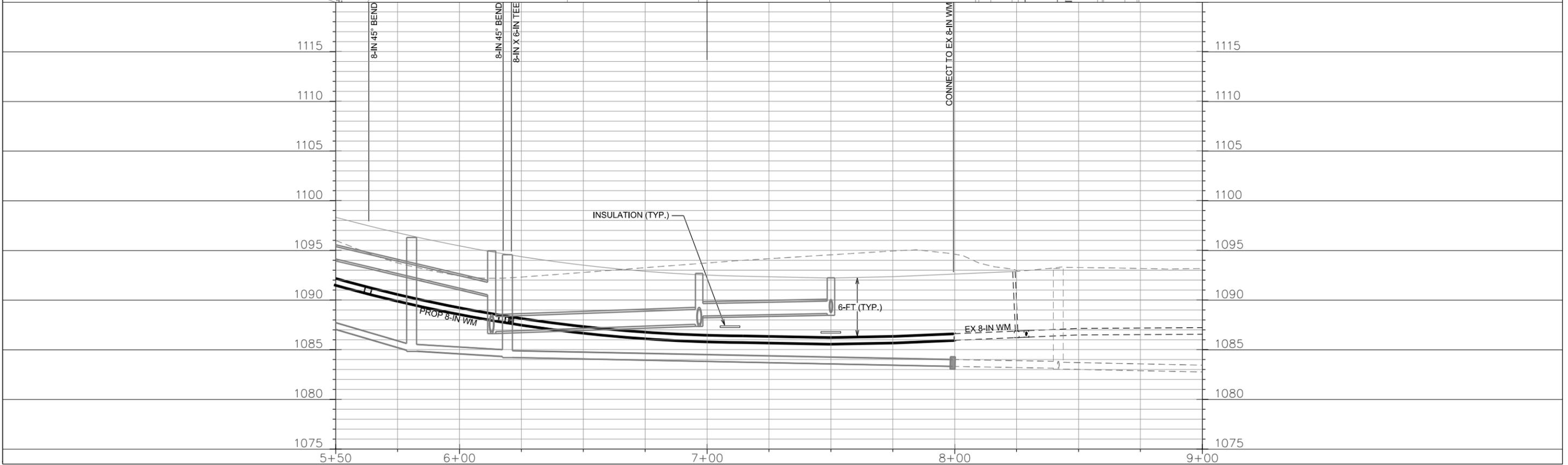
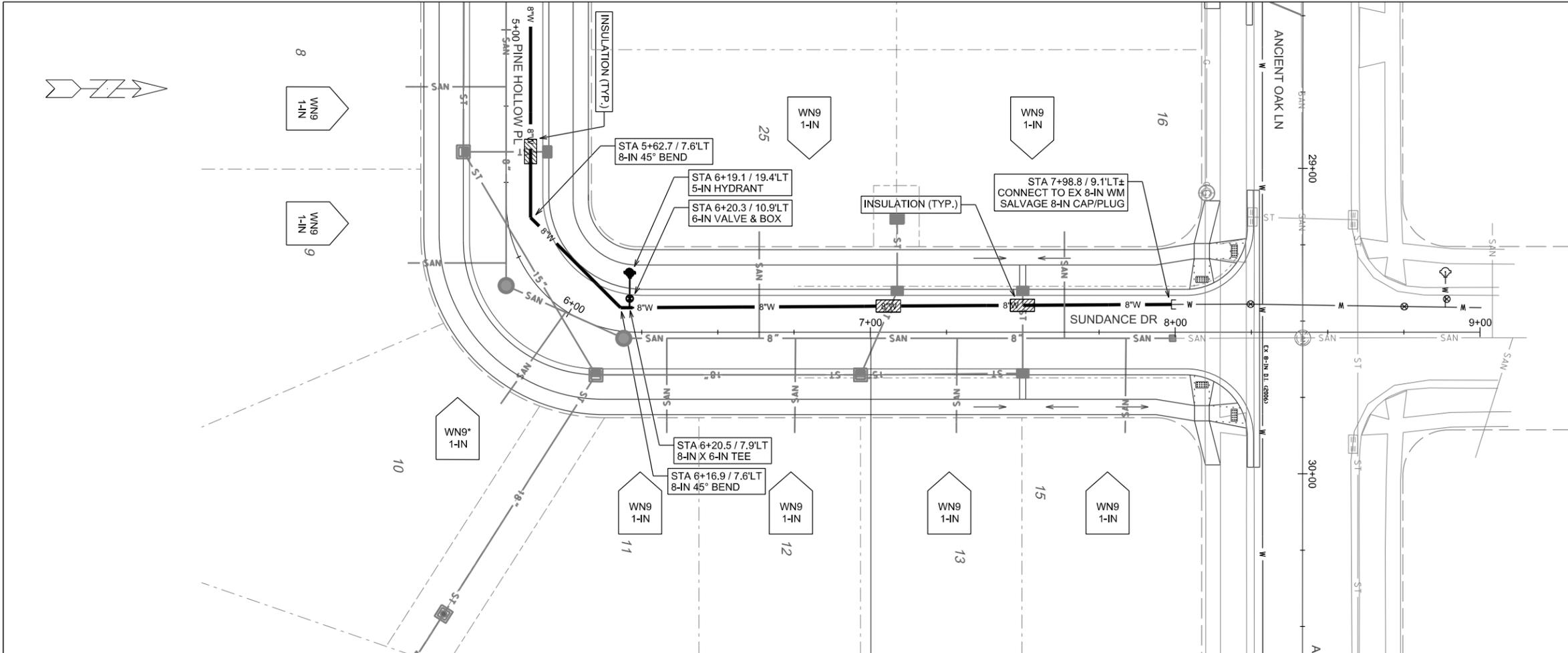
TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

TDD(FOR THE HEARING IMPAIRED)(800)542-2289

WS. STATUTE 182.0175 (1974) REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE

HAWK'S CROSSING
DETENTION POND PLAN
PAGE: 3 OF 7
DATED: JULY 24, 2014; REVISED SEPTEMBER 12, 2014

QUAM ENGINEERING, LLC
Residential and Commercial Site Design Consultants
www.quamengineering.com
4893 Larson Beach Road; McFarland, Wisconsin 53558
Phone (608) 838-7750; Fax (608) 838-7752

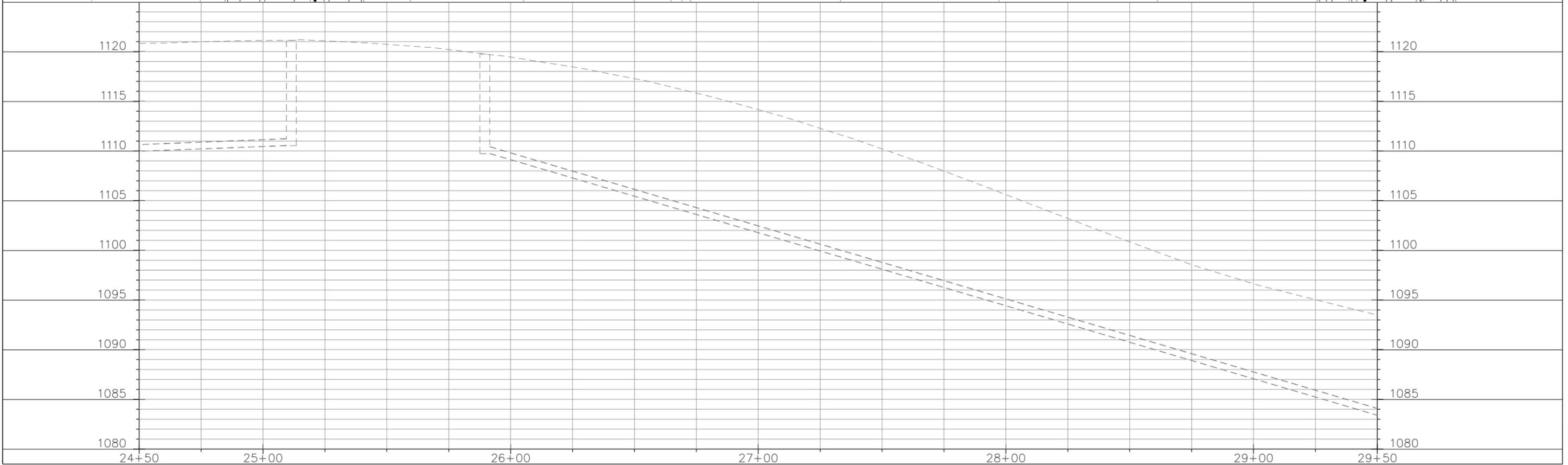
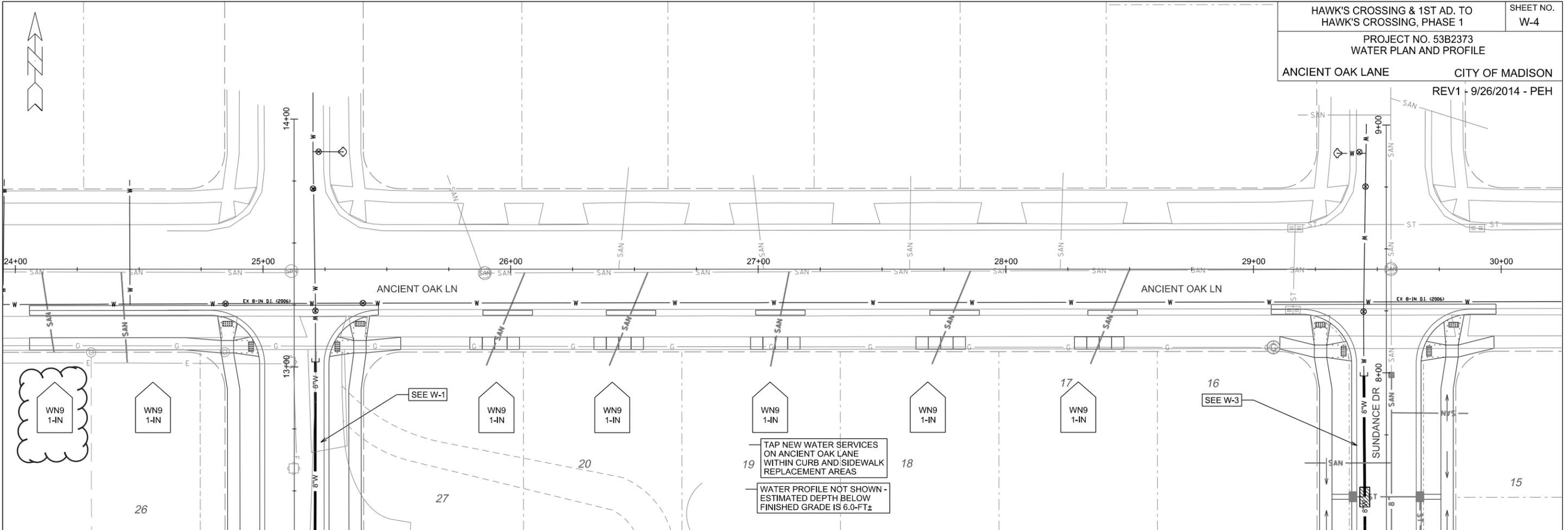


PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

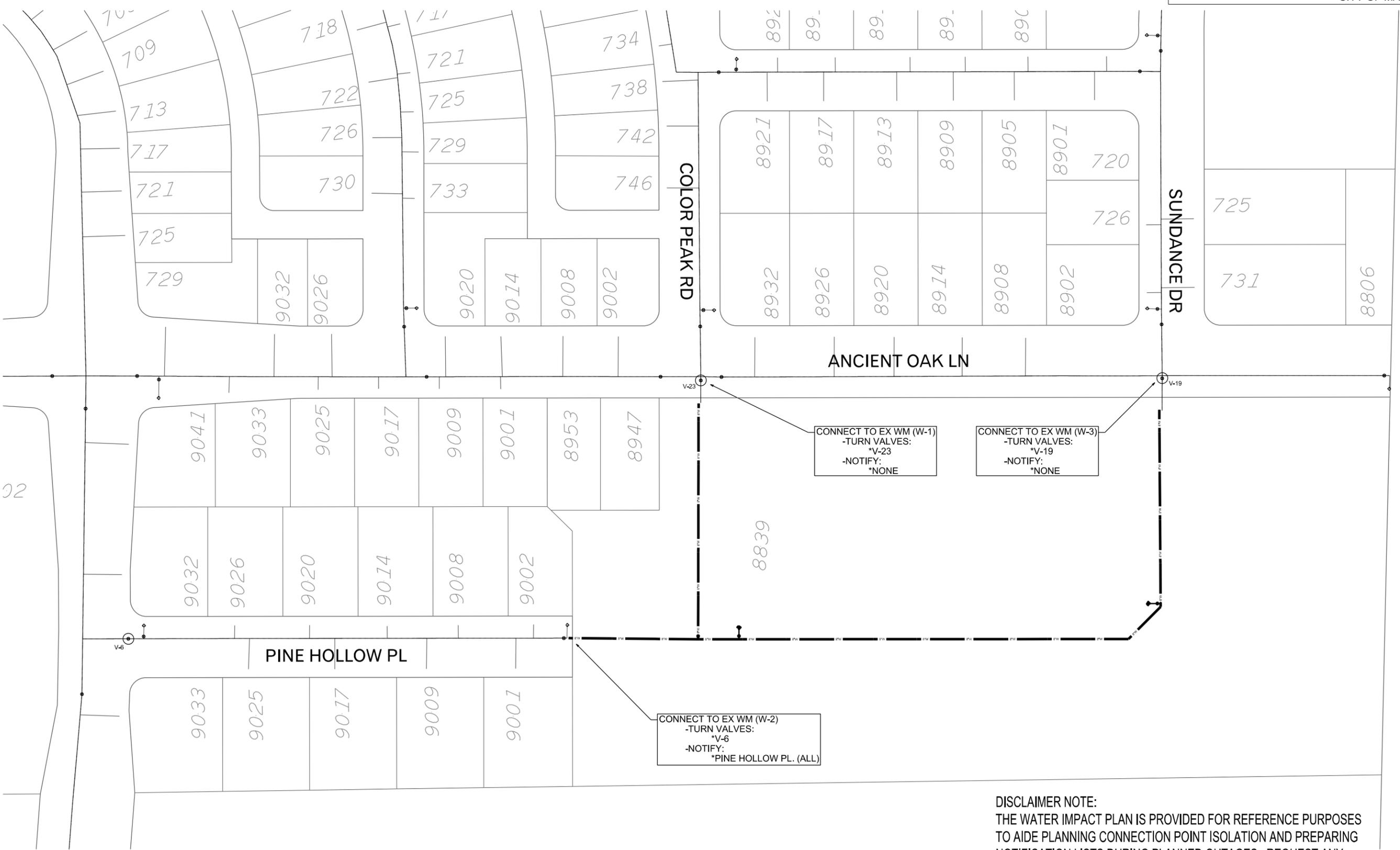


PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



DISCLAIMER NOTE:
 THE WATER IMPACT PLAN IS PROVIDED FOR REFERENCE PURPOSES TO AIDE PLANNING CONNECTION POINT ISOLATION AND PREPARING NOTIFICATION LISTS DURING PLANNED OUTAGES. REQUEST ANY ALTERNATIVE CONNECTION METHODS IN WRITING, PER SPECS.

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

FILE NAME:

DATE:

DISCLAIMER NOTE:
UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXACT HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UNDERGROUND AND OVERHEAD UTILITIES PRIOR TO COMMENCING WORK.

CONSTRUCTION NOTES:

1. CONSTRUCT NEW WATER MAIN 6.0' BELOW FINISHED GRADE, UNLESS OTHERWISE NOTED. INSULATE MAIN WITH POLYSTYRENE BOARD AT UTILITY CROSSINGS OR OTHER AREAS IDENTIFIED BY ENGINEER AS HAVING INADEQUATE COVER.
2. VERIFY SIZE OF EXISTING WATER SERVICES AND RECONNECT SERVICES AS INDICATED.
3. MINIMIZE DISTRUPTION OF SERVICE TO EXISTING CUSTOMERS. NOTIFY PER CONTRACT REQUIREMENTS OF ANY PLANNED WATER OUTAGE.
4. THE EXISTING UTILITIES SHOWN ON THIS PLAN REPRESENT THE BEST INFORMATION AVAILABLE TO THE WATER UTILITY AT THE TIME OF PLAN PREPARATION. CONTRACTOR IS RESPONSIBLE FOR HAVING EACH UTILITY LOCATED PRIOR TO COMMENCING WORK.

- WN1 REPLACE THE EXISTING LEAD SERVICE WITH A NEW COPPER SERVICE.
- WN2 EXTEND AND RECONNECT THE EXISTING COPPER SERVICE TO THE NEW WATER MAIN.
- WN3 EXISTING SERVICE TO BE ABANDONED WHEN THE WATER MAIN IS CUT OFF.
- WN4 DISCONNECT FROM THE OLD WATER MAIN AND RECONNECT THE EXISTING COPPER WATER SERVICE LATERAL TO THE NEW WATER MAIN.
- WN5 RELOCATE THE EXISTING FIRE HYDRANT.
- WN6 ABANDON WATER VALVE ACCESS STRUCTURE.
- WN7 FURNISH AND INSTALL THE NEW TOP SECTION FOR THE WATER ACCESS STRUCTURE.
- WN8 ABANDON THE VALVE BOX.
- WN9 FURNISH THE DITCH, COMPACTION, AND ALL MATERIALS AND LABOR FOR THE INSTALLATION OF NEW SERVICE LATERAL.
- WN10 REMOVE AND SALVAGE EXISTING HYDRANT
- WN11 REPLACE THE EXISTING COPPER SERVICE WITH A COPPER SERVICE
- WN20+ SEE WATER IMPACT PLAN FOR CONNECTION POINT ISOLATION AND WATER SHUT-OFF NOTFICATION INFORMATION.

ESTIMATE OF MATERIALS SUPPLIED BY CONTRACTOR:

* ESTIMATE OF MATERIALS IS FOR INFORMATION ONLY. ENGINEER DOES NOT GUARANTEE ACCURACY OF MATERIAL TAKE-OFF.

- 40-FT - 6-IN PIPE
- 960-FT - 8-IN PIPE
- 1,110-FT - POLYWRAP
- 2 - 6-IN VALVE & BOX
- 1 - 8-IN VALVE & BOX
- 2 - 8-IN 45° BEND
- 1 - 8-IN X 6-IN TEE
- 1 - 8-IN X 8-IN TEE
- 2 - 5-IN HYDRANT
- 40-FT - 2-IN STYROFOAM INSULATION
- 1-IN COPPER TUBING AS REQ'D

MATERIALS SUPPLIED BY CITY:

NONE

REUSED MATERIALS

- 1 - 8-IN X 6-IN TEE

SALVAGED MATERIALS

- 1 - 5-IN HYDRANT
- 2 - 8-IN MJ CAP/PLUG

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

CALL DIGGERS HOTLINE TOLL FREE 811 OR 1-800-242-8511

FAX-A-LOCATE 1-800-338-3860

TDD (FOR HEARING IMPAIRED) 1-800-542-2289

WIS. STATUTE 182.0175 (1974) REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE.

